



**DEPARTMENT OF PLANNING AND DEVELOPMENT
CITY OF CHICAGO**

February 7, 2024

Meg George
Akerman LLP
444 W. Lake St., Suite 900
Chicago, IL 60606

Re: Air Rights Waterway Business Residential Planned Development No.1426, Revised Site Plan Approval for Subarea A and interim surface parking lots serving Bally's Casino.

Dear Ms. George:

Please be advised that your request for site plan approval to Planned Development No. 1426 ("PD 1426"), Subarea A and the associated interim surface parking lots, has been considered by the Department of Planning and Development (DPD) pursuant to Section 17-13-0800 of the Chicago Zoning Ordinance and in accordance to the provisions of Statement Number 12 of PD 1426. You are requesting on behalf of your clients, Bally's Chicago Operating Company LLC., the property owners of Subarea A, a site plan approval for development at the subject site.

You are requesting site plan approval specifically within Sub-Area A to allow for the construction of a mixed-use entertainment complex to be located at 777 W. Chicago Avenue, in addition you are seeking site plan approval for adjacent parcels to be utilized as interim surface parking lots. The development as proposed includes a casino, a 3,000-seat entertainment venue, and various eating and drinking and retail establishments. In addition, the proposal contains a multi-level parking garage below grade, and a river walk in compliance with the River Design Guidelines that extends the entire length of the property from Grand Avenue north to Chicago Avenue, in Phase 1 the Riverwalk will be completed to full build out from Chicago Avenue to the southern edge of the park area with an interim connection continuing south to Grand Avenue; this remaining portion will be completed to full once the adjacent sub-area is approved via site plan approval request through the Department. The proposed bulk, uses, and density of the proposal all remain compliant with documents already reviewed and approved by the Chicago City Council and published in the City Council Journal dated December 14, 2022.

Pursuant to Statement No. 12 of PD 1426, it was acknowledged that the final design of the proposed development was in a state of evolution and as a result would be finalized and memorialized via this process. Additionally, reference has been made in several documents to inclusion of public art as part of the proposed casino development, none of that art is finalized or included as part of the site plan approval letter issued here today; the inclusion of public art will be done in coordination between the applicant, the DPD, the Department of Cultural Affairs and Special Events and, when any public right-of-way is involved, the Department of Transportation (CDOT).

We have reviewed the following drawings and exhibits and those are made part of this planned development and accompany this letter:

- Existing Zoning Map
- Existing Land Use Map
- General Land Use Map
- Planned Development Boundary and Property Line Map
- Right of Way Adjustment Map
- Site Sections
- Development Plan Associated with Infrastructure Requirements
- Sub-Area and Parcelization Plan
- Development Phasing Diagram
- Phase I: Interim Site Plan for Parking and Riverwalk
- Phase I: Interim Site Plan for Parking
- Phase I: Employee Parking Lot South
- Phase I: Employee Parking Lot North
- Site Access and Loading Plan Level 1
- Site Access and Loading Plan Level 2
- Site Level Plan – Level 1
- Site Level Plan – Level 2
- Site Level Plan – Level 3
- Site Sections – 1 of 2
- Site Sections – 2 of 2
- Public Open Space Plan
- Generalized Landscape Plan
- Bike and Pedestrian Connectivity Plan – Full Build-Out
- Bike and Pedestrian Connectivity Plan – Phase 1
- Transitway Accommodation Plan
- Design Guidelines
- Phase 1 – Green Roof Plan
- Phase 1 – Floor Plan L1
- Phase 1 – Floor Plan LL1
- Phase 1 – Floor Plan LL2
- Phase 1 – Floor Plan LL3
- Phase 1 – Typical Hotel Floor Plans
- Phase 1 – Building Section 1 of 2
- Phase 1 – Building Section 2 of 2
- Phase 1 – Proposed Landscape Plan
- Phase 1 – Riverwalk Site Sections
- Phase 1 – Riverwalk Site Section C (Event Center)
- Phase 1 – Building Elevations North and South
- Phase 1 – Building Elevations East and West
- Rendered Images of the Proposed Development (12)
- Update River Design Guidelines Checklist and 12 illustrative exhibits
- (7) Section and Vignette Exhibits detailing proposed building construction
- CL1.22 – Planting Plan – Interim Parking Lots
- CL2.00 – Planting Schedule
- CL3.00 – Landscape Details

While the above list is extensive it is still the obligation of the development team to remain compliant with PD 1426, the Host Community Agreement, and the Municipal Code of Chicago. Additional review of submitted documents will occur at the time of permitting as if the case with all development proposals to ensure compliance.

DPD has worked closely with CDOT on this Site Plan Review request. As a result of these reviews CDOT has reviewed and approved the phase 1 site plan and companion traffic memo, finding it acceptable for approval, conditional upon ongoing coordination on the Jefferson St. cross section and treatment of the western edge, ongoing coordination on the design of signalized intersections design on Grand and Chicago Ave., ongoing coordination on east-west connection streets on Erie, and future Huron, Ancona & Desplaines, and on the finalization and approval of the complete landscape plans.

Regarding your request, DPD has determined that allowing the proposed development in Sub-Area A and the establishment of interim surface parking lots will not create an adverse impact on the remainder of the PD 1426 or the surrounding neighborhood, will not result in an increase in the height, bulk, or density of Subarea A as approved, and will not change the character of the development.

Accordingly, pursuant to the authority granted by the Chicago Zoning Ordinance and PD 1426, I hereby approve the foregoing site plan approval request, but no other changes to PD 1426. The attached exhibits are in accordance with and satisfy the requirements of PD 1426. Accordingly, this site plan approval request is hereby approved.

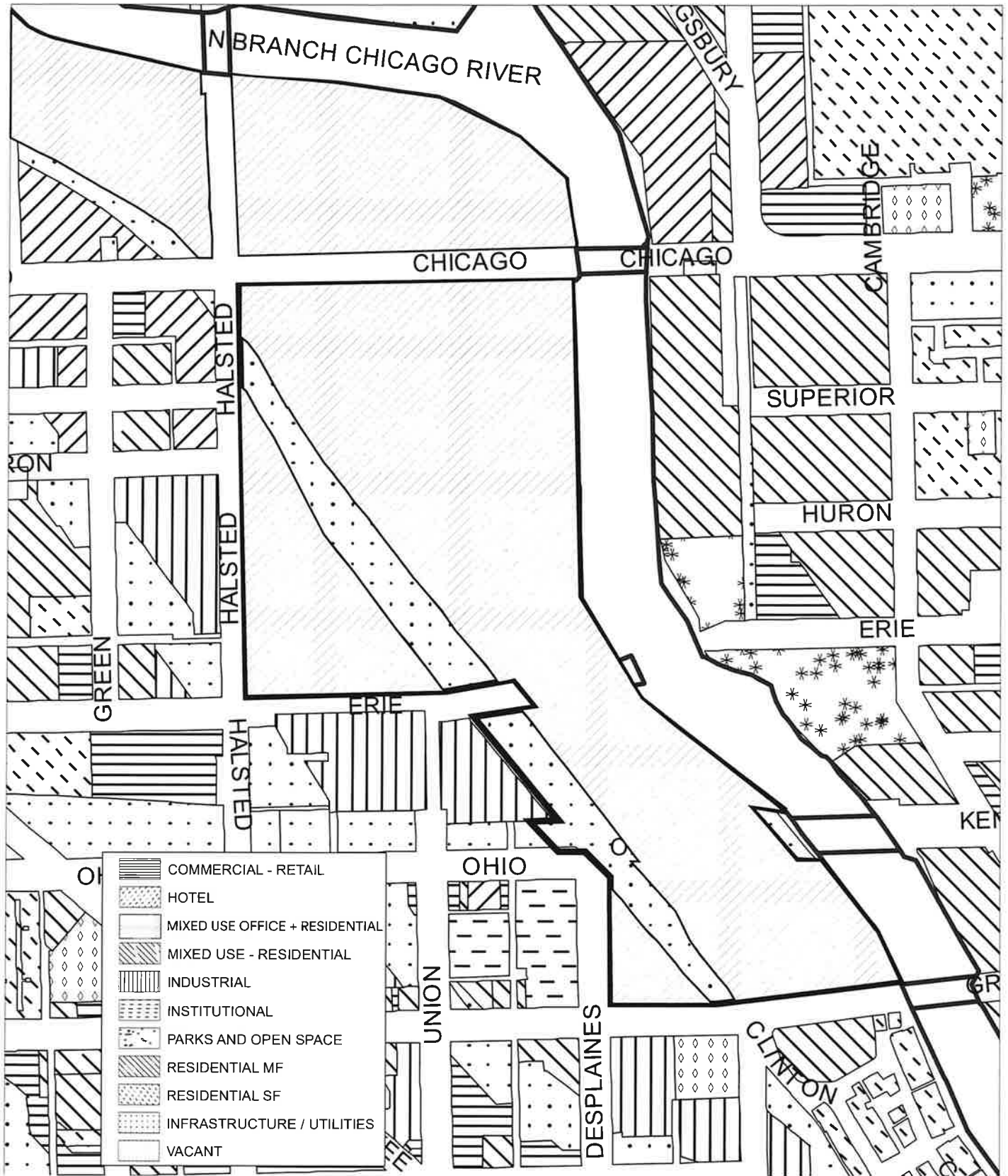
Sincerely,



Patrick Murphey
Zoning Administrator

CC: Cindy Roubik, Noah Szafraniec, Ethan Gillman, Mike Marmo, Erik Glass, Main file

121 NORTH LASALLE STREET, ROOM 1101, CHICAGO ILLINOIS 60602



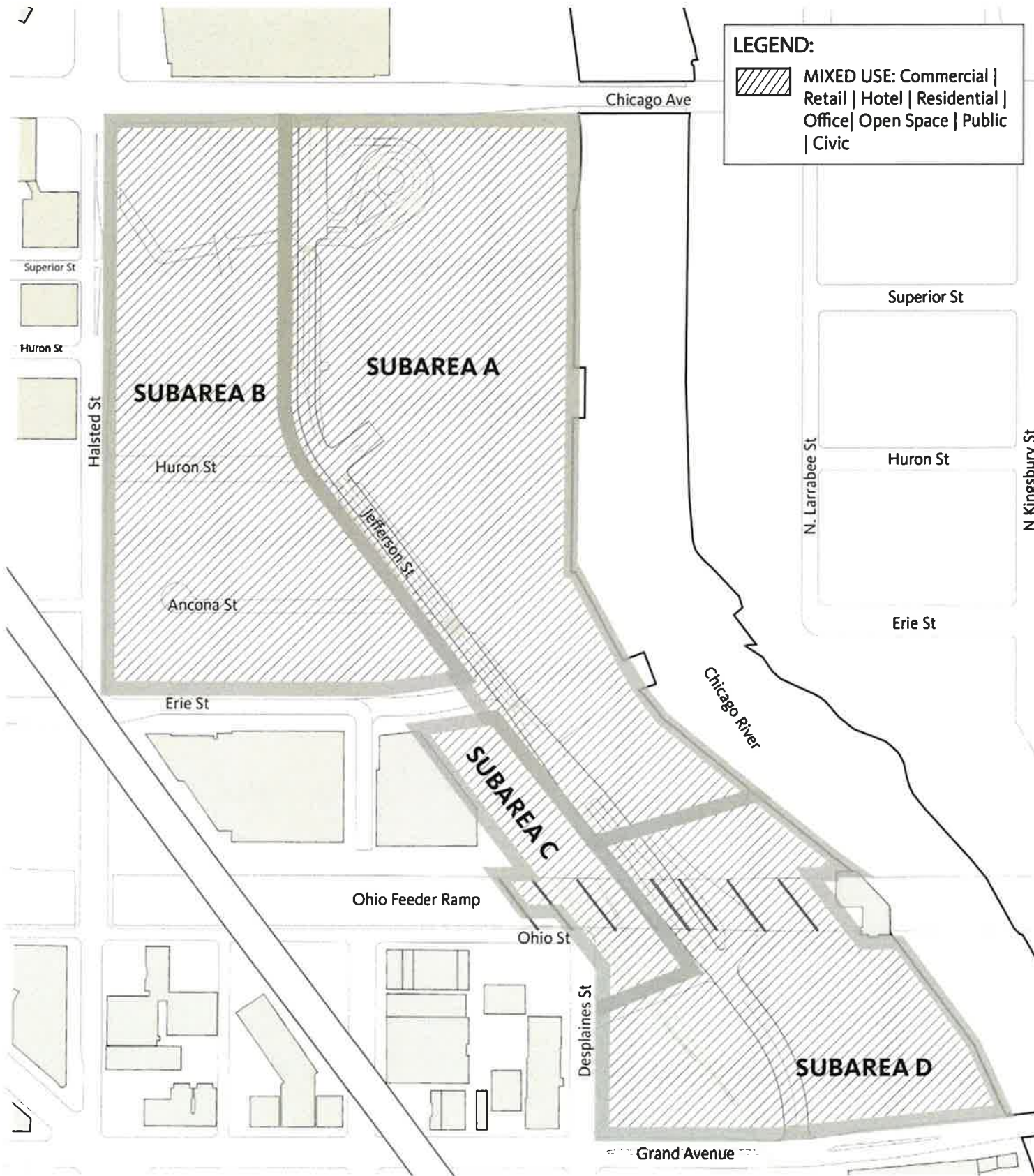
EXISTING LAND USE MAP

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **January 17, 2024**

SCALE: 1" = 170'-0"





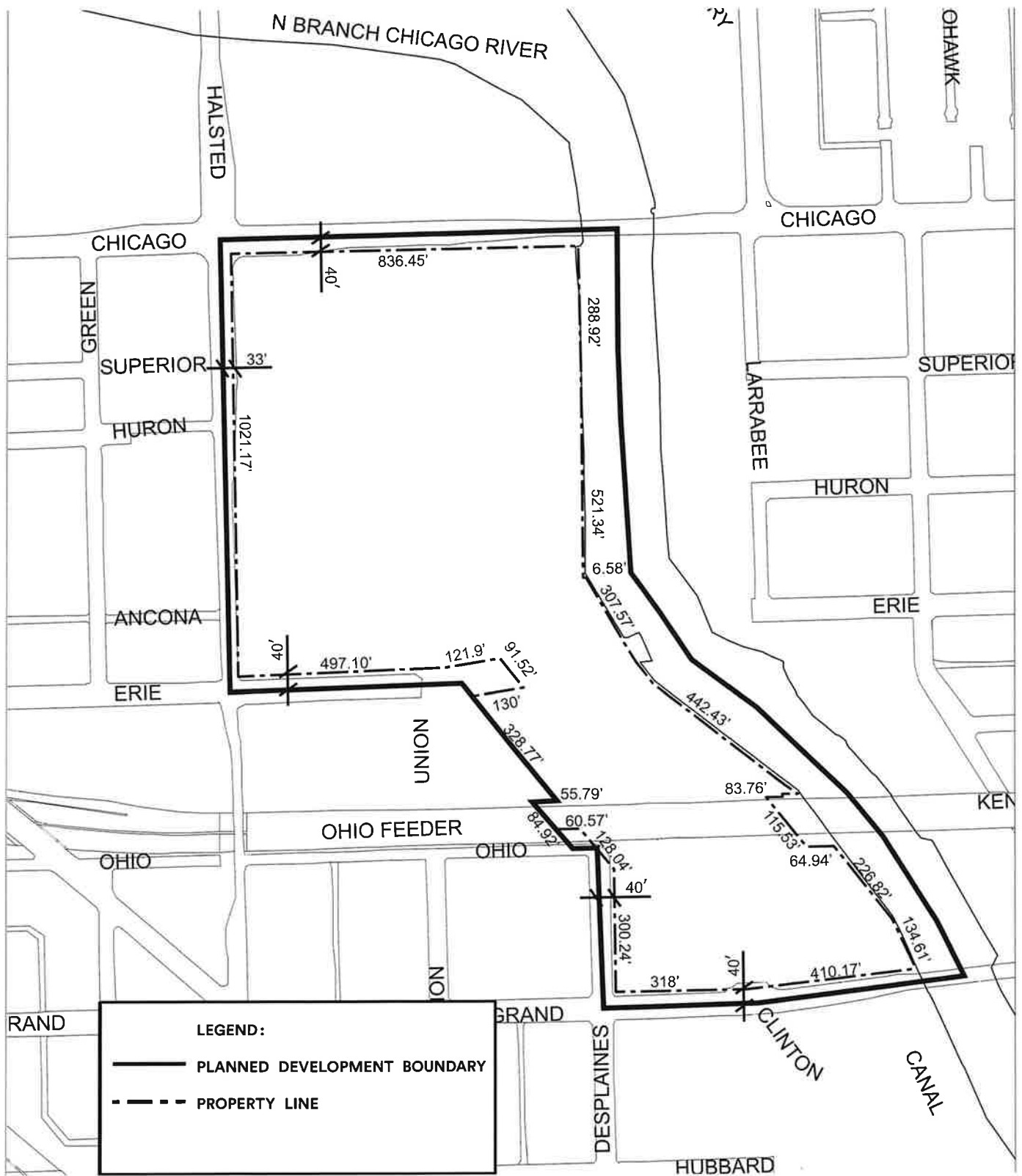
GENERAL LAND USE MAP

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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SCALE: 1" = 240'-0"





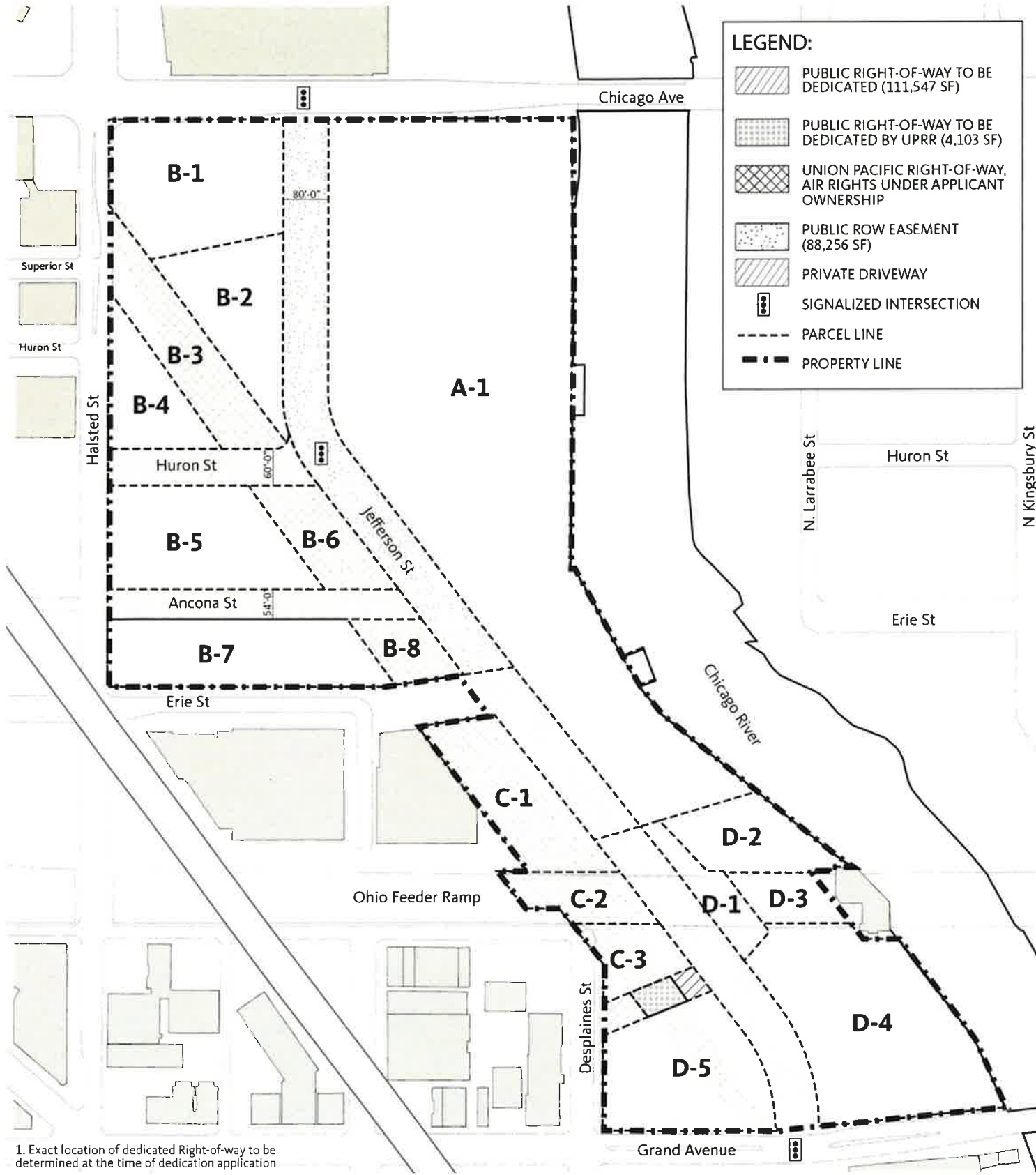
PLANNED DEVELOPMENT BOUNDARY AND PROPERTY LINE MAP

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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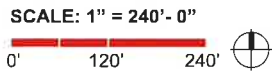


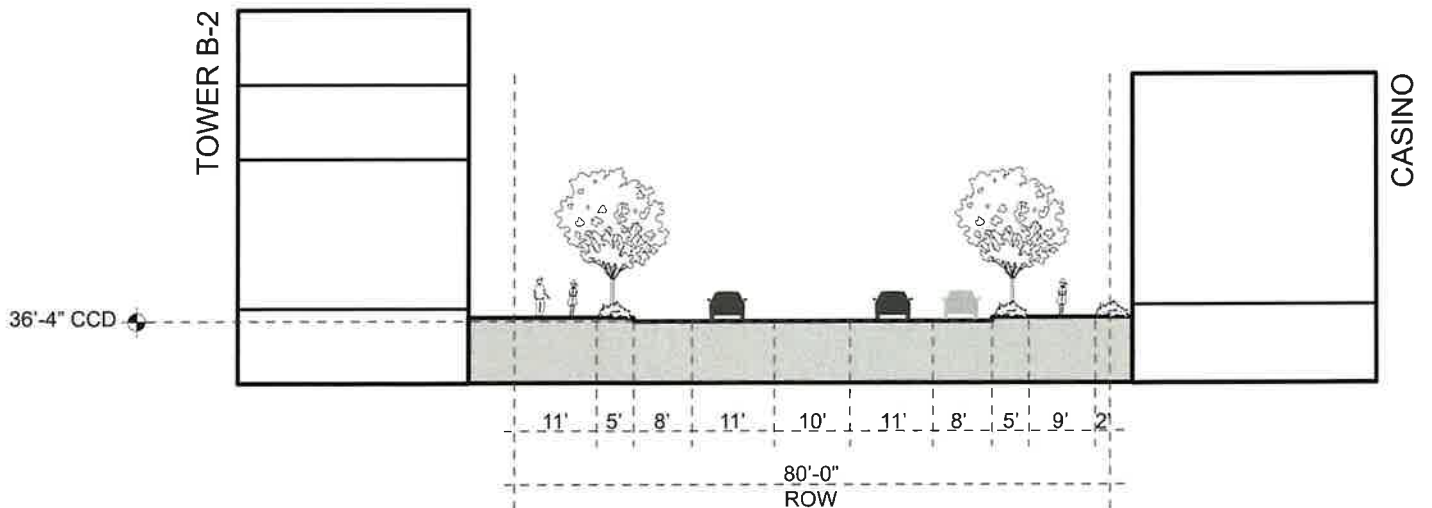
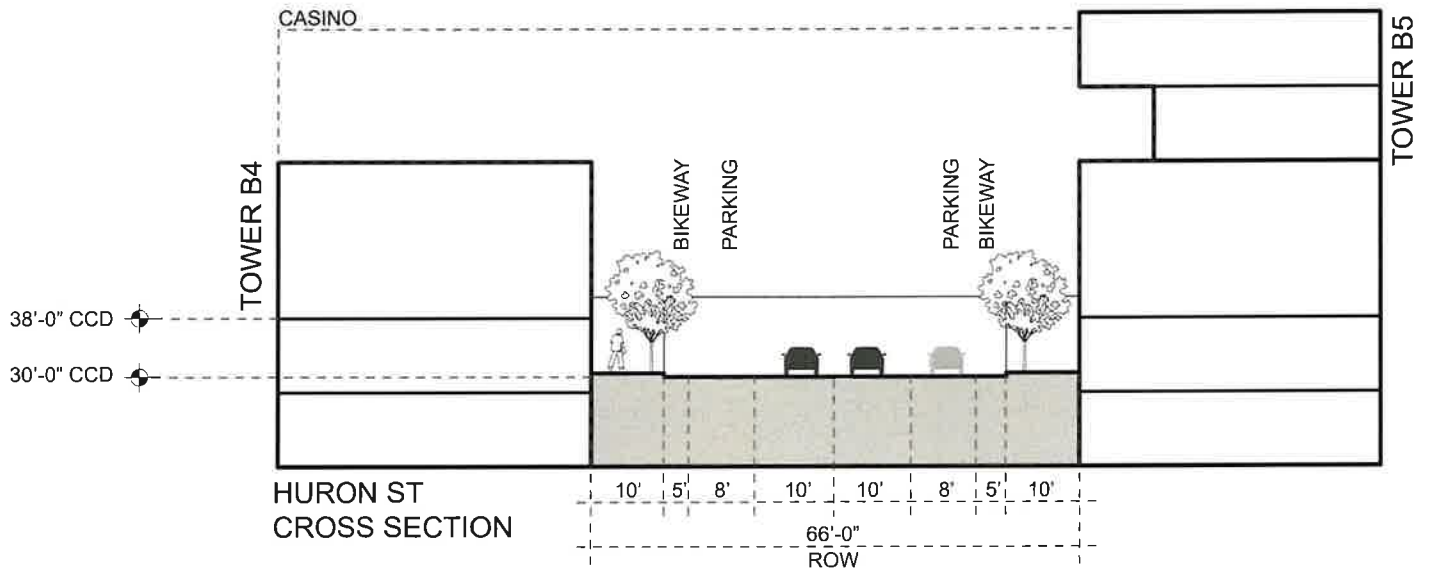
1. Exact location of dedicated Right-of-way to be determined at the time of dedication application
2. U.P. ROW to be held for future transitway, pending coordination with CDOT and DPD

RIGHT OF WAY ADJUSTMENT PLAN

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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JEFFERSON ST CROSS SECTION

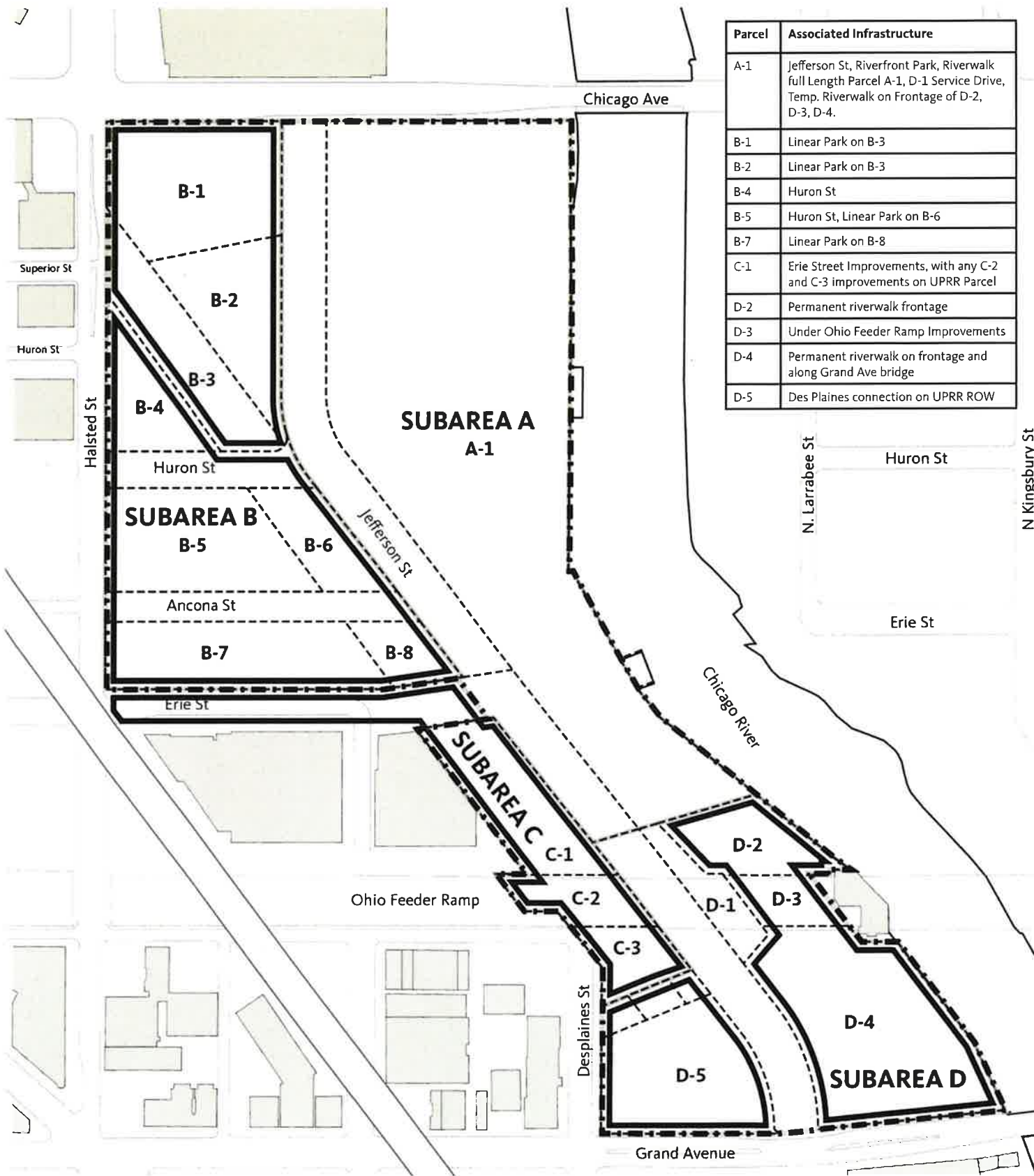
SITE SECTIONS

SCHEMATIC DESIGN FOR AT-
GRADE LEVEL IMPROVEMENTS
ONLY.



Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
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 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; &
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Parcel	Associated Infrastructure
A-1	Jefferson St, Riverfront Park, Riverwalk full Length Parcel A-1, D-1 Service Drive, Temp. Riverwalk on Frontage of D-2, D-3, D-4.
B-1	Linear Park on B-3
B-2	Linear Park on B-3
B-4	Huron St
B-5	Huron St, Linear Park on B-6
B-7	Linear Park on B-8
C-1	Erie Street Improvements, with any C-2 and C-3 improvements on UPRR Parcel
D-2	Permanent riverwalk frontage
D-3	Under Ohio Feeder Ramp Improvements
D-4	Permanent riverwalk on frontage and along Grand Ave bridge
D-5	Des Plaines connection on UPRR ROW

DEVELOPMENT PLAN ASSOCIATED WITH INFRASTRUCTURE REQUIREMENTS

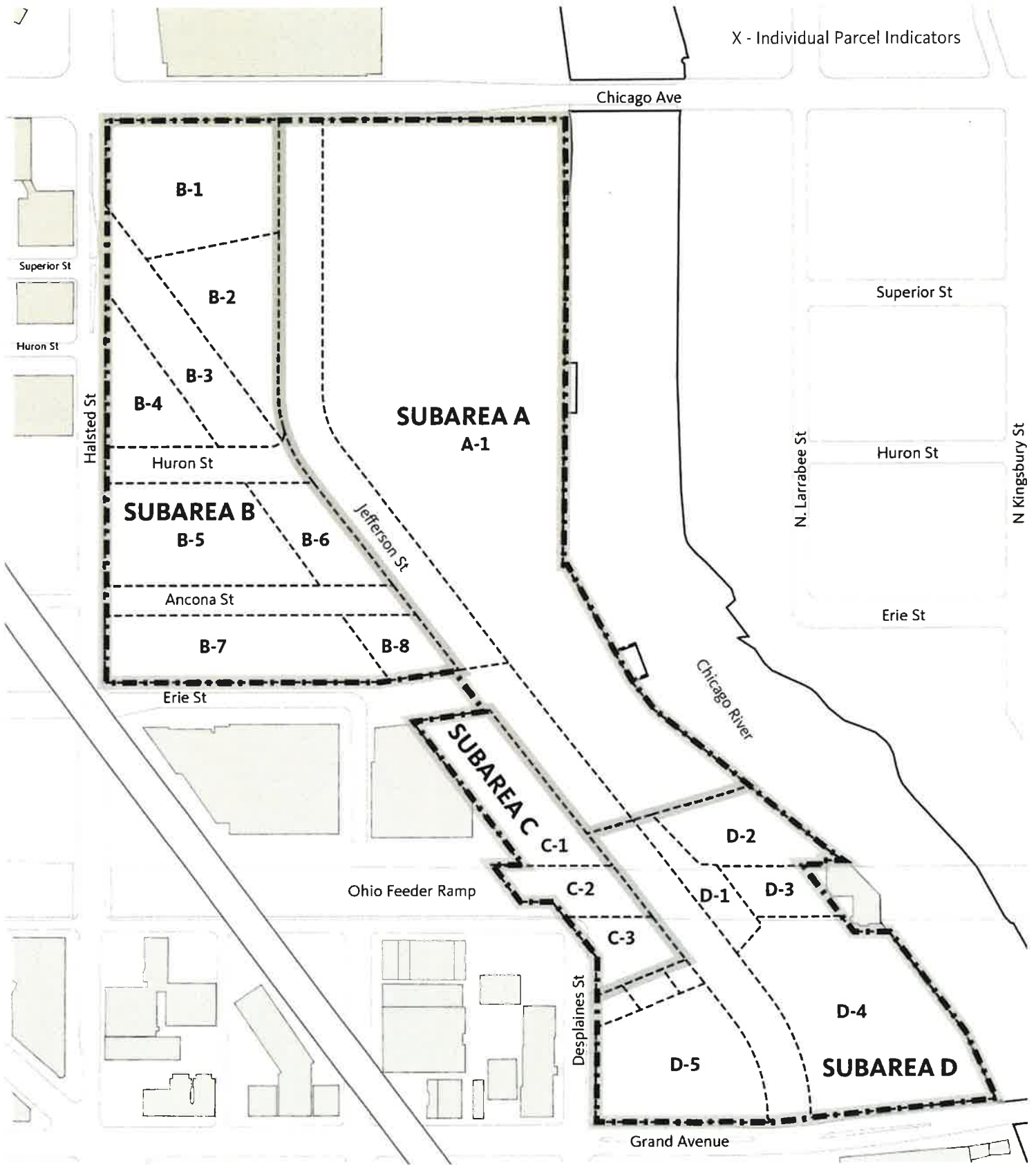
Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
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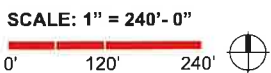
X - Individual Parcel Indicators

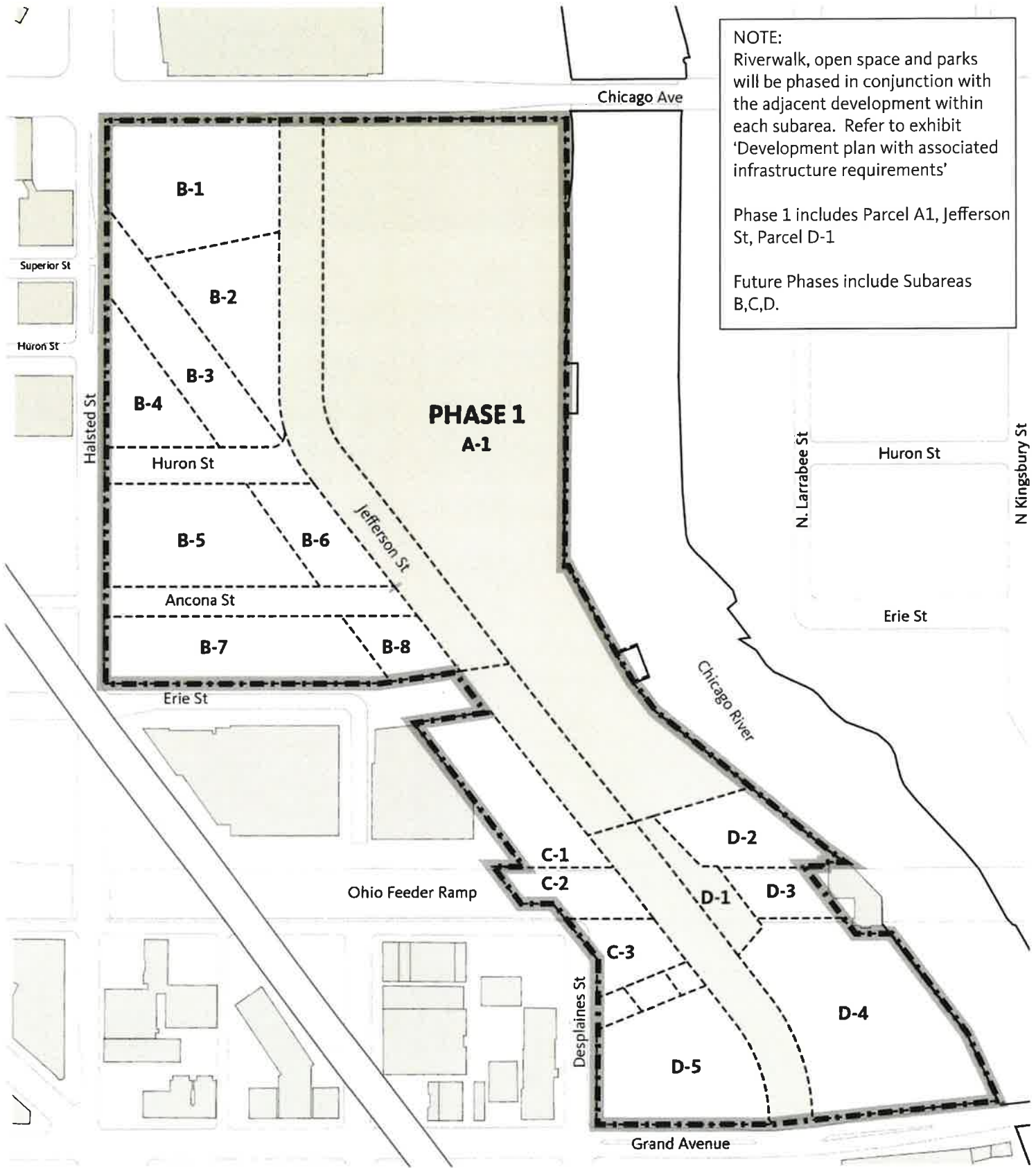


SUB AREA AND PARCELIZATION PLAN

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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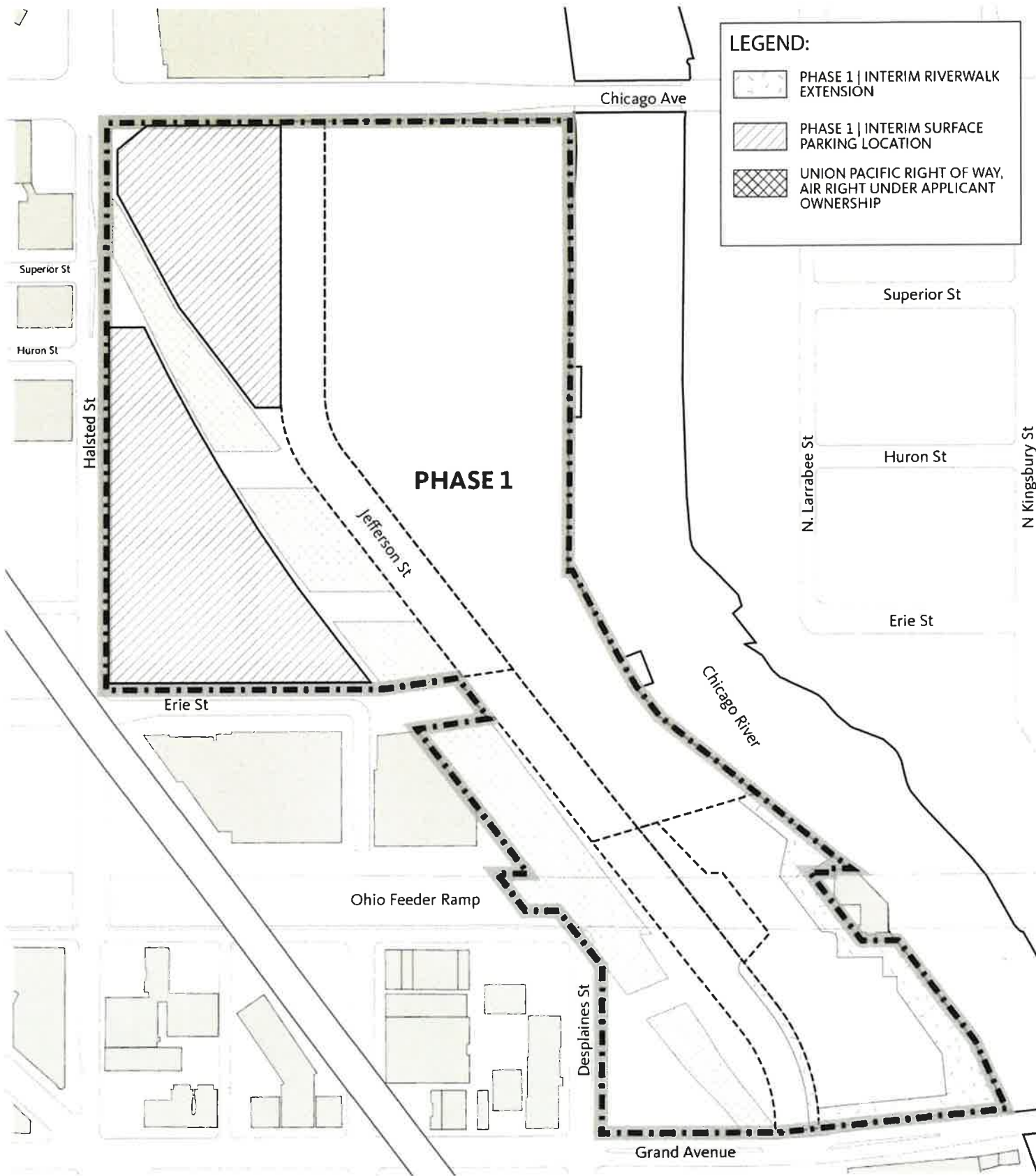


DEVELOPMENT PHASING DIAGRAM

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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SCALE: 1" = 240'- 0"
 0' 120' 240'



NOTE: Refer to detailed interim parking site plan

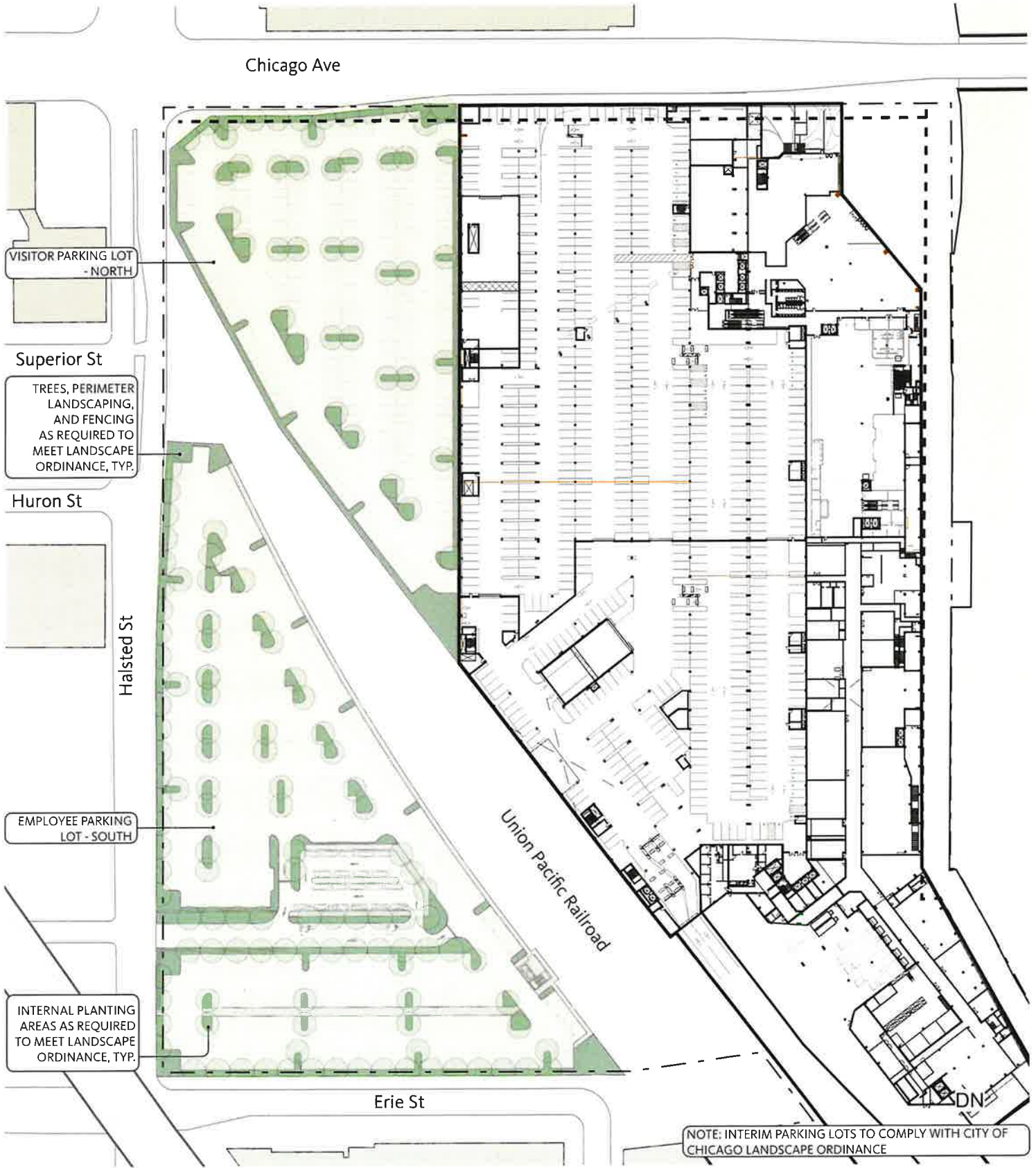
PHASE 1: INTERIM SITE PLAN FOR PARKING AND RIVERWALK

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

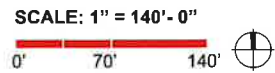
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Parking		2,677 sp
Patron Spaces		1,840 sp
Employee Spaces		393 sp
Valet Spaces		444 sp



PHASE 1: INTERIM SITE PLAN FOR PARKING

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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NOTE: INTERIM PARKING LOTS TO COMPLY WITH CITY OF CHICAGO LANDSCAPE ORDINANCE

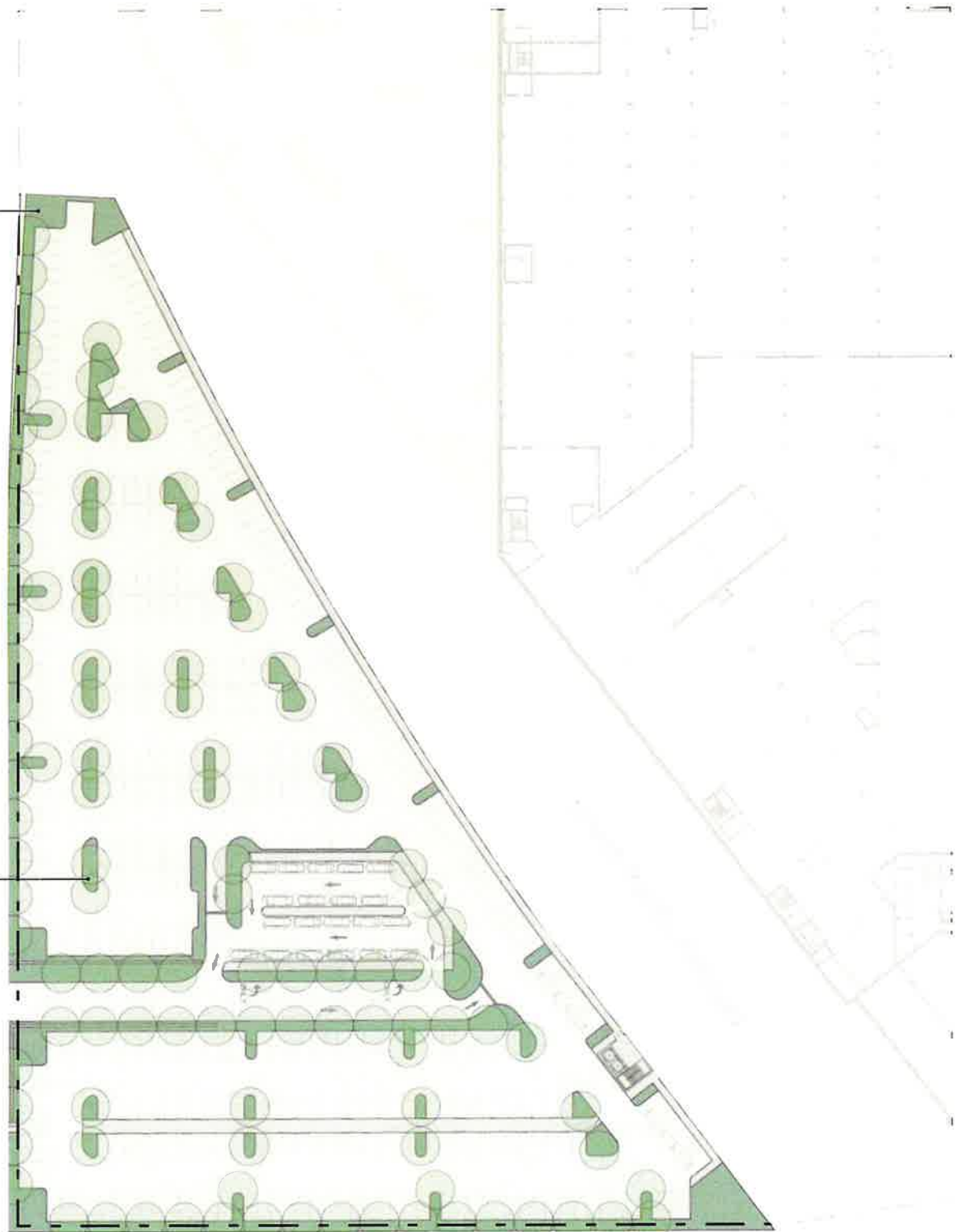
Superior St

Huron St

Halsted St

TREES, PERIMETER
LANDSCAPING,
AND FENCING
AS REQUIRED TO
MEET LANDSCAPE
ORDINANCE, TYP.

INTERNAL PLANTING
AREAS AS REQUIRED
TO MEET LANDSCAPE
ORDINANCE, TYP.



NOTE: INTERIM PARKING LOTS TO COMPLY WITH CITY OF CHICAGO LANDSCAPE ORDINANCE

PHASE 1: EMPLOYEE PARKING LOT - SOUTH

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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SCALE: 1" = 100'-0"



Chicago Ave

TREES, PERIMETER
LANDSCAPING,
AND FENCING
AS REQUIRED TO
MEET LANDSCAPE
ORDINANCE, TYP.

INTERNAL PLANTING
AREAS AS REQUIRED
TO MEET LANDSCAPE
ORDINANCE, TYP.



NOTE: INTERIM PARKING LOTS TO COMPLY WITH CITY OF CHICAGO LANDSCAPE ORDINANCE

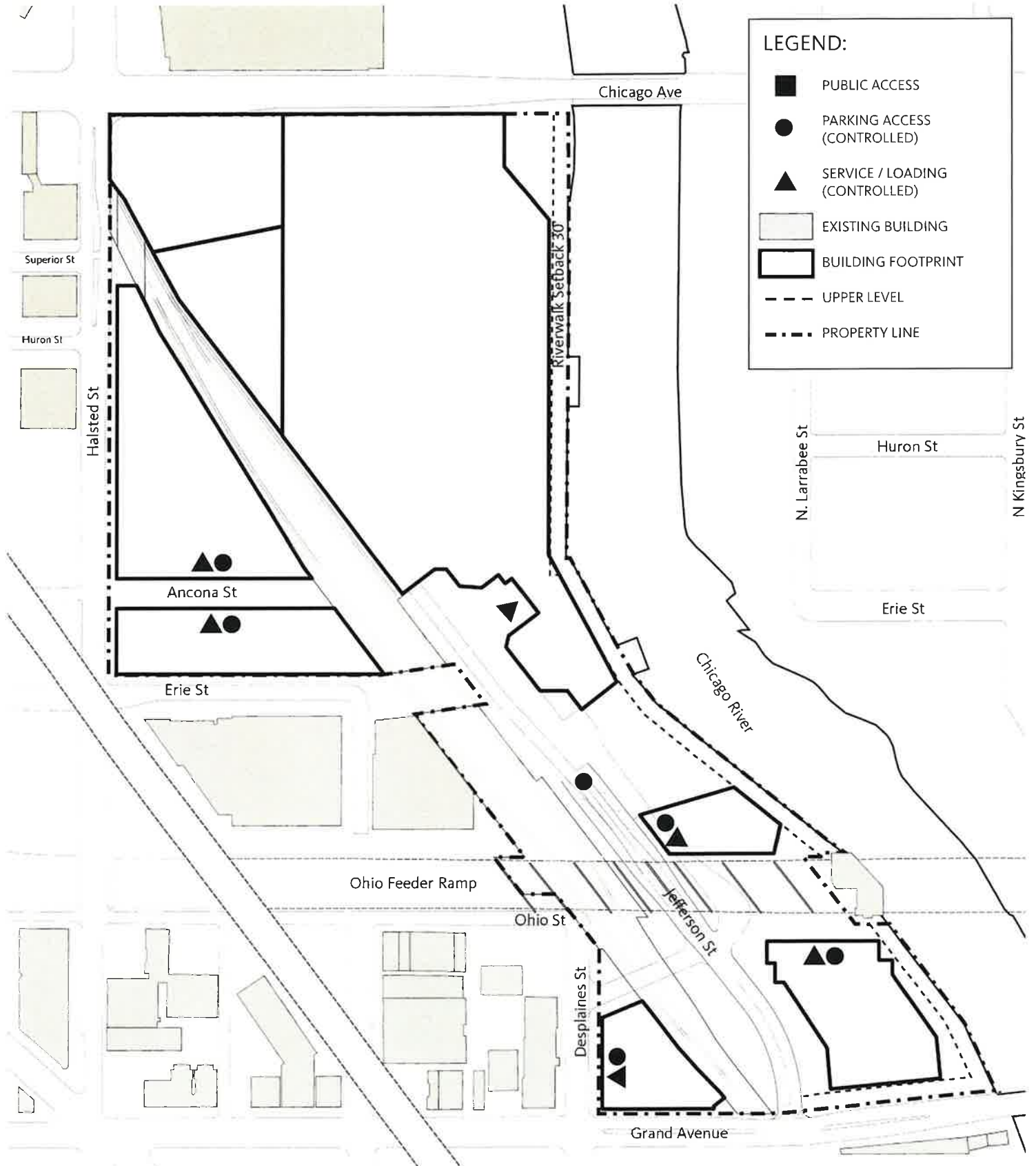
PHASE 1: EMPLOYEE PARKING LOT - NORTH

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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SCALE: 1" = 100'-0"





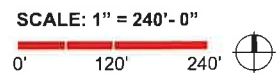
LEGEND:

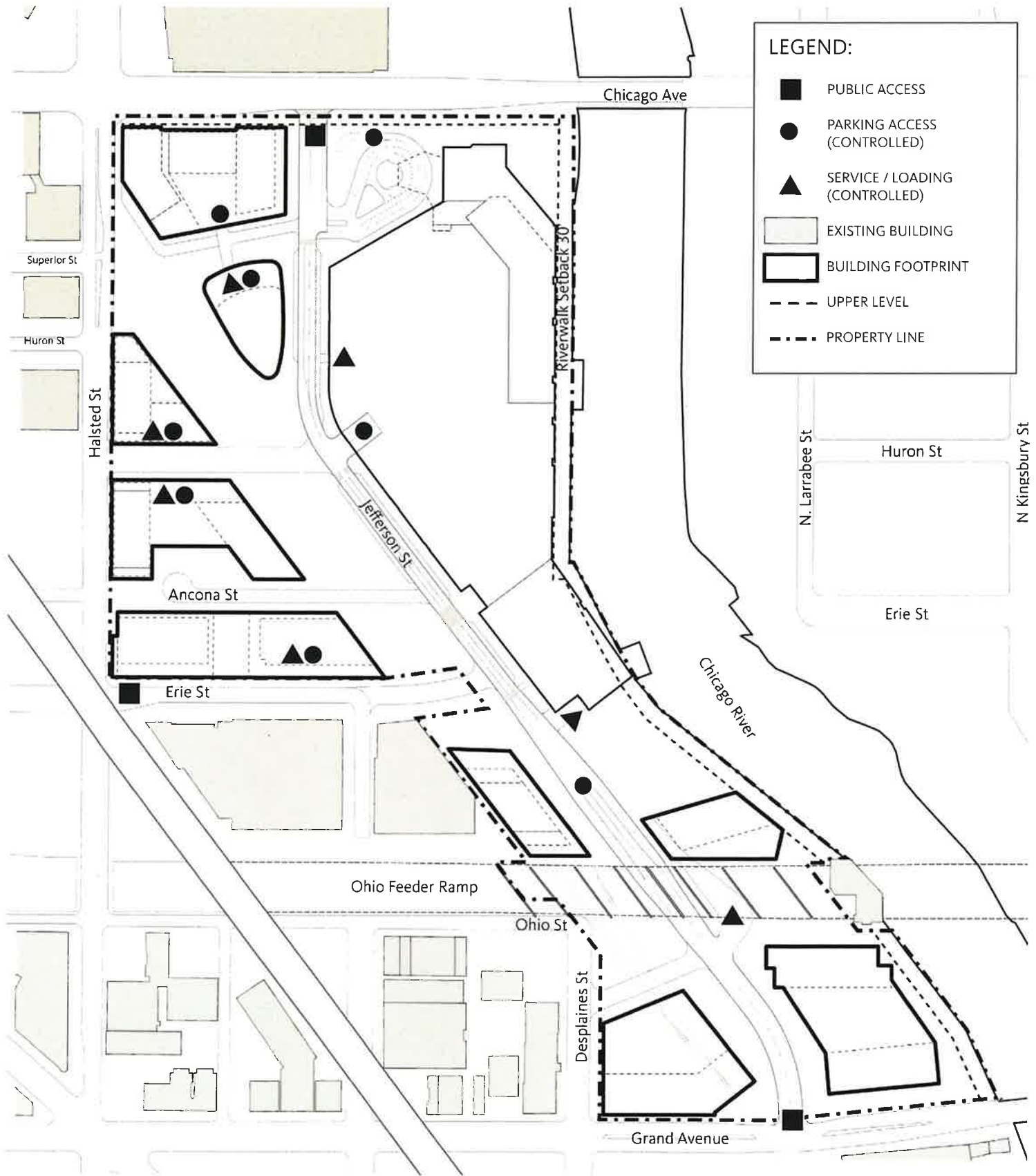
- PUBLIC ACCESS
- PARKING ACCESS (CONTROLLED)
- ▲ SERVICE / LOADING (CONTROLLED)
- ▭ EXISTING BUILDING
- ▭ BUILDING FOOTPRINT
- - - UPPER LEVEL
- · - · - PROPERTY LINE

**SITE ACCESS AND LOADING PLAN: LEVEL 01
(+0.0 +30.0 CCD)**

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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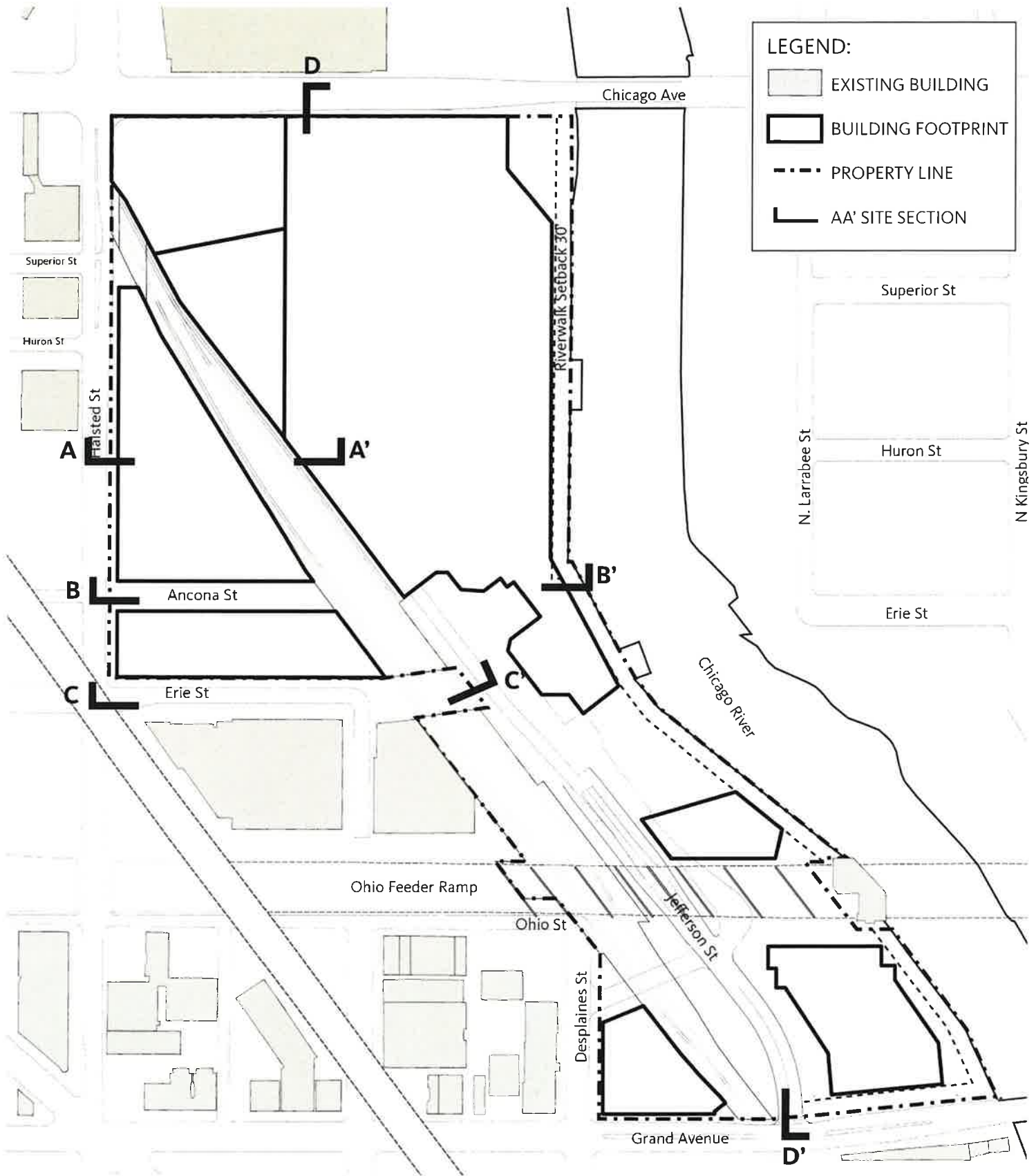
**SITE ACCESS AND LOADING PLAN: LEVEL 02
(+30.0 +45.0 CCD)**

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740
 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; &
 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
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SCALE: 1" = 240' - 0"





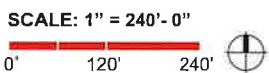
LEGEND:

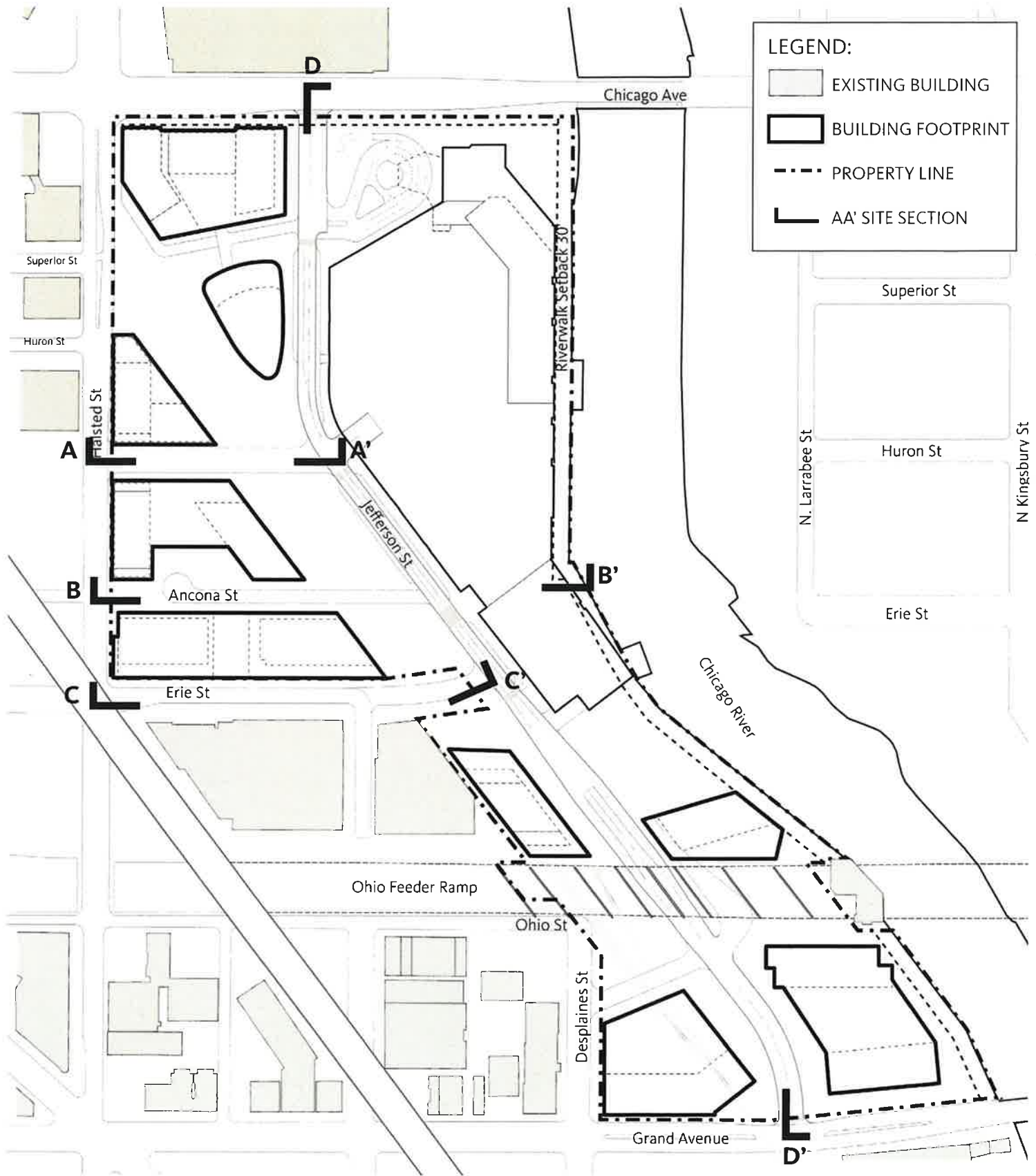
- EXISTING BUILDING
- BUILDING FOOTPRINT
- PROPERTY LINE
- AA' SITE SECTION

SITE LEVEL PLAN: LEVEL 01 (+0.0 TO +30.0 CCD)

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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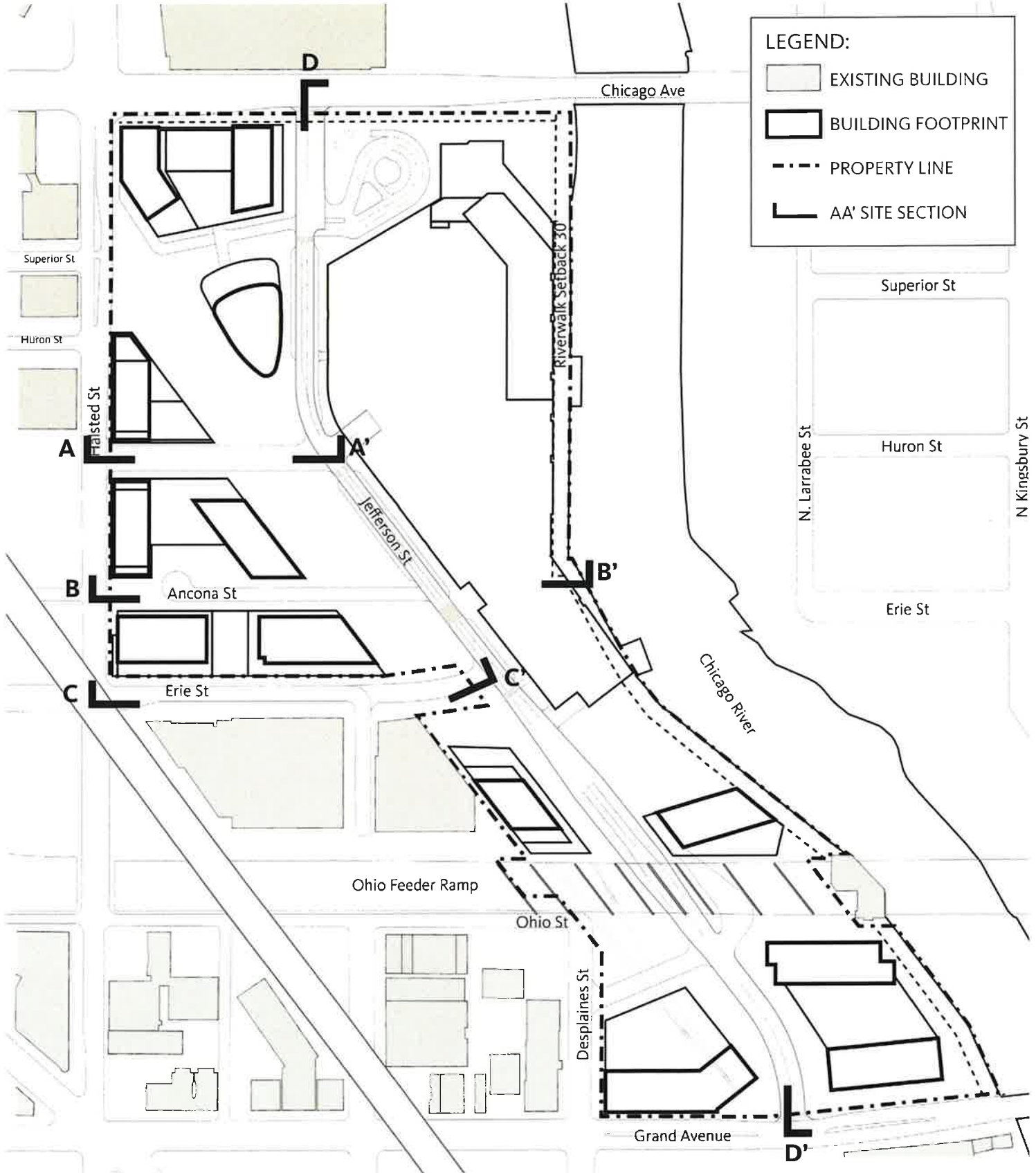
SITE LEVEL PLAN: LEVEL 02 (+38.0 TO +45.0 CCD)

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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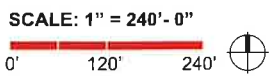


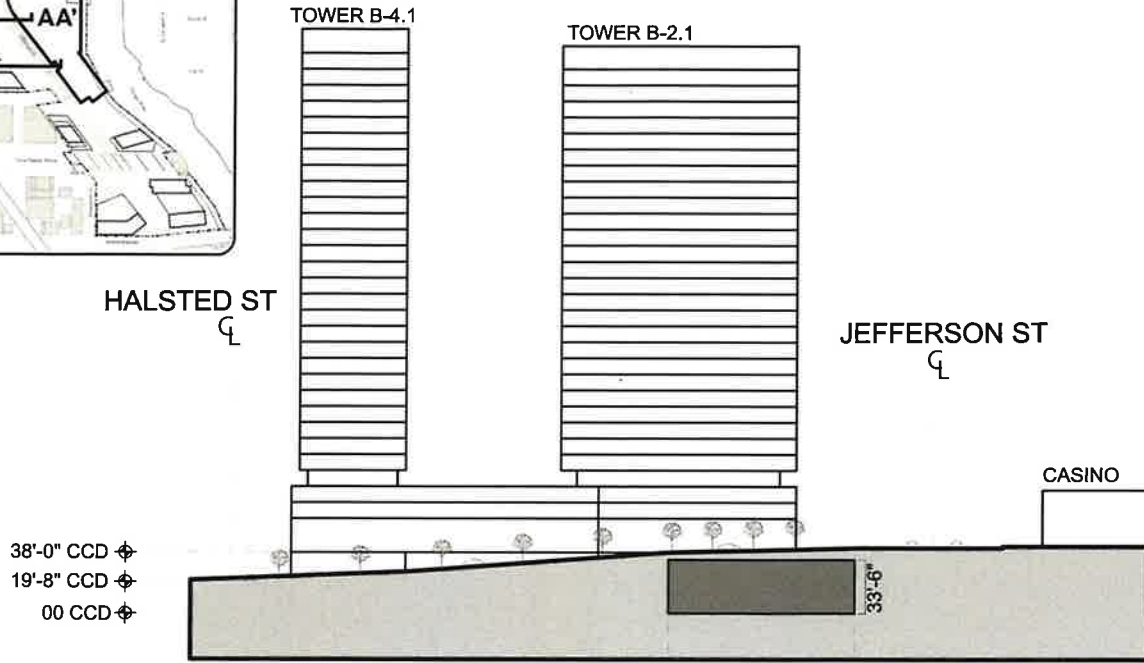
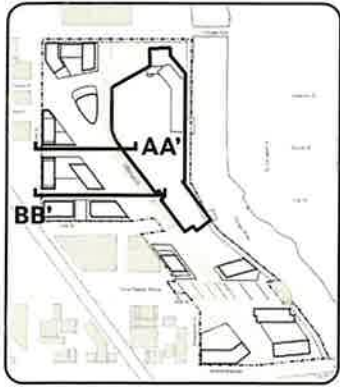


SITE LEVEL PLAN: LEVEL 03 (+45.0 CCD)

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

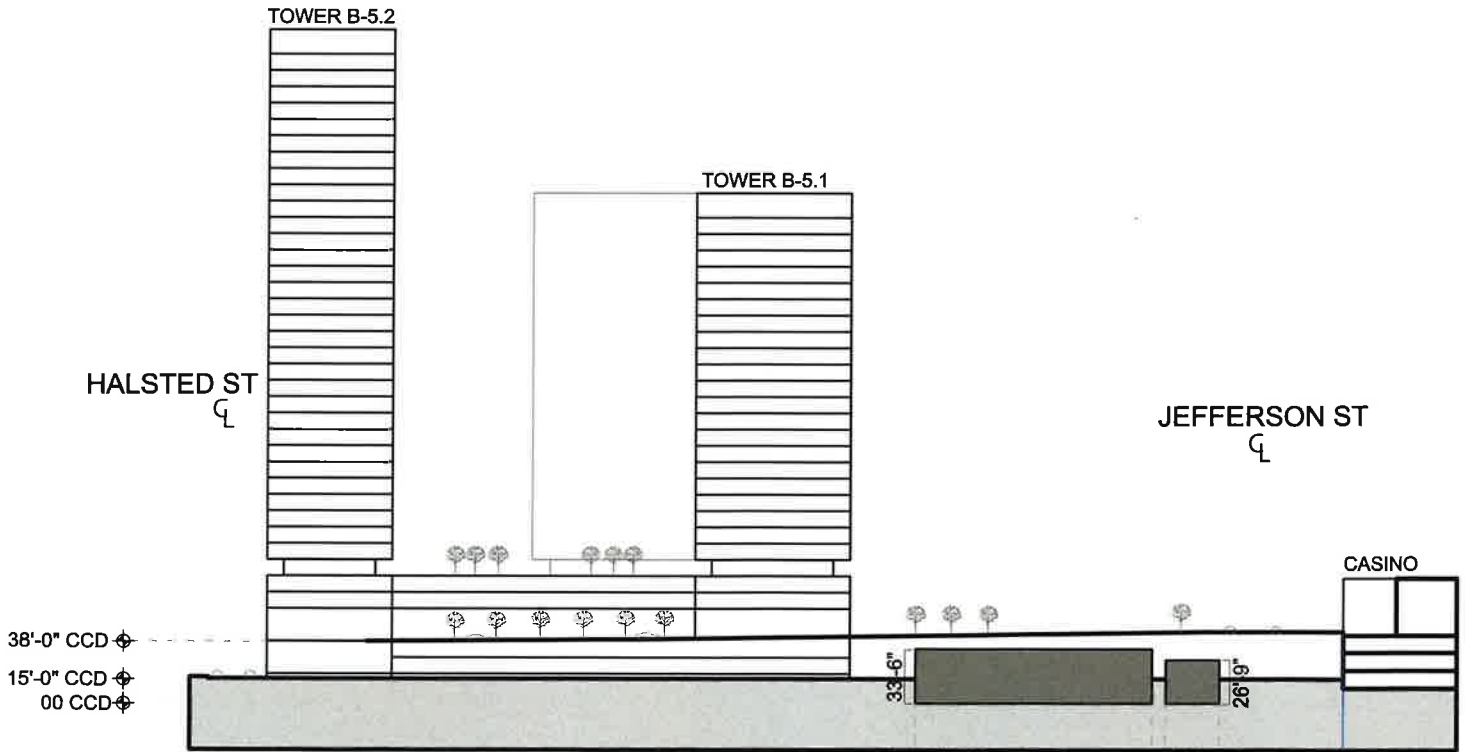
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AIR RIGHTS

SITE SECTION AA' THROUGH HURON ST LOOKING NORTH



AIR RIGHTS

AIR RIGHTS

SITE SECTION BB' THROUGH ANCONA ST LOOKING NORTH

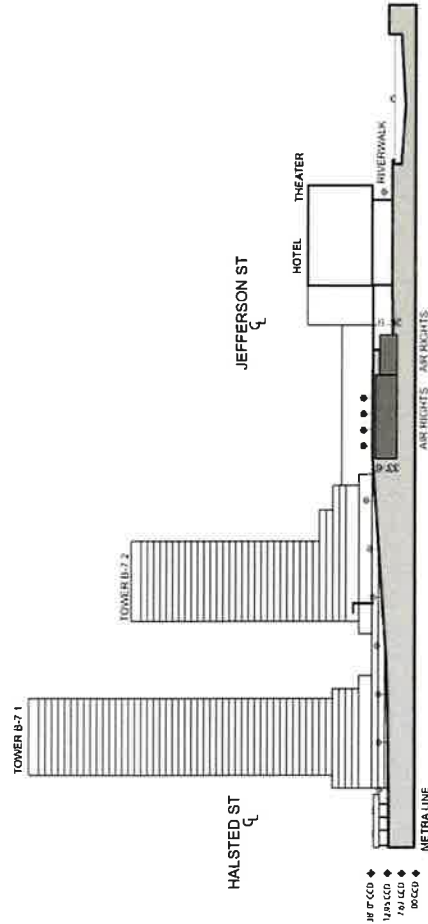
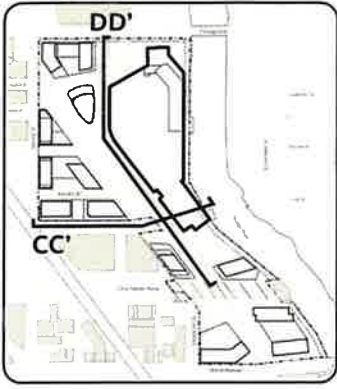
SITE SECTIONS (1 OF 2)

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

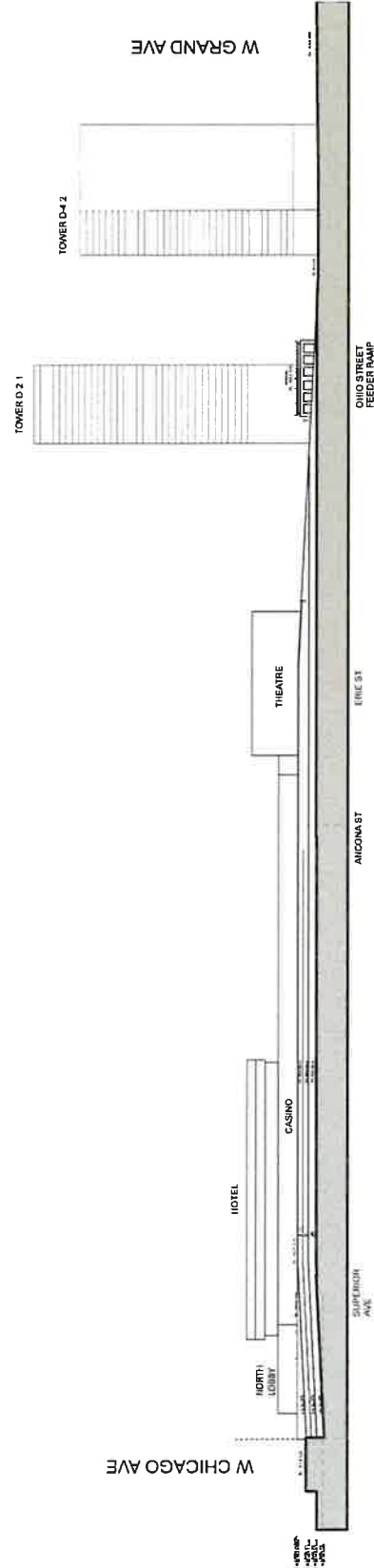
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SCALE: 1" = 120'-0"
 0' 60' 120'





SITE SECTION CC' THROUGH ERIE ST LOOKING NORTH



SITE SECTION DD' THROUGH JEFFERSON ST LOOKING EAST

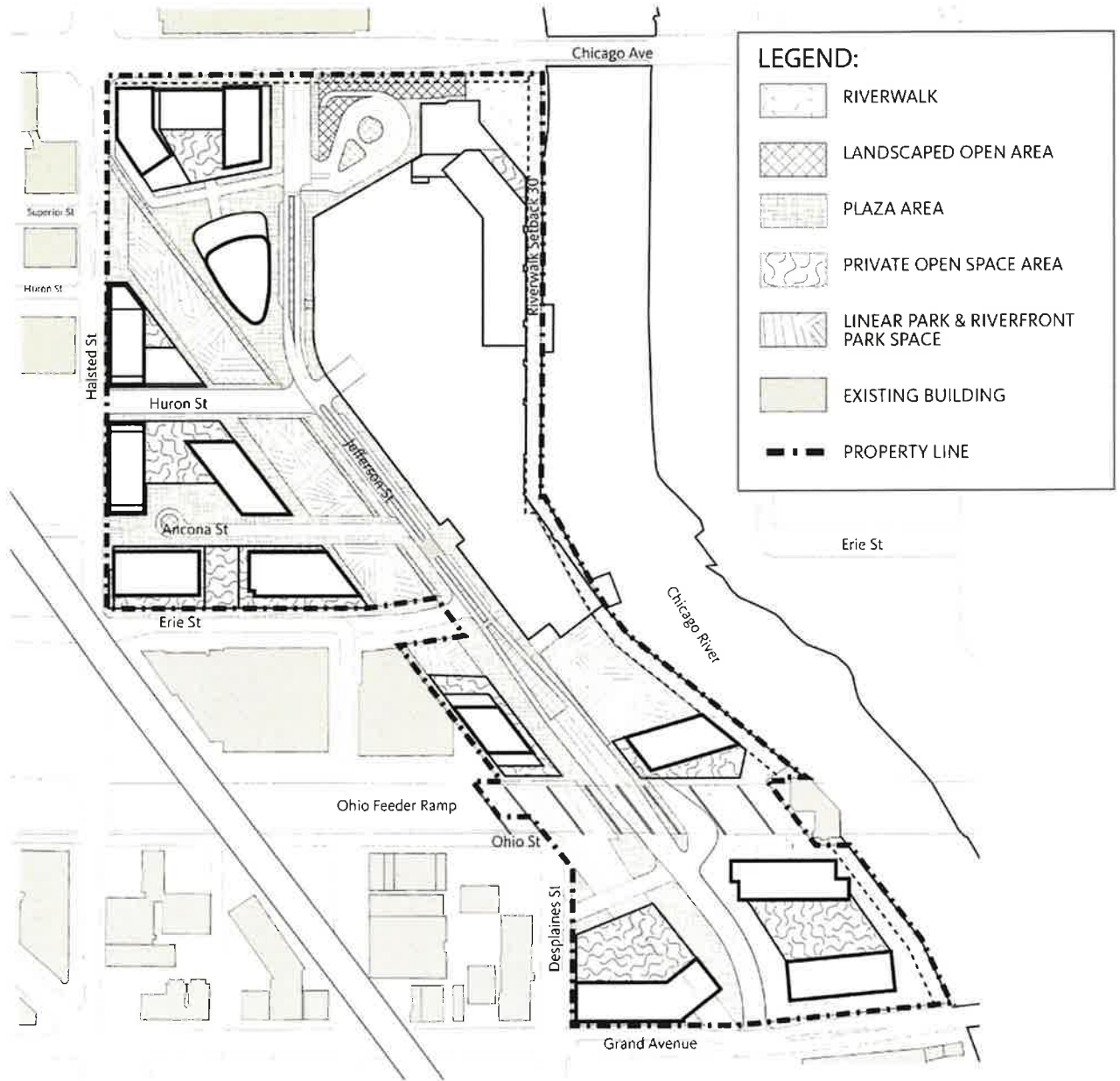
SITE SECTIONS (2 OF 2)

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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SCALE: 1" = 400'-0"
 0' 200' 400'



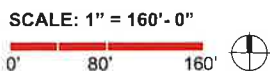


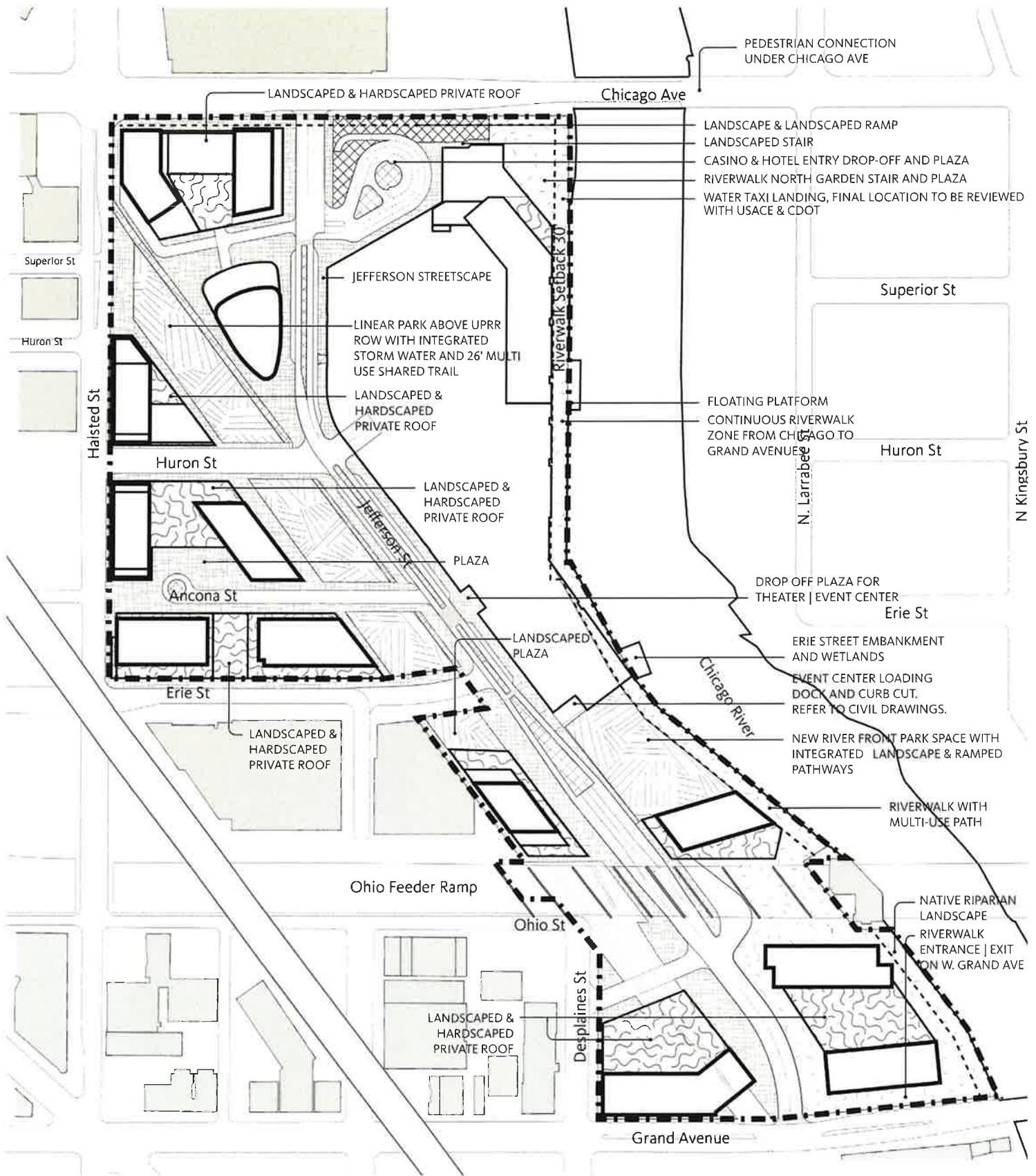
OPEN SPACE CALCULATIONS	GSF	ACRES	NOTES
Publicly Accessible Open Space			
Riverwalk (include natural areas and landscaped stormwater features)	146,666	3.37	24% of Public Open Space
Landscaped Open Areas (includes natural areas and landscaped stormwater features)	78,631	1.81	13% of Public Open Space
Plaza Area/ Elevated Plaza (includes natural areas and landscaped stormwater features)	264,292	6.07	43% of Public Open Space
Subtotal: Publicly Accessible Open Space	489,589	11.91	41% of Total Site Area
Subtotal: Private and Common Open Space	124,919	2.87	10% of Total Site Area
TOTAL OPEN SPACE (Public & Private Space)	614,508	14.11	51% of Total Site Area

PUBLIC SPACE OPEN PLAN

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **January 17, 2024**



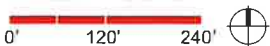


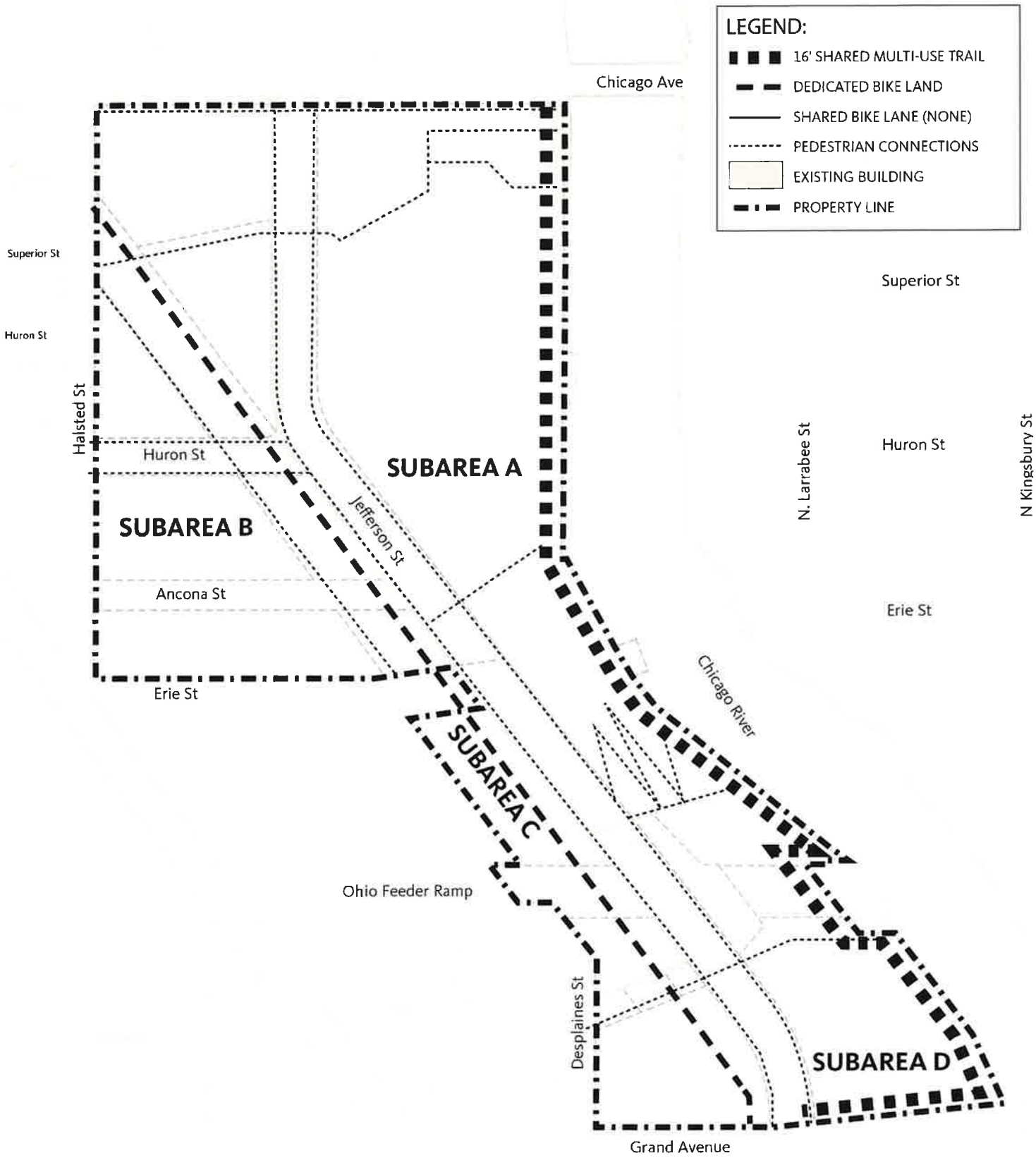
GENERALIZED LANDSCAPE PLAN

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **January 17, 2024**

SCALE: 1" = 240'-0"

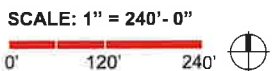


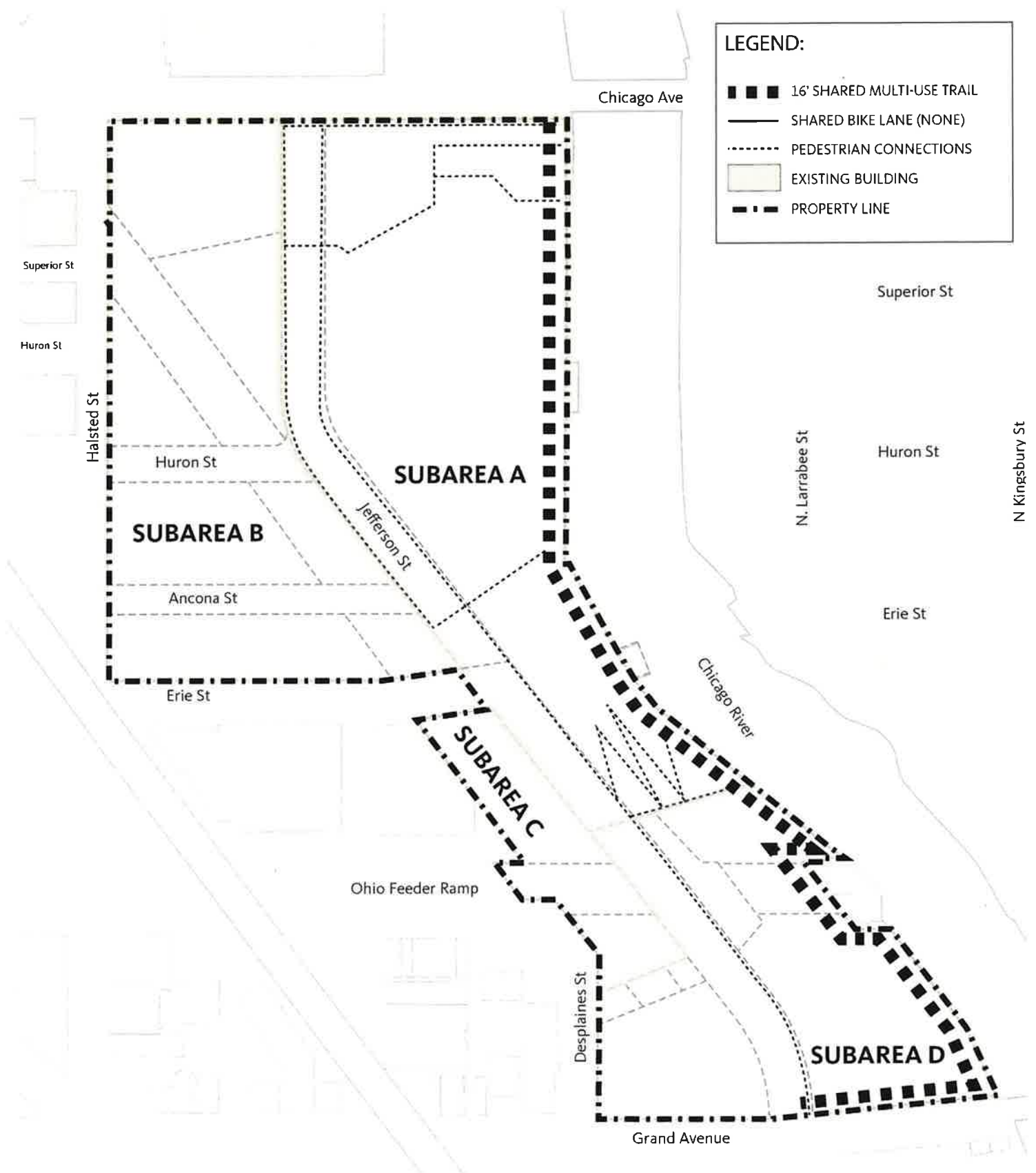


**BIKE AND PEDESTRIAN CONNECTIVITY PLAN
- ULTIMATE BUILD OUT**

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

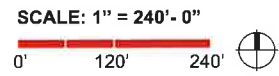
Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **January 17, 2024**





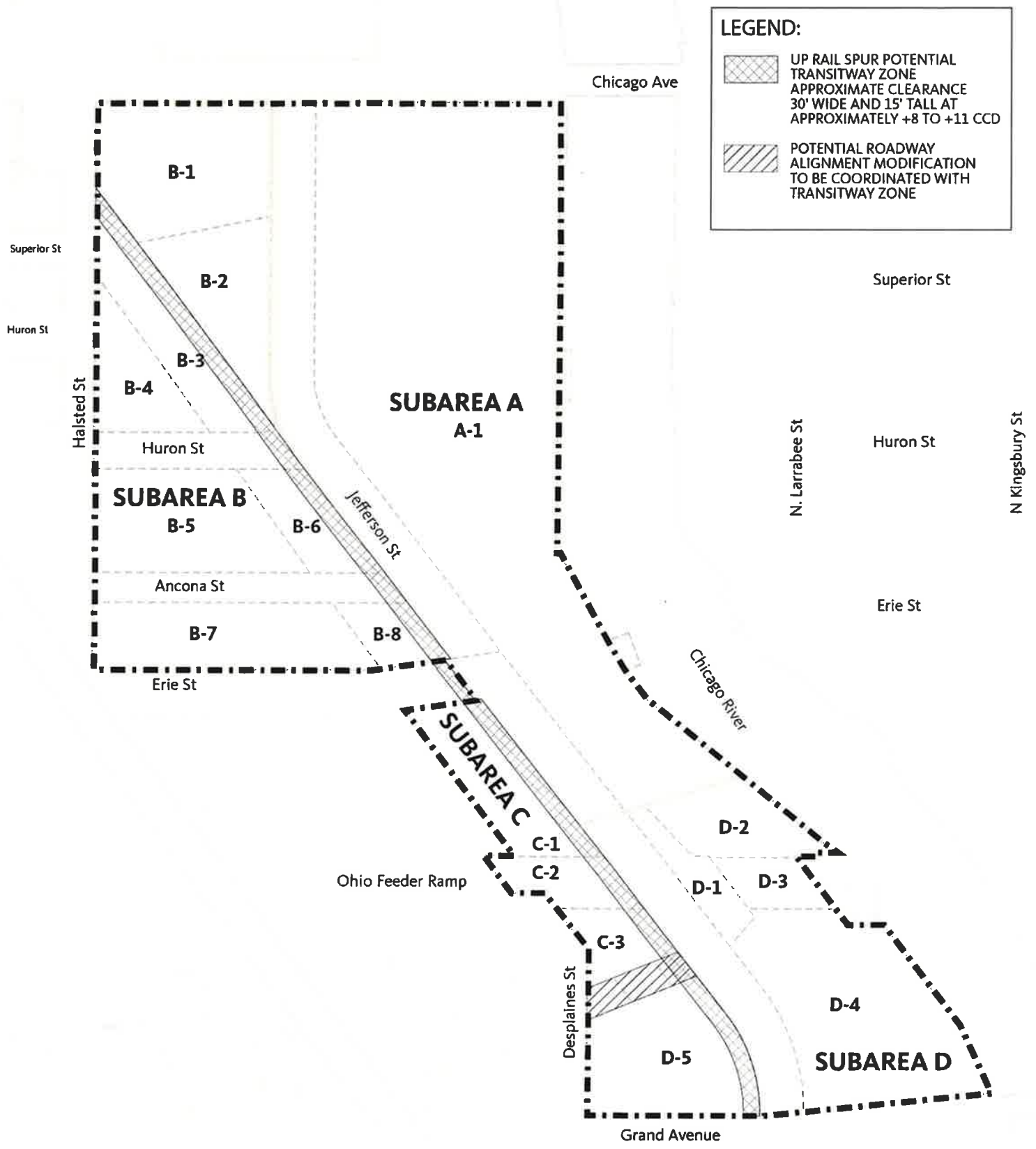
LEGEND:

- ■ ■ 16' SHARED MULTI-USE TRAIL
- SHARED BIKE LANE (NONE)
- ⋯ PEDESTRIAN CONNECTIONS
- EXISTING BUILDING
- - - PROPERTY LINE



PHASE 1: BIKE AND PEDESTRIAN CONNECTIVITY PLAN

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.
 Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
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LEGEND:

- UP RAIL SPUR POTENTIAL TRANSITWAY ZONE APPROXIMATE CLEARANCE 30' WIDE AND 15' TALL AT APPROXIMATELY +8 TO +11 CCD
- POTENTIAL ROADWAY ALIGNMENT MODIFICATION TO BE COORDINATED WITH TRANSITWAY ZONE

TRANSITWAY ACCOMMODATION PLAN

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **January 17, 2024**

SCALE: 1" = 240'-0"

Gensler site

All development within this planned development must substantially comply with the Design Guidelines of the North Branch Framework adopted by the Chicago Plan Commission on May 18, 2017, or as amended, the Chicago River Corridor Design Guidelines and Standards, or as amended, and with the design standards and guidelines outlined in the Zoning Ordinance, Section 17-8-0900 Standards and Guidelines. The guidelines listed below provide additional standards for buildings and public spaces to complement the specific context of this planned development:

GENERAL DEVELOPMENT GUIDELINES

- Building siting and base design shall define the street walls along Jefferson Street, Chicago Avenue, and Halsted Street, with setbacks allowed for plazas and open spaces.
- Jefferson Street shall be developed as a central street with wide sidewalks, street trees and landscaping to promote stormwater management.
- Active ground floor uses shall be integral to buildings along the west side of Jefferson Street, Chicago Avenue, and Halsted Street, and shall include active uses such as retail, lobbies, or office space.
- Active ground-floor uses shall have direct access to publicly-accessible open spaces and the riverwalk where feasible.
- Buildings at the corner of Halsted Street and Chicago Avenue shall incorporate massing, siting, and façade designs to express and reinforce a district gateway.
- In general, vehicular pick-up and drop-off shall be oriented away from Chicago Avenue and Grand Avenue. Drop off zones on Halsted to be coordinated with CDOT and CTA. Parking and service entrances for the casino and other buildings along Jefferson Street will be provided via access drives or driveways. Pick-up and drop-off locations within the public way may be incorporated only where curbside space exists for that use.
- Curb-cuts should not exceed 20' in width and parking entries should be integrated with the building's architecture through the use of similar materials, patterns, textures, and color. Curb cuts shall be coordinated with CDOT and may be widened within CDOT's standards if required to provide adequate access for service, loading, and shuttle services.
- To preserve access to light, a minimum separation of 40' between towers is required. Where site constraints permit, towers will be oriented to maximize passive cooling, natural lighting, and energy efficiency.
- Undeveloped parcels may support interim uses including, but no limited to recreational open space, dog friendly areas, and surface parking. Interim use plan improvements and time frame will require review and approval by DPD.

BUILDING ENVELOPE DESIGN GUIDELINES

- Building designs shall achieve a varied and distinctive skyline.
- Mechanical penthouses and any upper level parking floors shall be screened with high-quality architectural elements consistent with the overall building facade.
- Parking levels shall be screened adequately to obscure car headlights and sound from neighboring buildings.
- Podium and tower designs shall relate to each other to provide a cohesive expression.
- Podium roofs will incorporate active amenity decks and landscaping as appropriate to building uses.
- Balconies and any required ventilation shall be integrated within the design of the building façade.
- Tenant signage for each building should be considered as part of the façade design to ensure consistency of placement, size, materials, and method of illumination.

MATERIAL GUIDELINES FOR BUILDINGS

- Buildings will not be clad with low quality materials and materials with low aesthetic value such as Exterior Insulation and Finish System (EIFS), exterior exposed Concrete Masonry Units (CMU), thin brick, or residential-type vinyl and metal siding.
- Buildings will employ architectural materials consistent with contemporary building practices, such as high quality wall systems in glass, metal, masonry, high-quality architectural concrete, or hardwood. Glazing shall not be highly reflective or mirrored and be bird friendly design.
- Building envelopes will support environmentally responsible design by reducing heat loads, improving energy efficiency, maximizing occupant comfort, and using sustainable materials.
- Podium and ground-floor level materials and design facing the streets, publicly accessible open spaces, and the riverwalk will be detailed to enhance the pedestrian environment and will be complimentary to the context.
- Ground-floor facades will have a high percentage of clear / un-tinted glazing along the primary street frontages.

LANDSCAPE GENERAL GUIDELINES

- A riverfront publicly accessible park will connect directly to the riverwalk and include gathering places, a lawn, passive play spaces, native landscaping, opportunities for direct viewing of river, and stormwater infiltration.
- Landscaped stairs and ramps will connect the riverwalk and Jefferson Street, and provide view corridors from Halsted Street to the river. Programming will include a variety of uses such as seating, play areas, a location for public art, and a dog park.
- The space under the Ohio Feeder Ramp will be activated with recreational uses that connect to the riverwalk park spaces.
- Publically accessible open spaces will be designed to the applicable standards of Chicago Park District Parks.
- The riverwalk will connect to the north side of Chicago Avenue using space provided by CDOT under the new Chicago Avenue Bridge.
- The riverwalk will connect to the street level intersection of Grand Avenue and Jefferson Street, and not preclude space for future Riverwalk connection under Grand Avenue bridge.
- Landscaping throughout will incorporate stormwater management best practices to detain, clean, and reduce the volume of stormwater discharge.
- Landscaped areas will enhance local habitat for wildlife through use native plantings.
- In key locations, floating wetlands can be located along the riverwalk where feasible.

DESIGN GUIDELINES

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **January 17, 2024**



ROOF AREAS

Theater Roof	39,349 SF
Casino Roof	218,129 SF
Hotel Roof	35,183 SF
Total Roof Area	292,661 SF

	ROOF AREA USED FOR MEP	50,495 SF
	GREEN ROOF AREA	97,397 SF
	40.2% OF LEFT OVER AREA	97,397 SF

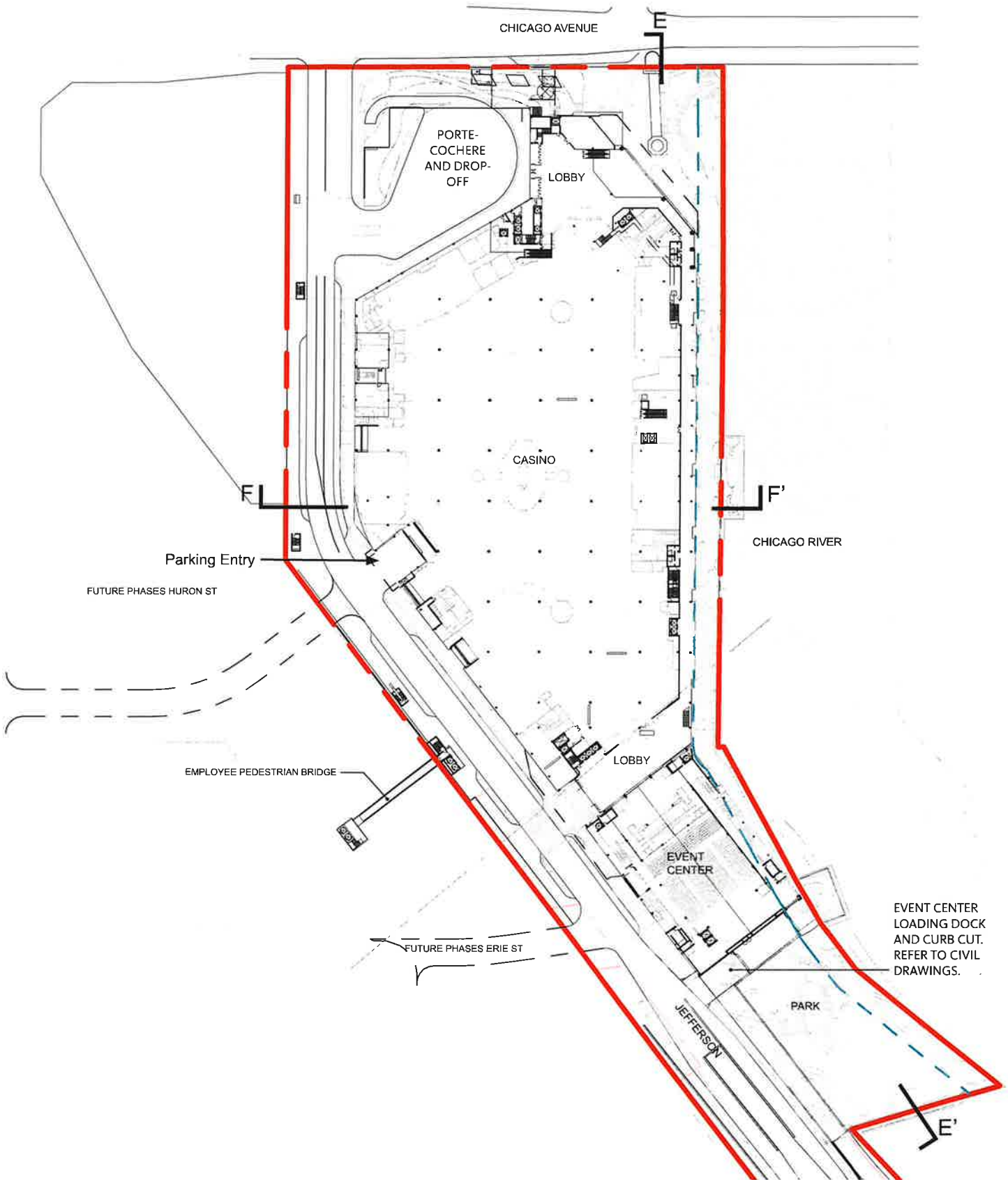
PHASE 1: GREEN ROOF PLAN

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **January 17, 2024**

SCALE: 1" = 160'- 0"





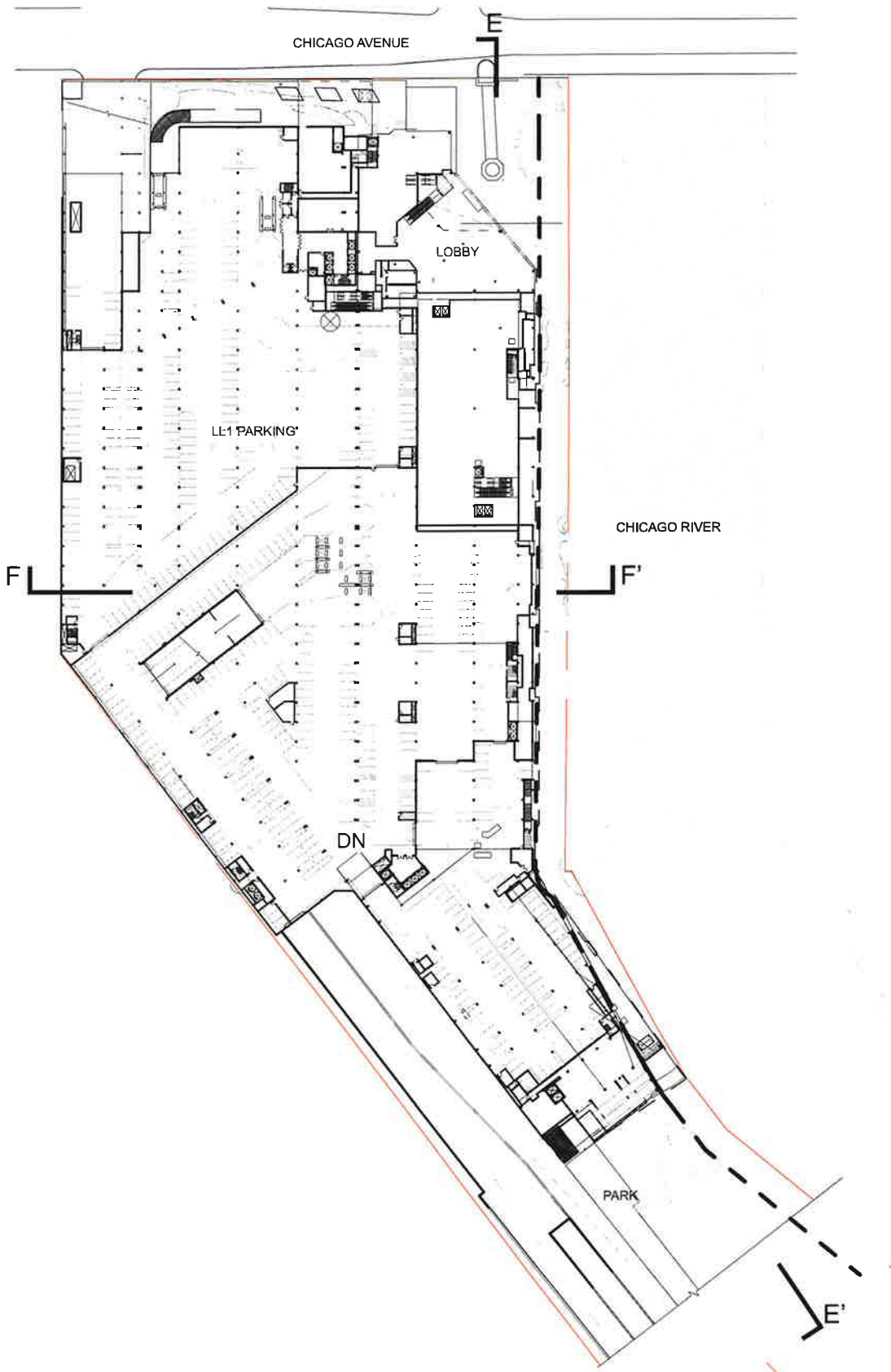
PHASE 1: FLOOR PLAN - L1

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **January 17, 2024**

SCALE: 1" = 160'-0"





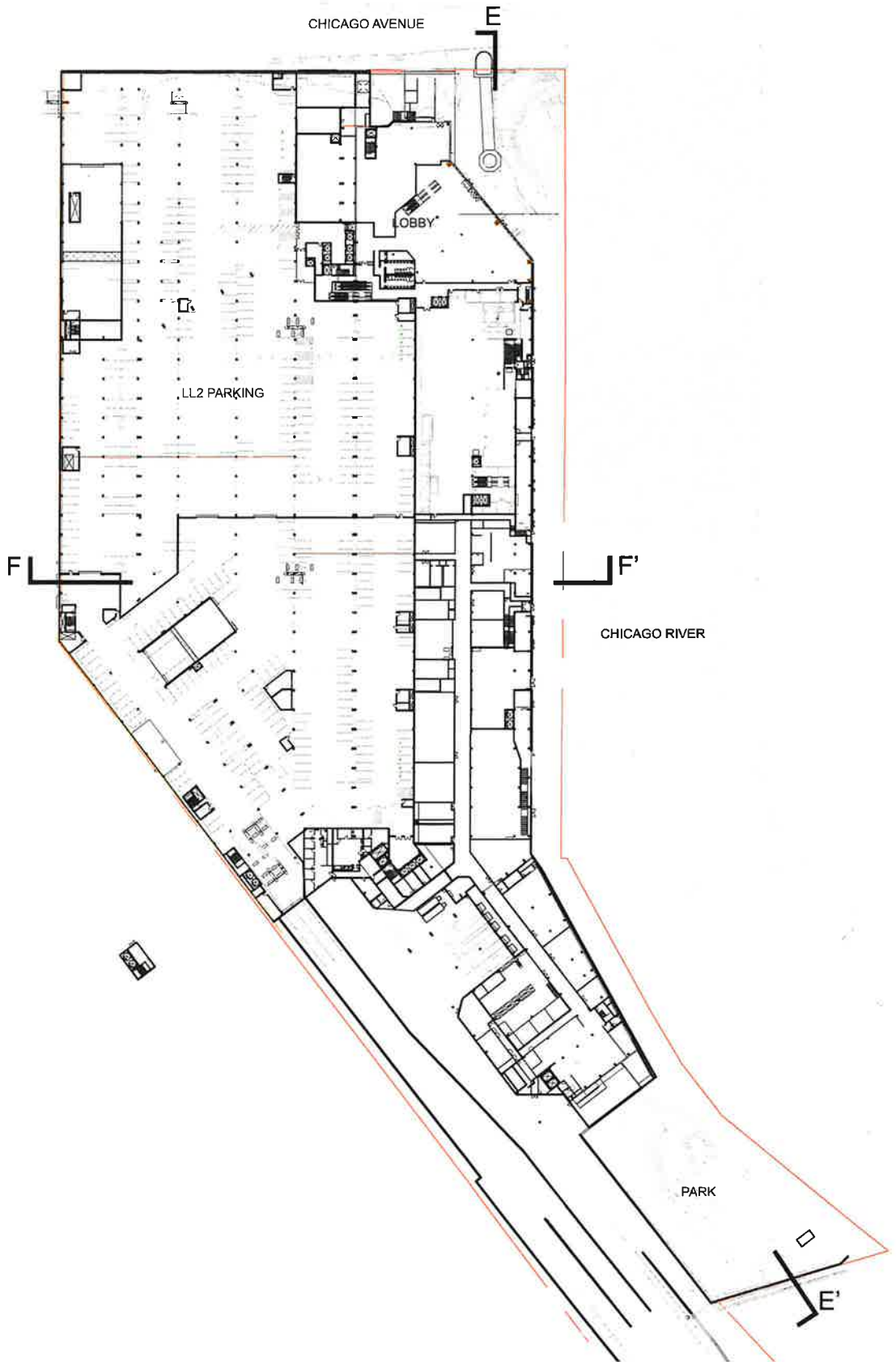
PHASE 1: FLOOR PLAN - LL1

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **January 17, 2024**

SCALE: 1" = 160'-0"
 0' 80' 160'

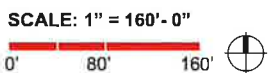


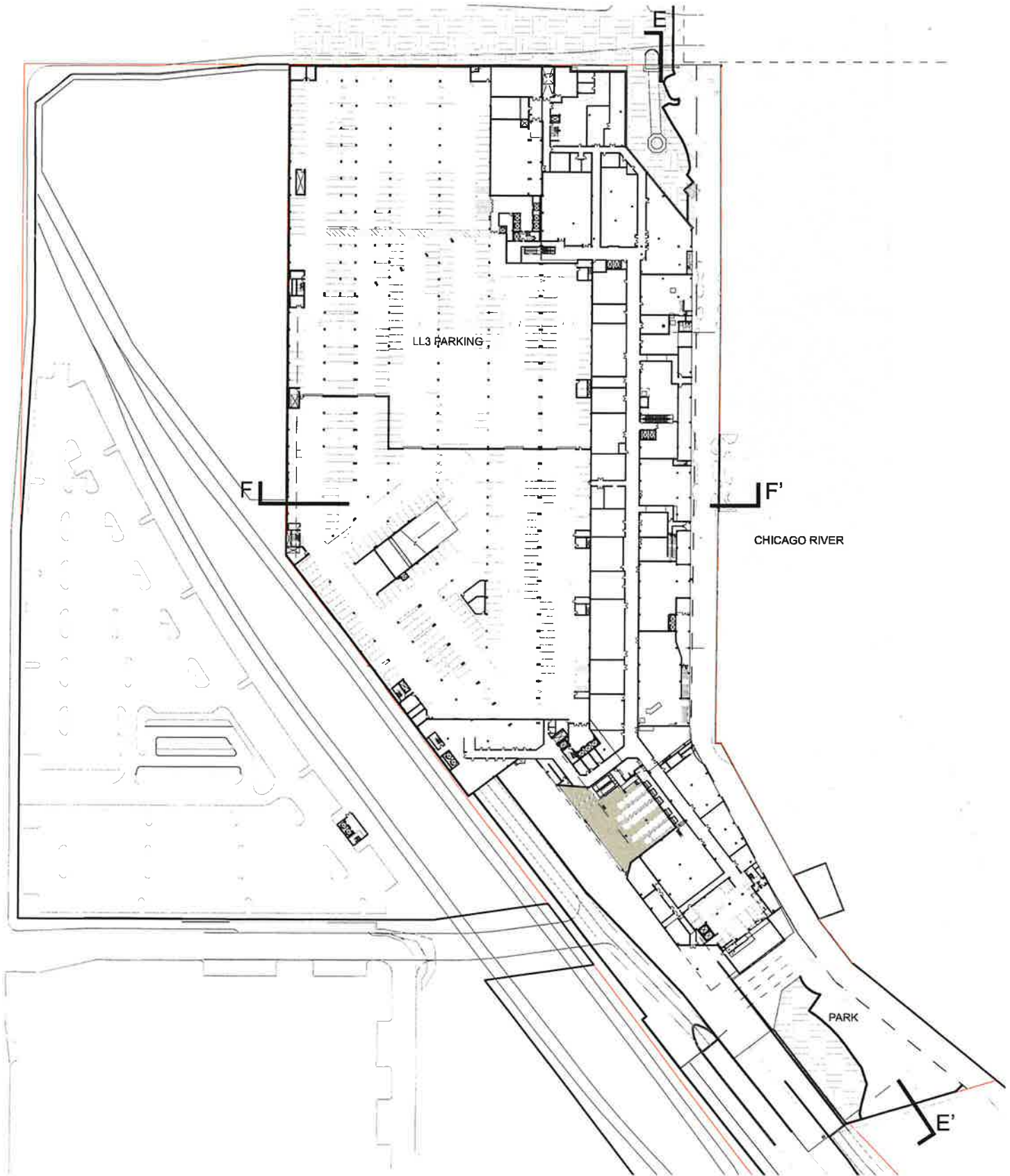


PHASE 1: FLOOR PLAN - LL2

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **January 17, 2024**





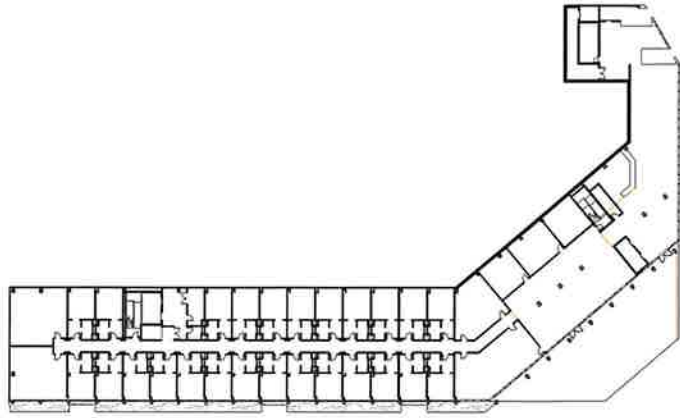
PHASE 1: FLOOR PLAN - LL3

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

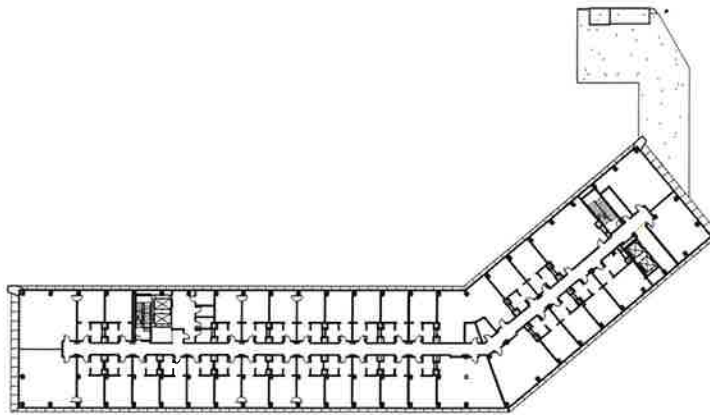
Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **January 17, 2024**

SCALE: 1" = 160'-0"





HOTEL FLOOR PLAN 1

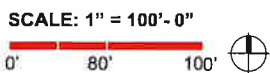


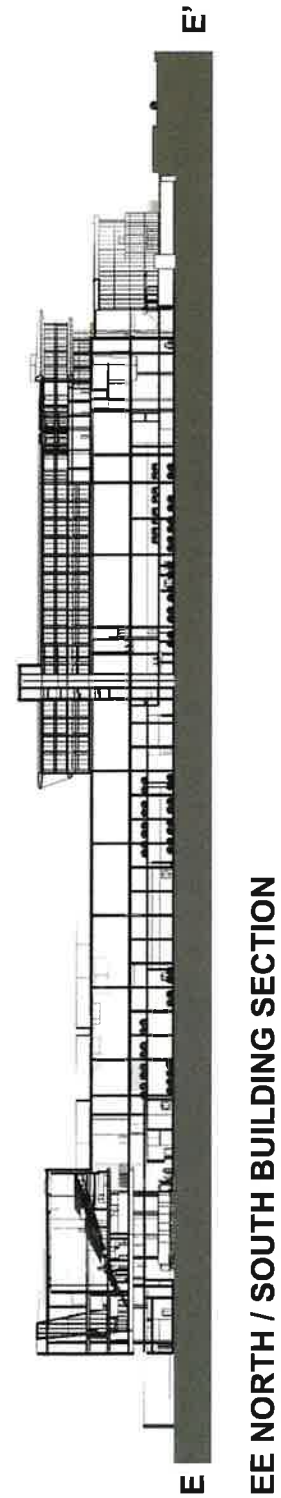
HOTEL FLOOR PLAN 2

TYPICAL HOTEL PLANS

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **January 17, 2024**

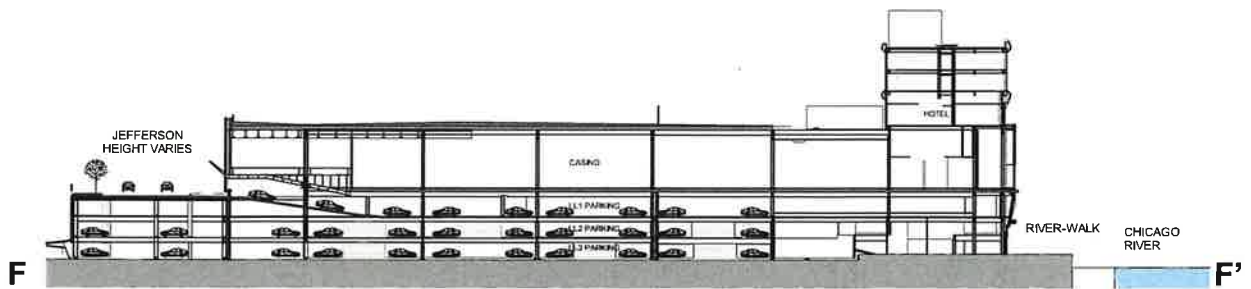
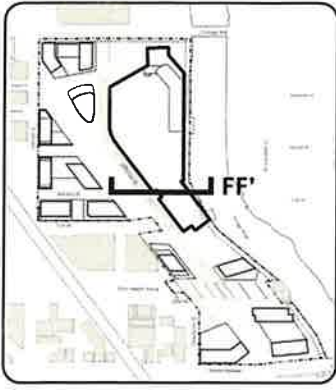




PHASE 1: BUILDING SECTION - 1 (OF 2)

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

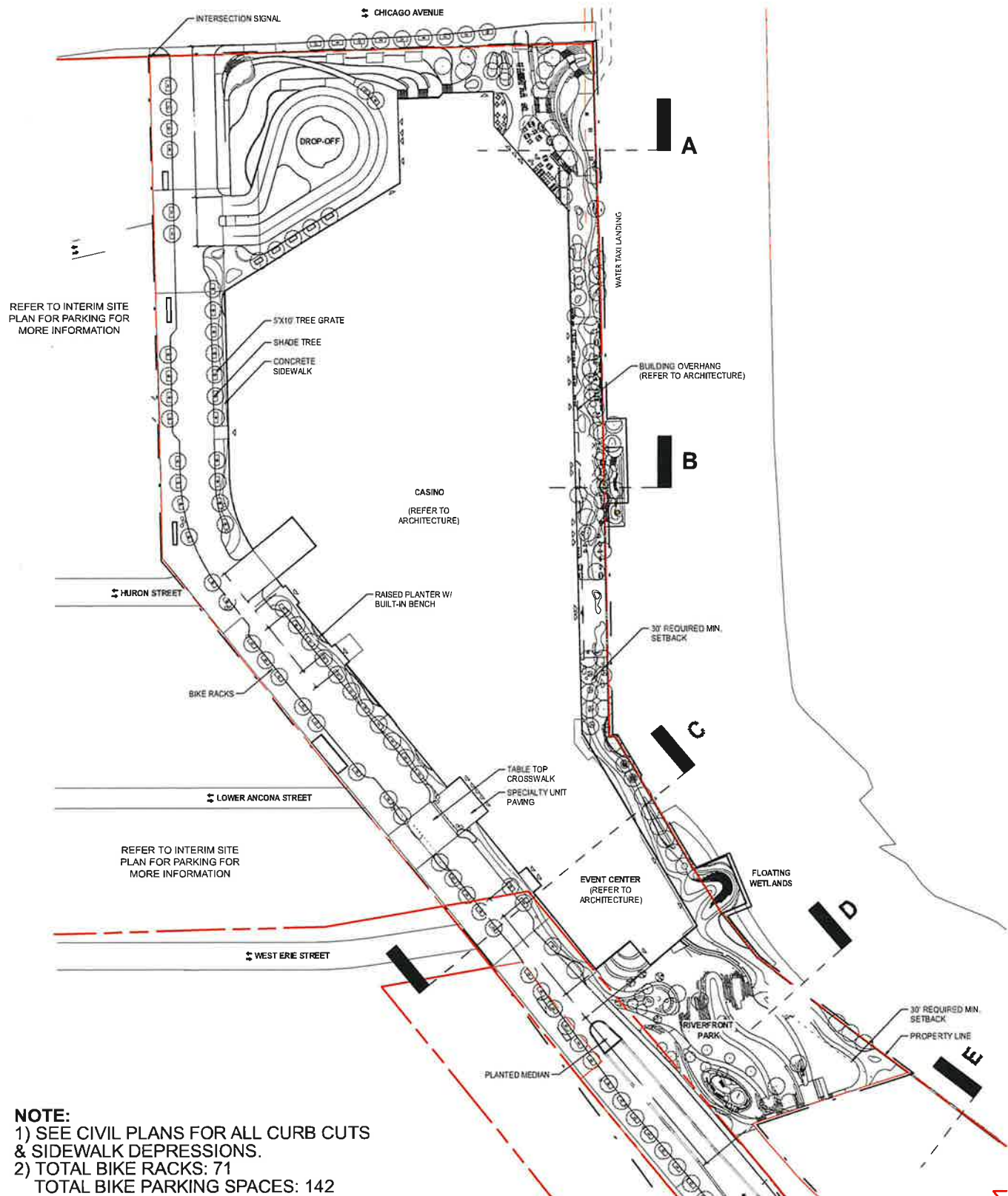
Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **January 17, 2024**



**PHASE 1: BUILDING SECTION - 2 (OF 2)
FF EAST / WEST BUILDING SECTION**

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740
 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; &
 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
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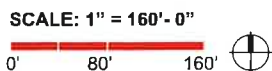


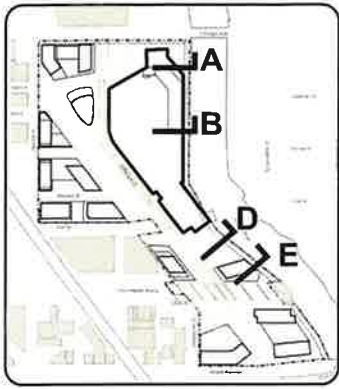
NOTE:
 1) SEE CIVIL PLANS FOR ALL CURB CUTS & SIDEWALK DEPRESSIONS.
 2) TOTAL BIKE RACKS: 71
 TOTAL BIKE PARKING SPACES: 142

PHASE 1: PROPOSED LANDSCAPE PLAN

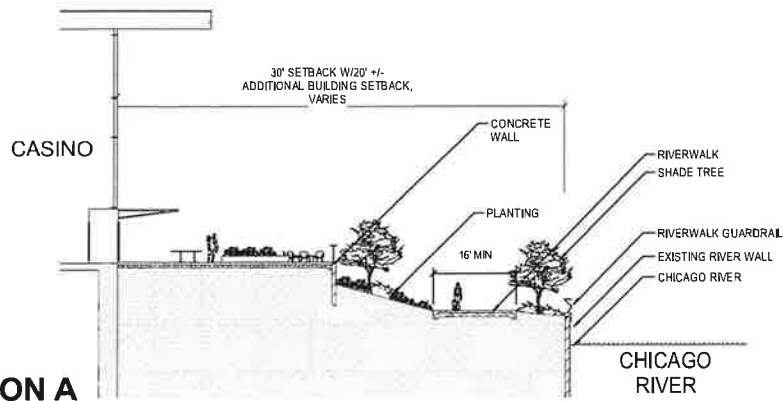
Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
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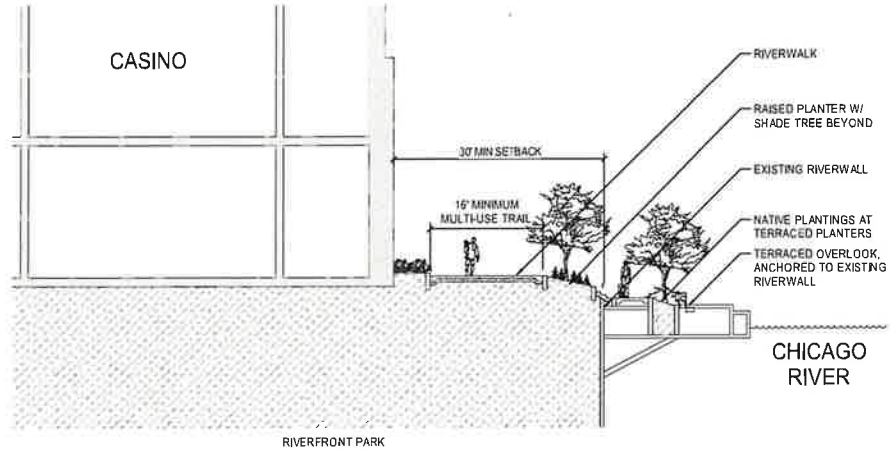




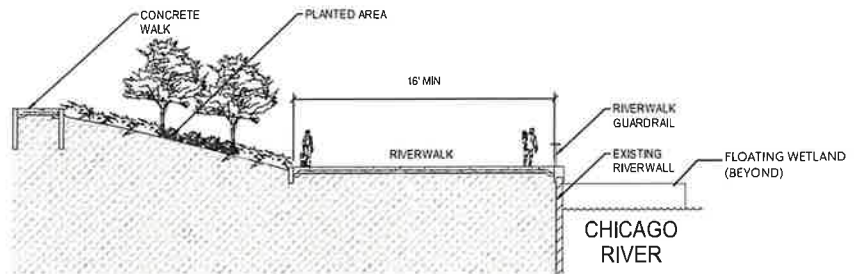
SECTION A



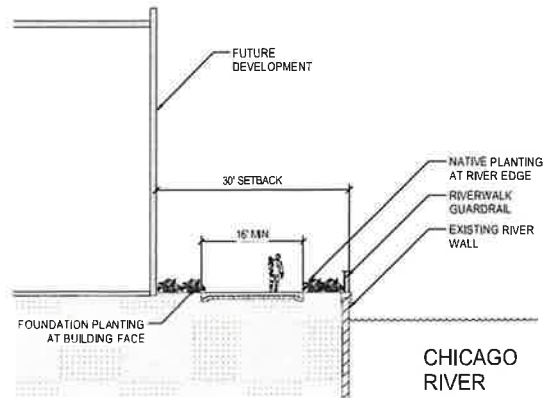
SECTION B



SECTION D



SECTION E



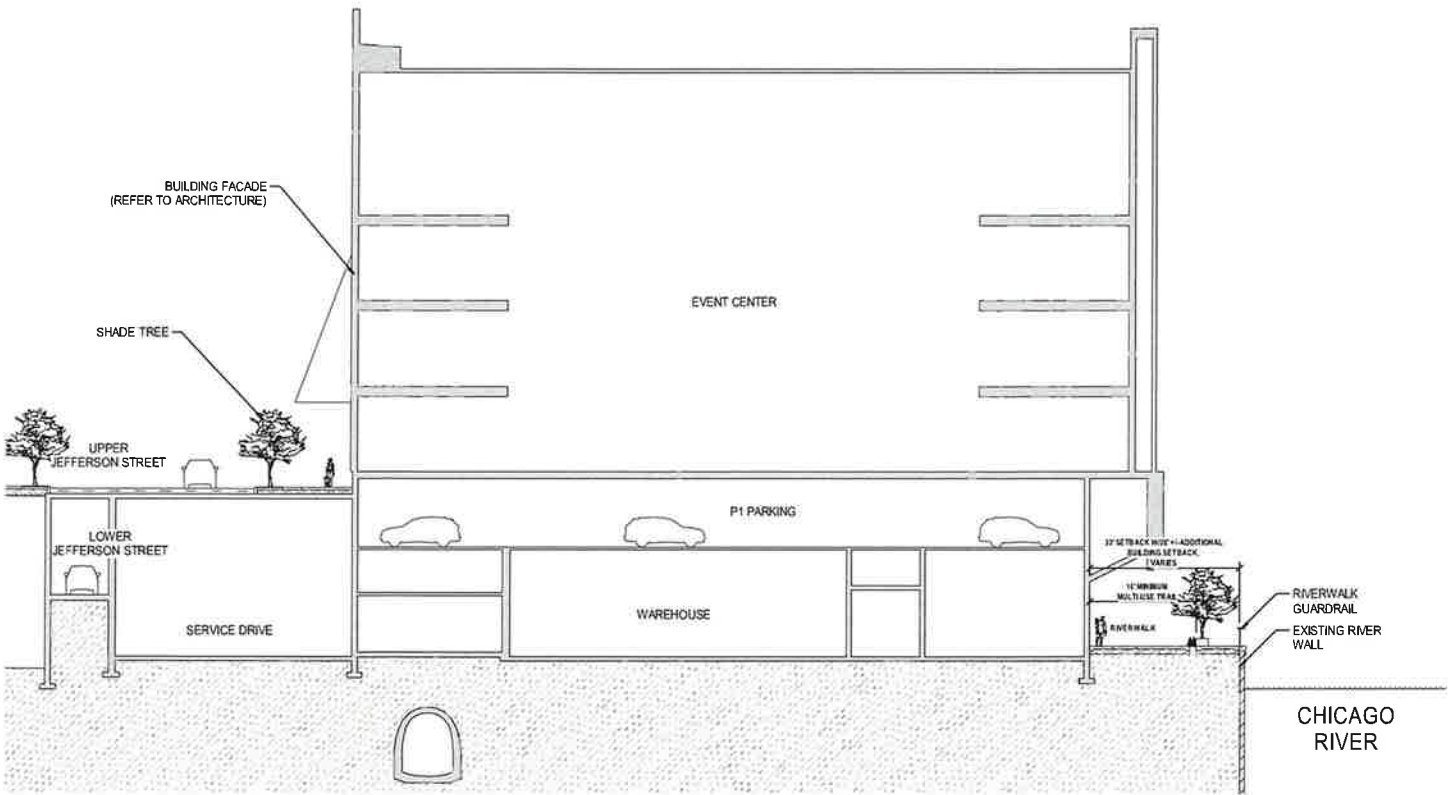
PHASE 1: RIVERWALK SITE SECTIONS

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **January 17, 2024**

SCALE: 1" = 20'- 0"
 0' 10' 20'





SECTION C

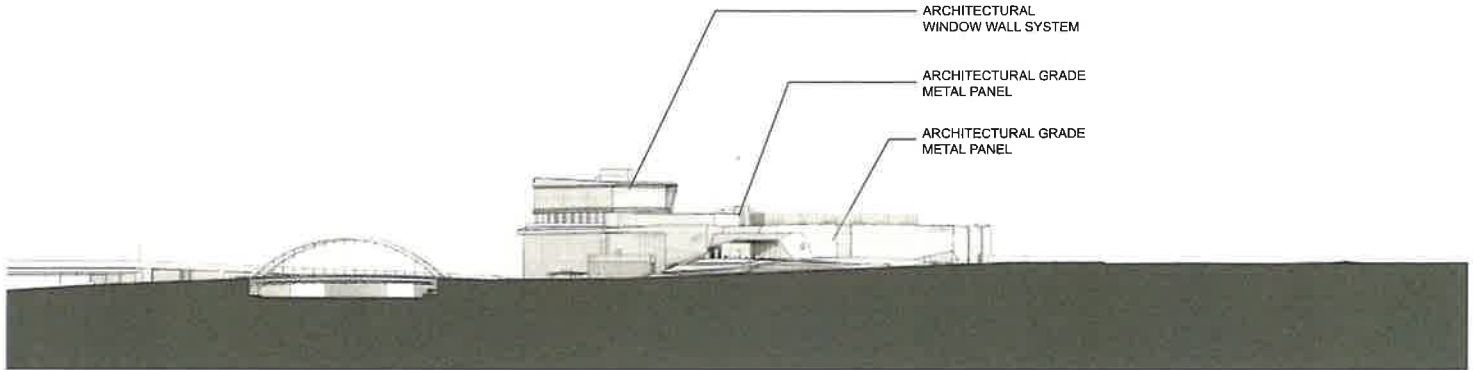
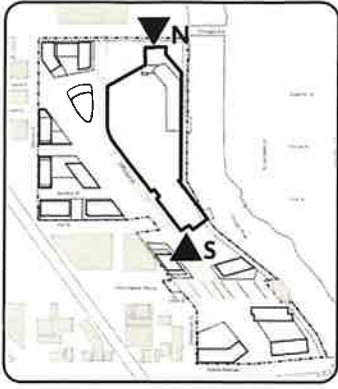
PHASE 1: RIVERWALK SITE SECTION C

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

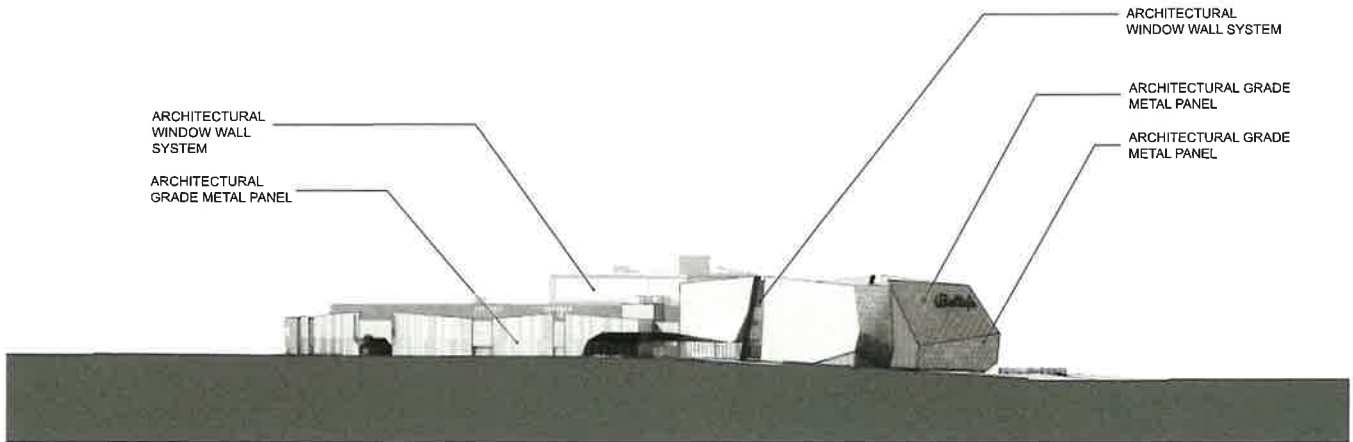
Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **January 17, 2024**

SCALE: 1" = 40'- 0"
 0' 20' 40'





NORTH ELEVATION



SOUTH ELEVATION

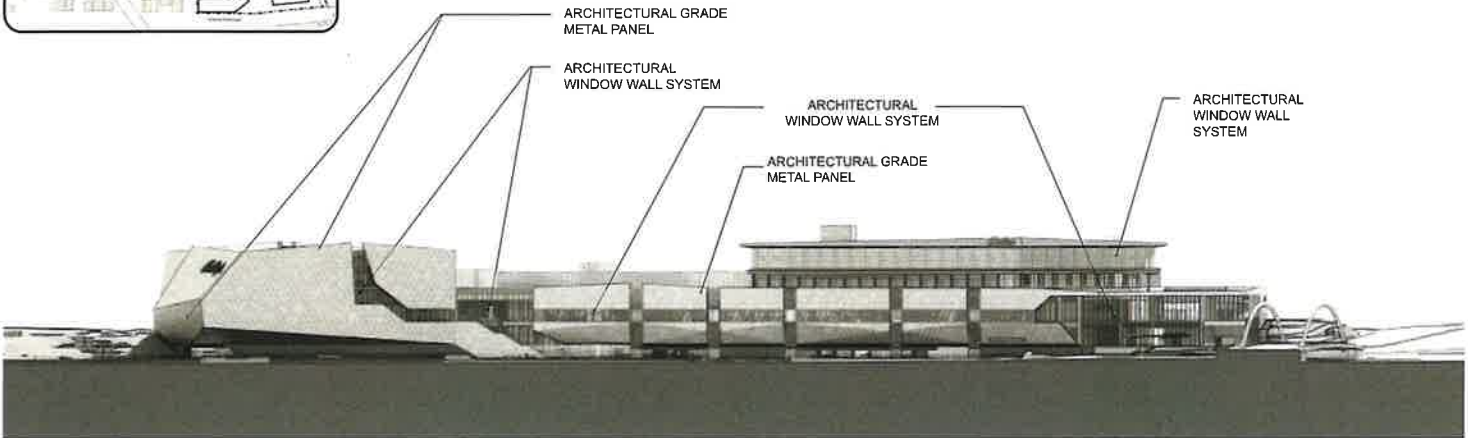
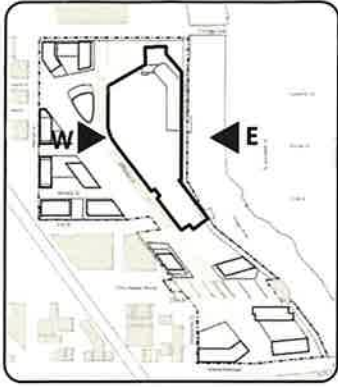
PHASE 1: BUILDING ELEVATIONS - NORTH / SOUTH

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE...; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

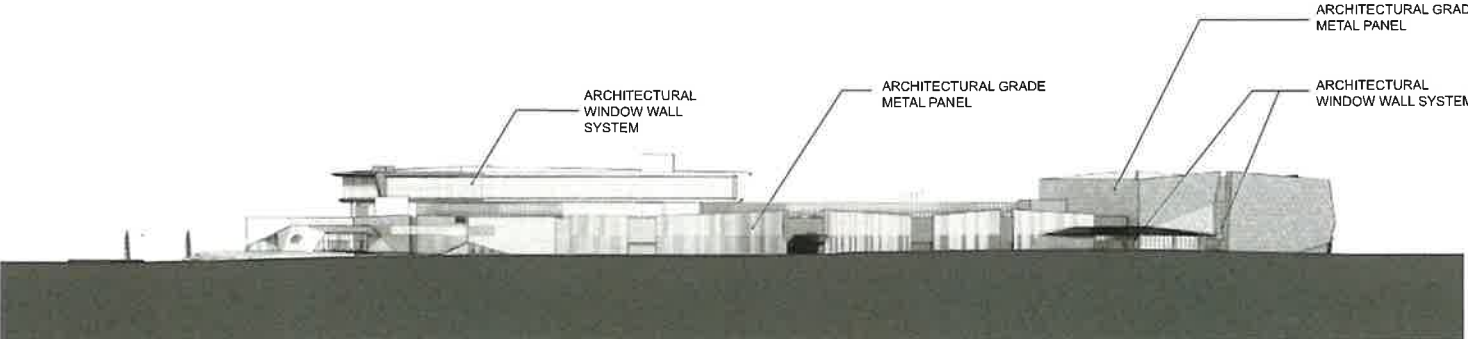
Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **January 17, 2024**

0' 125' 250'





EAST ELEVATION



WEST ELEVATION

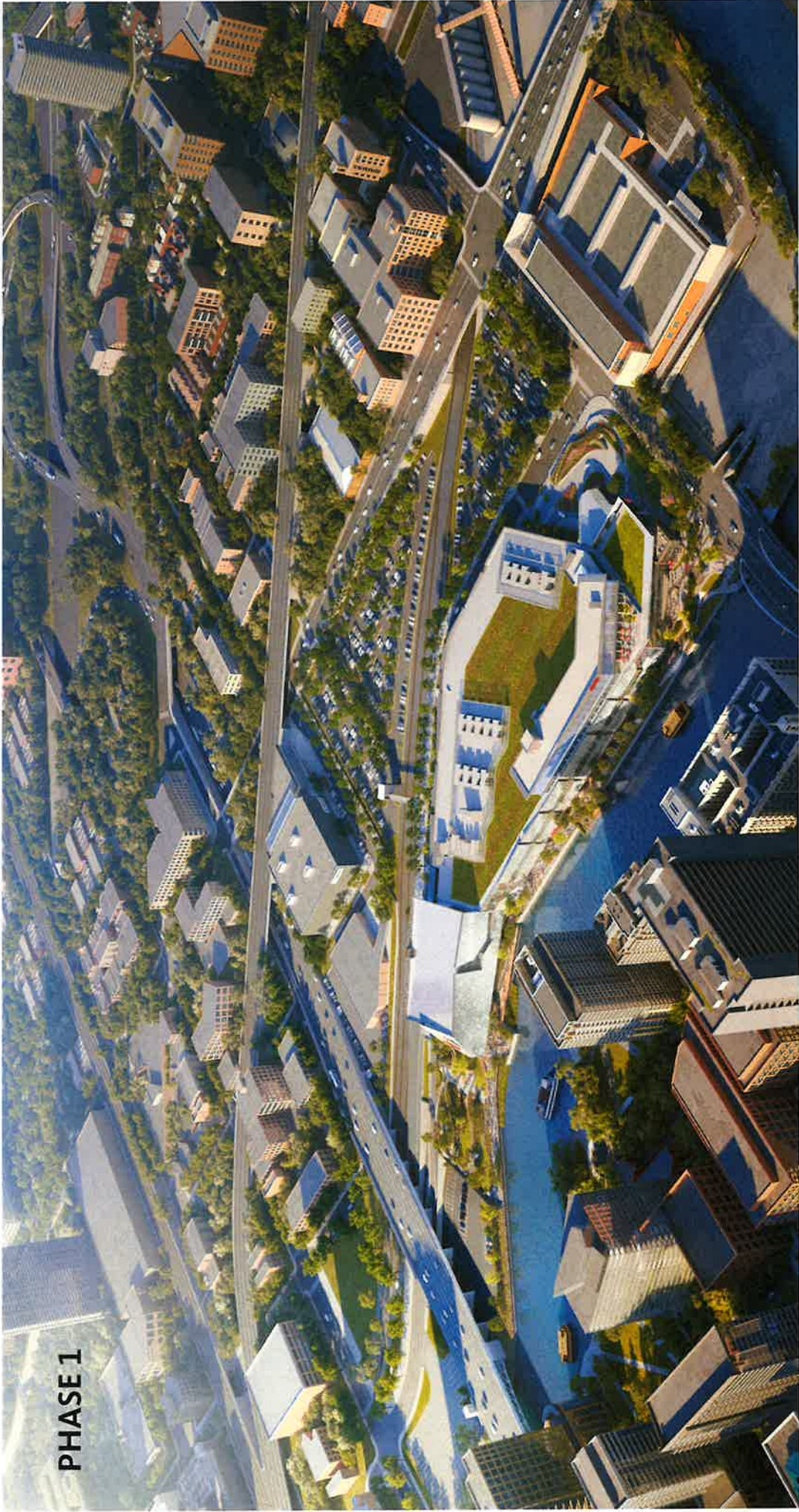
PHASE 1: BUILDING ELEVATIONS - EAST / WEST

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
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 Updated: **January 17, 2024**



PHASE 1



PHASE 2 (OVER YEARS)



BALLY'S LANDING



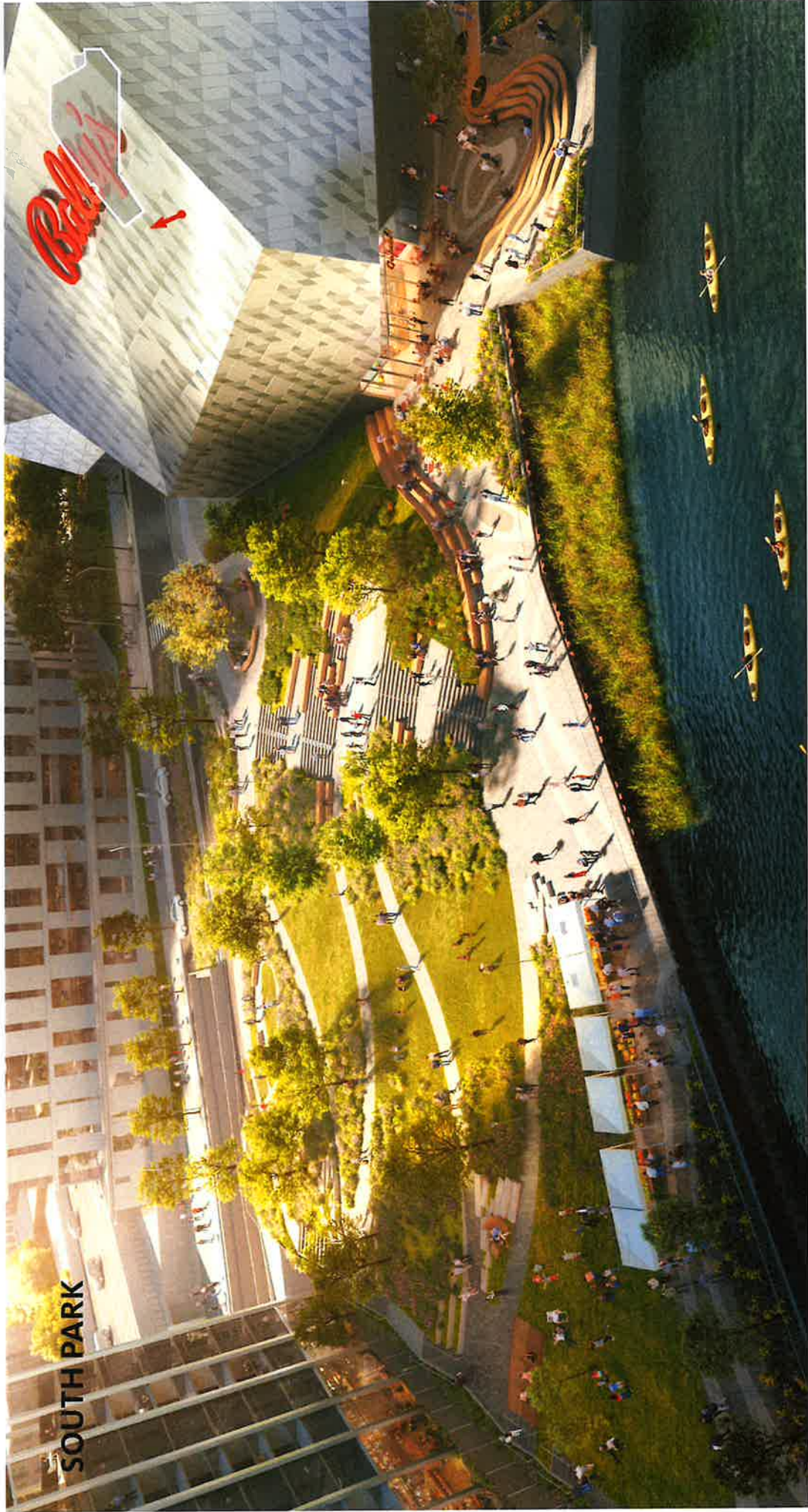


RIVERWALK

Cafe

CITY'S EDGE





SOUTH PARK

Bally's



ENTERTAINMENT HUB

DROP-OFF / PORTE COCHERE



BALLY'S WAY | DROP-OFF PLAZA



Dec 13, 2023 | JEFFERSON ST EXHIBIT

BALLY'S WAY | PARKING ENTRANCE





BALLY'S WAY | EVENT CENTER





BALLY'S WAY | DROPTOP PLAZA

6.0 IMPLEMENTATION

Updated: January 17, 2024

Project Information

Project Location

Project Address	703 W. Chicago Avenue	
Type of Project (land use)	Casino, Mixed Use and Entertainment District	
Size of Project	30 Acres	
Is this a river dependent or critical service use?	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No
Character Zone	<input type="checkbox"/> Northwest Zone <input checked="" type="checkbox"/> North Branch Zone <input type="checkbox"/> Loop Zone	<input type="checkbox"/> South Branch Zone <input type="checkbox"/> Southwest Zone <input type="checkbox"/> Bubbly Creek Zone
30 ft. river setback (as verified by plat of survey)	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No
Public Access	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No

Chicago River Design Guidelines

3.2 Multi-Use Path

3.2.1 Design Criteria	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No
3.2.2 Public Access	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No
3.2.3 Minimum Path Width, select all that apply	<input type="checkbox"/> N/A, please describe <input type="checkbox"/> Separated 12' bicycle, 8' pedestrian <input checked="" type="checkbox"/> Combined, 16'	<input type="checkbox"/> Combined, 10' <input type="checkbox"/> Combined, 10' with 2' paved, gravel, or mowed shoulders <input type="checkbox"/> Other, please describe below
3.2.4 Paving and Materials, select all that apply	<input type="checkbox"/> Granite <input type="checkbox"/> Architectural Granite <input checked="" type="checkbox"/> Poured-in-Place Concrete <input checked="" type="checkbox"/> Concrete <u>Pavers</u> <input type="checkbox"/> Unit <u>Pavers</u>	<input type="checkbox"/> Permeable <u>Pavers</u> <input type="checkbox"/> Asphalt <input type="checkbox"/> Decomposed Aggregate <input checked="" type="checkbox"/> Other, please describe below

Comments, please not which section from the list above (e.g. 3.2.3) you're describing

The project is in design phase and material selections are still under consideration. However, paving at the multi-use path is anticipated to be predominantly poured-in-place concrete with accent areas of concrete pavers. All materials will be in compliance with Chicago River North Branch Design Guidelines.

3.3 Furnishings

3.3.1 Site Furnishing Guidelines

Materials, select all that apply

- Stainless Steel
- Galvanized Steel
- Powder Coated Steel

- Hardwoods, describe below
- Recycled Plastic Lumber, describe below
- Other, please describe below

Benches & Tables, on per 250 linear feet (LF) of river frontage.

LF of river frontage
Total Benches
Total Tables

2,000 LF
8 min.
8 min.

Trash and Recycling Receptacles, one metal trash and one metal recycling receptacles per 250 linear feet (LF) of river frontage

LF of river frontage
Total Trash Receptacles
Total Recycling Receptacles

2,000 LF
8 min.
8 min.

Railings

Yes

No

Comments, please note which section from the list above you're describing

The project is in design phase and material selections are still under consideration. However, site furnishings are anticipated to be a combination of stainless steel, hardwood, and concrete. All materials will be in compliance with Chicago River North Branch Design Guidelines.

3.4 Seating and Gathering Areas

3.4.1 Seating Area Guidelines

Location, one per 500 linear feet (LF) of river frontage.

LF of river frontage
Total Seating Areas

2,000 LF
4 min.

Furnishings. Each seating area should provide a minimum of two benches and one trash receptacle.

Total Benches
Total Trash Receptacles

8 min.
4 min.

3.5 Lighting

3.5.1 Lighting Guidelines, see follow pages for an example of an acceptable product data sheet.

Fixture Height, recommended between 14-30 feet tall

Yes, between 14-30'

Other, please describe below

Color Temperature, LED bulbs that provide white light with a color temperature of 3000K or below.

3000K or below

Other, please describe below

Light Pollution

Dark Sky Compliant

N/A

Additional Features

Yes, please describe below

No

Security Lighting

Yes

No

Comments, please note which section from the list above you're describing

The project is in design phase and lighting plan is not yet determined. However, it is anticipated that Phase 1 will be Dark Sky Compliant within the 30 foot riverwalk setback zone at a minimum.

3.6 Wayfinding and Signage

3.5.1 Lighting Guidelines 6.1 Signage

Guidelines

- Brand and Identity Yes No N/A
- Directional Signage, indicate total signs Yes No N/A
- Mile Marker, locate every quarter mile Yes No N/A
- Regulatory Signage, indicate total signs Yes No N/A
- Identity Signage, indicate total signs Yes No N/A

Comments, please note which section from the list above you're describing

The project is in design phase and wayfinding/signage plans are not yet complete. However, all proposed signage within the Riverwalk Setback shall comply with the Chicago River Sign Brand Standards

3.7 Landscaping

3.7.1 Preservation and Restoration Guidelines

- Preservation, preserve existing habitat and plantings Yes N/A No
- Disturbance, minimize site disturbance Yes N/A No
- Protection, protect existing vegetation during construction by installing tree protection fence Yes N/A No
- Tree Preservation and Removal, preserve mature, healthy, native shade and evergreen trees Yes N/A No

3.7.2 Plant Selection Guidelines

- Plant Selection, per Appendix 7.4. Submergent Yes No
- Identify total number of plants selected from Appendix list. Emergent Yes No
- Riparian Yes No
- Upland Yes No
- Fencing, 4-6' ornamental metal fence for vehicular use areas, 4' ornamental metal fence for non-vehicular use areas Yes, height _____ No N/A
- Trees, 2 per 25 LF of river frontage for vehicular use areas, 1 per 25 LF for non-vehicular use areas LF of river frontage Yes No
- Total Vehicular Area Trees Yes No
- Total Non-Vehicular Area Trees Yes No
- Hedges, continuous hedge on the river side of fence is required for vehicular use areas Yes N/A No
- Foundation Plantings, required for non-vehicular use areas Yes N/A No

1,921.1 LF

Revised per DPD comments received 01/29/24.

N/A

77

Revised per DPD comments received 01/29/24.

Revised per DPD comments received 01/29/24.

Comments, please note which section from the list above you're describing

The project is in design phase and the landscape plan is preliminary. Proposed plant materials to be native and adaptive species compliant with the Chicago River Design Guidelines Appendix 7.4 Plant Palette. The design team will provide updated landscape plans and plant materials later in the process. A preliminary list of plant materials is incorporated into the 9/7/2022 River Ecology + Governance presentation. Of the total 1,921.1 LF of Riverwalk, Phase 1 of the Planned Development includes 1,350 LF of Riverwalk, to be completed with the development of Sub-Area A, Parcel A-1. The proposed Phase 1 landscape plan will provide, at a minimum, 54 trees at 2 trees per 25 LF, per the guidelines. The remaining riverfront parcels south of the South Park, included in Sub-Area D, will comply with the tree planting requirements of the Chicago River Design Guidelines as they are built out.

3.8 Riverbank Treatments

3.8.1 Riverbank Guidelines

Existing sloped riverbank is to be retained and improved

Yes No

N/A - There is no existing sloped riverbank within the project extents. River edge is steel sheet piles.

3.9 River Edge Treatments

3.9.1 River Edge Guidelines

Describe the proposed river edge stabilization and enhancement treatments.

The current steel (sheet pile) seawall has a concrete topping on the northern zone adjacent to the casino and exposed steel to the south. An existing concrete pier exists where the former Erie Street bridge abutment was located. The design team is proposing improvements along the river wall to include floating wetlands, a floating platform, and ways to interact with the water edge. All improvements will be contingent on approval by Authorities Having Jurisdiction (AHJ).

Revised per DPD comments received 01/29/24.

3.10 Sloped Bank Treatments

3.10.1 Sloped Bank Guidelines

Describe the proposed sloped bank treatments.

N/A

3.11 Vertical Bulkhead or Seawall Treatments

3.11.1 Vertical Bulkhead or Seawall Guidelines

Describe the proposed vertical bulkhead and seawall guidelines treatments.

Along the vertical seawall there is an opportunity to provide floating wetlands to naturalize the edge.

3.12 Guidelines for Improvements Outside of the Required Setback

3.12.1 Design, Orientation, and Massing of New Structures and Buildings

Placement, locate buildings and vehicular areas outside of the river setback

Yes, height Varies No
 N/A

River-facing ~~facade~~, river-facing facade should be designed as a principal or major ~~facade~~

Yes, height Varies No
 N/A

Massing and Articulation, locate lower buildings with active frontage adjacent to river setback area. Step back massing along river. Locate taller buildings behind low buildings

Yes N/A No

Neighborhood Transitions, step down height of buildings to transition to the scale of adjacent neighborhoods

Yes N/A No

First Floor, activate first floors of buildings with direct access to river and multi-use path

Yes N/A No

Wildlife, incorporate bird-friendly design standards into building designs

Yes N/A No

Sunlight, river corridor should have sunlight for approximately six (6) hours per day

Yes N/A No

3.12.2 Screening Guidelines

Outdoor Storage, if necessary, storage areas should be located beyond the minimum 30' setback area

- Yes, height _____
- N/A
- No

Materials, select all that apply

- Poured-in-Place Concrete
- Split Face Concrete Masonry Units
- Ground Face Concrete Masonry Units
- Heavy Wood
- Other, please describe below

Walls and Fences, screening walls and fences should be planted with vines at the base

- Yes
- N/A
- No

Access, fencing that separates the riverfront from the outside of the setback area should be avoided

- Yes
- N/A
- No

3.13 Transition Between Adjacent Developments

Describe the proposed transitions and treatments between adjacent developments

Transitions will be provided to the riverwalk and river front park that include: Stairs and landing at Bally's Landing, stairs and sloped walkway to Chicago Avenue, stairs and sloped walkway from the Riverfront Park up to Jefferson Street. Complimentary land uses are located adjacent to the riverwalk and riverfront park.

Menu of Improvements

4.1 Overview

4.1.1 Application

PD Project Size

- Large, >1,980 LF of riverfront
- Medium, 660-1,980 LF of riverfront

□ Others

Total Expected Menu Items

Required # of Priority Menu Items

4.3 Nature



4.3.2 New Naturalized Shoreline (priority)

Describe the proposed new naturalized shoreline improvements

The existing river edge condition consists of a steel (sheet pile) seawall, areas of concrete topping on the northern zone adjacent to the casino, and an existing concrete pier where the former Erie Street bridge abutment was located. To achieve a naturalized edge, the design team is reviewing improvements along the river wall to include floating wetlands and areas of native & naturalized plantings adjacent to the seawall edge. All improvements will be contingent on AHJ approval.



4.3.3 Stormwater Management Best Practices (priority)

Describe the proposed stormwater management best practices

The PD 1426 district stormwater approach will transition away from the combined sewer system and comply with the City's stormwater ordinance. The landscape treatments will include a combination of permeable pavement, bio-infiltration systems, and floating wetlands to help capture and clean the storm water run-off.



4.3.4 Aquatic Wildlife Habitats (priority)

Describe the proposed aquatic wildlife habitats

Aquatic wildlife habitats are planned to include floating wetlands adjacent to the riverwall.



4.3.5 Robust Urban Habitats

Describe the proposed robust urban habitats

Robust urban habitats are planned along the riverwalk and as part of the design of the riverfront park. Proposed plant materials to be native and adaptive species compliant with the North Branch Design Guidelines.



4.3.6 Increased Setback

Describe the proposed increased setback

Increases beyond the 30-foot setback for amenity spaces and habitat are proposed to occur along the riverwalk in multiple locations and include: stepping back the building at the north end of the site at Bally's Landing, a proposed water access platform near the mid-point of the casino, utilization of the existing Erie Street bridge abutment at the south end of the Casino, and floating wetlands adjacent to the abutment. Revisions to the building footprint at Bally's Landing account for an increase in the Riverwalk setback of 4,333 SF (6,695 SF original/11,028 SF current).



4.3.7 Large Riverfront Park

Describe the proposed large riverfront park

A large multi-level riverfront park is located at the river's bend on the southern edge of the Bally's Casino event center. The park includes multiple amenity zones and seating areas, accessible circulation, flexible lawn and paved open spaces, and vegetation consisting of native and adapted species.

4.4 Recreation



4.4.2 Access to Water and Docking Facilities

Describe the proposed access to water and docking facilities

A water taxi landing is being planned at the north end of the riverwalk near the Chicago Avenue bridge, adjacent to Bally's Landing. Additionally, an accessible water access platform is proposed near the mid-point of the casino.



4.4.3 Expanded Seating Areas

Describe the proposed expanded seating areas

To compliment the food and beverage venues, a variety of expanded seating areas are being planned adjacent to the casino and within the riverwalk zone, similar to the riverwalk adjacent to Wacker Drive.



4.4.4 Riverfront Overlooks

Describe the proposed riverfront overlooks

River overlook areas are being planned to provide greater access and exposure to the water's edge. The areas are proposed to include utilization of the existing Erie Street bridge abutment near the south park and a floating platform along the riverwall near the mid-point of the casino. Improvements will be contingent on AHJ approval.



4.4.5 Recreational Areas

Describe the proposed recreational areas

The full extent of the riverwalk and riverfront park are considered recreational areas for the use and enjoyment of the broader community.



4.4.6 Support Amenities

Describe the proposed support amenities

Community support amenities include boat landing, a variety of seating places, dog park, lawn gathering areas and flexible open space, and public art.

4.5 Connectivity



4.5.2 Underbridge Connections

Describe the proposed underbridge connections

An underbridge connection at Chicago Avenue is being planned and coordinated between the Onni property to the north and the Bally's property to the south of the Chicago Avenue bridge in conjunction with CDOT who are reconstructing Chicago Avenue and replacing the bridge structure.



4.5.3 Enhanced Connections to Street & Transportation Network

Describe the proposed enhanced connections to the street and transportation network

The project will meet all City and North Branch accessibility standards and will provide accommodations for both pedestrians and cyclists. Enhanced connections will be provided adjacent to the riverwalk and riverfront park that include: stairs and sloped walkways at the north end of the project to Chicago Avenue and Bally's Landing, as well as stairs and sloped walkways from the Riverfront Park up to Jefferson Street.



4.5.4 Elevator / Increased Accessibility

Describe the proposed elevator / increased accessibility

In addition to the stairs and sloped walkways at the north and south ends of the Casino project, public elevators will be available between the event center and casino to provide public access from the street level to the riverwalk.



4.5.5 Cantilevered & Floating Walkways

Describe the proposed cantilevered and floating walkways

In addition to utilization of the existing Erie Street bridge abutment as a pedestrian overlook along the riverwalk near the South Park, a cantilevered overlook/walkway platform extending into the river is proposed near the mid-point of the casino building footprint.



4.5.6 Interpretive Signage

Describe the proposed interpretive signage

There are numerous opportunities to provide interpretive signage along the riverwalk that tells the history of the site or provides information to support the river experience. These are being considered in conjunction with other riverwalk signage elements.



4.5.7 Public Art & Specialty Lighting

Describe the proposed public art and specialty lighting

Public art will be considered in the site design and could entail strategically placed sculpture, artful seating, interactive displays, murals and special paving patterns.

EXHIBITS | OVERVIEW

2.2.2 IMPROVEMENTS

1 **3' SETBACK IMPROVEMENTS** CANOPIES/AWNING SHOULD HAVE A MINIMUM CLEARANCE OF 10 VERTICAL FEET AND DON'T EXTEND 3 OR MORE FEET INTO THE SETBACK ZONE.

2.2.4 VARIANCE

2 **10' MAXIMUM VARIANCE WITH ENCROACHMENTS INTO THE 30' RIVER SETBACK A MAXIMUM OF 1/3 OF SITES RIVER FRONTAGE (LINEAR FEET)**

2.2.5 MITIGATION FOR VARIANCES

3 **30' SETBACK MITIGATION WITH OPEN SPACE 2.5 TIMES THE LAND/OPEN SPACE LOST TO THE ENCROACHMENT (AREA)**

EXCERPTS FROM: CHICAGO RIVER DESIGN GUIDELINES

2.2.2 Improvements or Structures Permitted in the Setback Area

Improvements or structures permitted in the setback area include:

- Private or covered walkways;
- Projections from buildings in the private development zone, including but not limited to awnings and canopies, bay windows and balconies, and overhanging eaves and gutters, provided the projection does not exceed three (3) or more feet into the setback zone, and has a minimum clearance of ten (10) vertical feet from setback grade;
- Stormwater best management practices (BMPs);
- Other structures such as arbors, benches, or planters;
- Fences and walls per zoning code requirements;
- Site furnishings including, but not limited to benches, light fixtures, drinking fountains, and bike racks;
- Storage and wrapping; and,
- Public gathering spaces and other riverfront amenities.

See Section 2.4 Seating & Gathering Areas on page 26 and Chapter 4 on page 40

2.2.3 Improvements or Structures Not Permitted in the Setback Area

Improvements or structures not permitted in the setback area include:

- Buildings or attachments of any kind;
- Vehicular use areas including, but not limited to, parking lots, driveways, service drives, loading docks, vehicle storage, or vehicular storage areas;
- Overhead utility and;
- Private yards, patios, lawns, or decks.

2.2.4 Variances

In certain cases a setback less than the required 30 feet may be permitted to address constrained sites, small, irregularly shaped sites, and to allow flexibility for optimal site plans.

Maximum variance (depth): Structures and private yards may encroach into the 30-foot river setback a maximum of ten (10) feet so that the minimum setback is never less than twenty (20) feet from the top of the bank.

Maximum variance (length): Structures and private yards may encroach into the required river setback provided that the encroachment, or the area with a reduced setback occurs along a

maximum of one-third (1/3) of the site river frontage, measured in linear feet (LF), so that the required setback never occurs along less than two-thirds (2/3) of the site's river frontage.

2.2.5 Mitigation for Variances

Requirement for additional open space for mitigation of variances:

Where driveways and/or private yards encroach into the setback, and the setback is therefore less than thirty (30) feet from the top of bank, additional land should be provided adjacent to the river setback and urban greenway zone to compensate for the loss of open space. This additional land should not be defined or developed as a private yard and should be free of structures.

Amount of additional open space for mitigation of variances:

Additional open space design should be provided adjacent to, and contiguous with, the setback zone at a rate of 2.5 times the land or open space lost to the encroachment.

The preparation of additional open space for mitigation of variances:

Additional open space design should avoid excessively long, deep, or narrow parcels of land that could be relatively or completely unusable and have little to no public benefit. Additional open space proportions should be no more than two (2) feet of open space per one (1) foot of frontage along the river setback line. Depth is measured perpendicular to the setback line.

SITE PLAN | 2.2.4 VARIANCE

2.2.4 VARIANCE

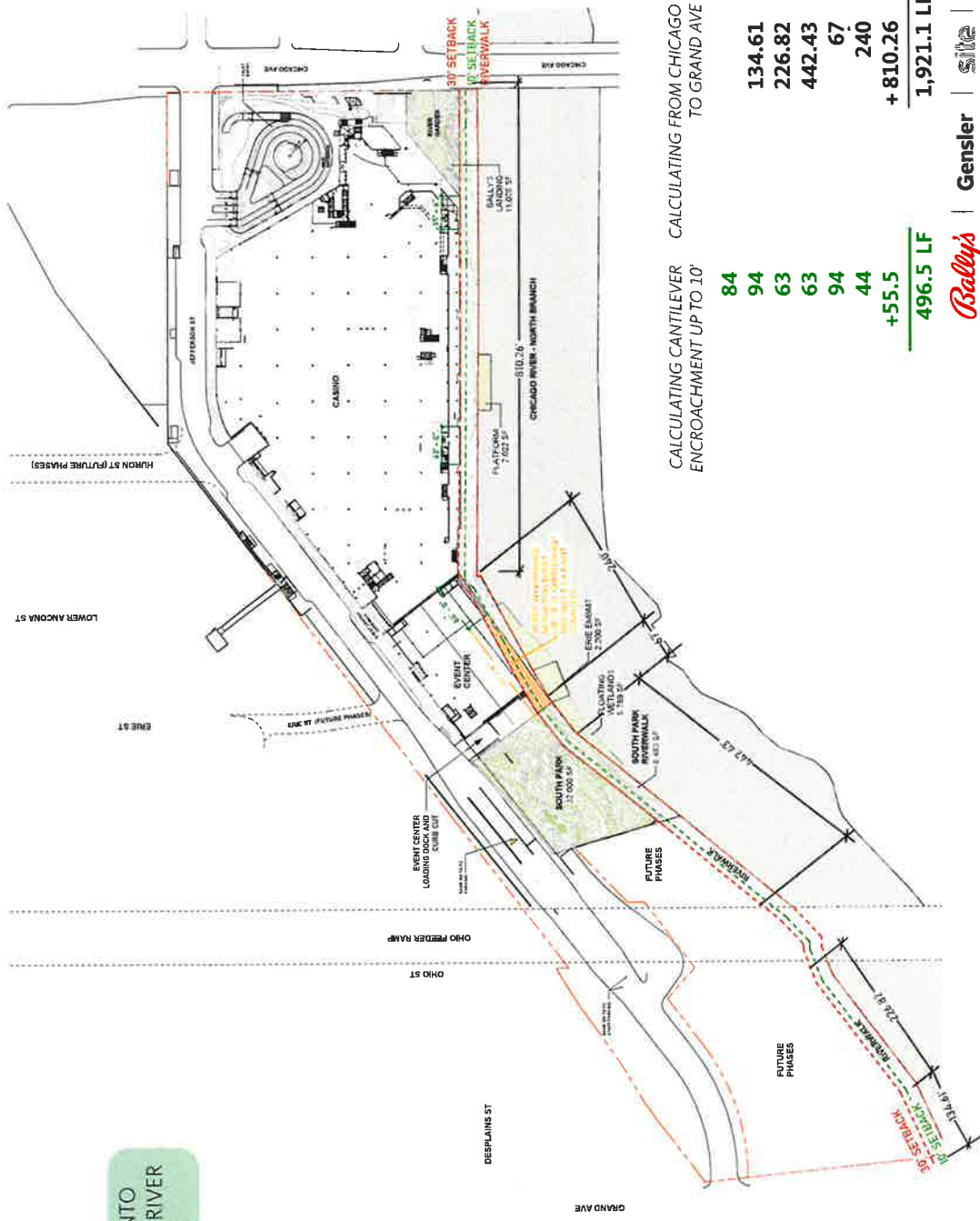
10' MAXIMUM VARIANCE WITH ENCROACHMENTS INTO THE 30' RIVER SETBACK A MAXIMUM OF 1/3 OF SITES RIVER FRONTAGE (LINEAR FEET (LF))

CALCULATING FROM CHICAGO AVE TO GRAND AVE
 TOTAL RIVERWALK FRONTAGE: **1,921.1 LF**
 1921.1/3 = **640.4 LF**

CANTILEVER ENCROACHMENT UP TO 10' : **496.5 LF**

496.5 LF < 640.4 LF

PERCENTAGE: **25.8%**
 MAX PERCENTAGE: **33.3%** (1/3 OF SITE)



CALCULATING CANTILEVER ENCROACHMENT UP TO 10'

- 84
- 94
- 63
- 63
- 94
- 44
- +55.5
- 496.5 LF**

CALCULATING FROM CHICAGO TO GRAND AVE

- 134.61
- 226.82
- 442.43
- 67
- 240
- + 810.26
- 1,921.1 LF**



SITE PLAN | MITIGATION FOR VARIANCE

2.2.5 MITIGATION FOR VARIANCE

30' SETBACK MITIGATION WITH OPEN SPACE 2.5 TIMES THE LAND/OPEN SPACE LOST TO THE ENCROACHMENT (AREA)

ENCROACHMENT BEYOND 10': **4,810SF**

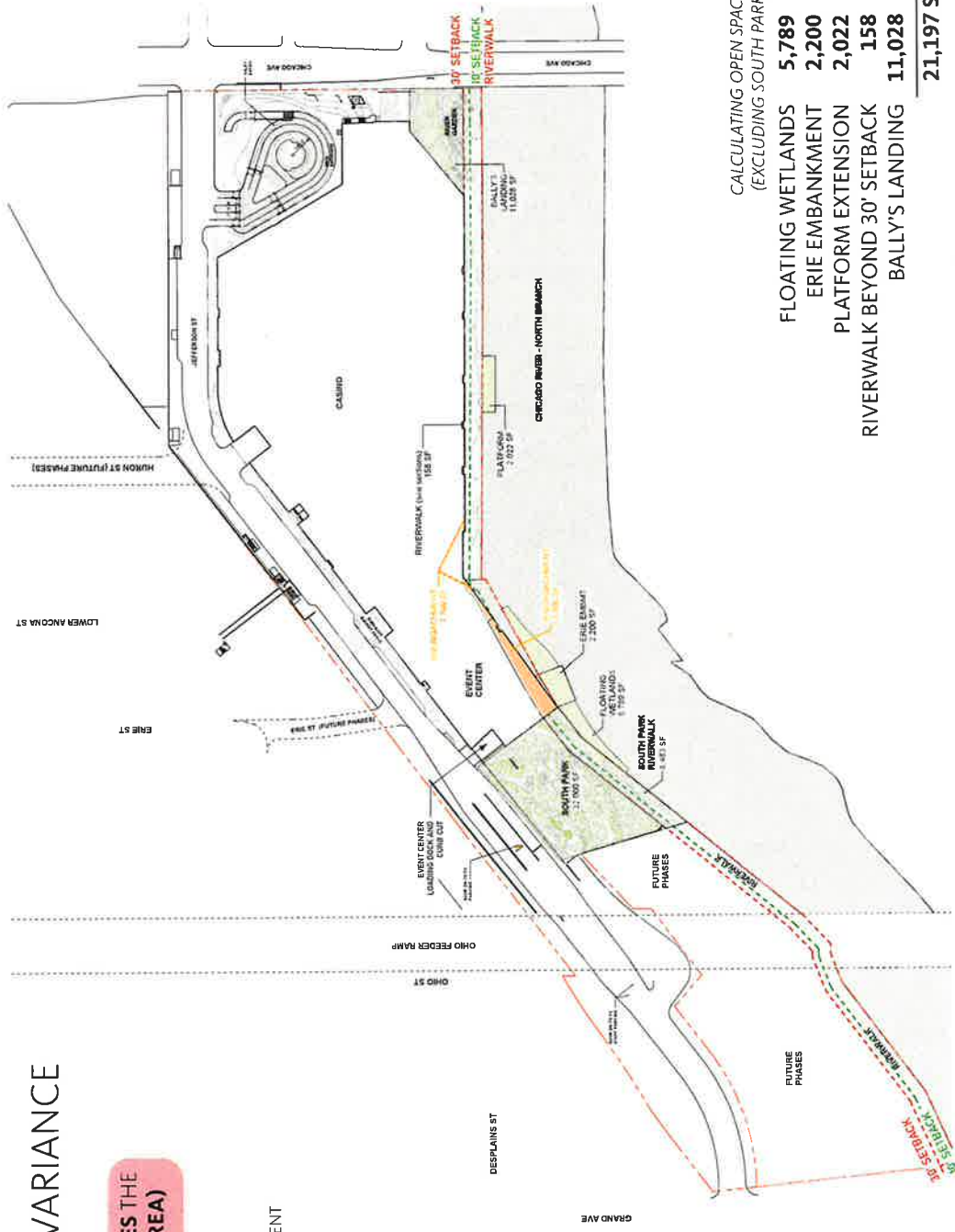
4,810 SF X 2.5 = **12,025 SF**

AREA REQUIRED FOR MITIGATION ENCROACHMENT

12,025 SF < **21,197 SF**

AREA REQUIRED AREA PROVIDED

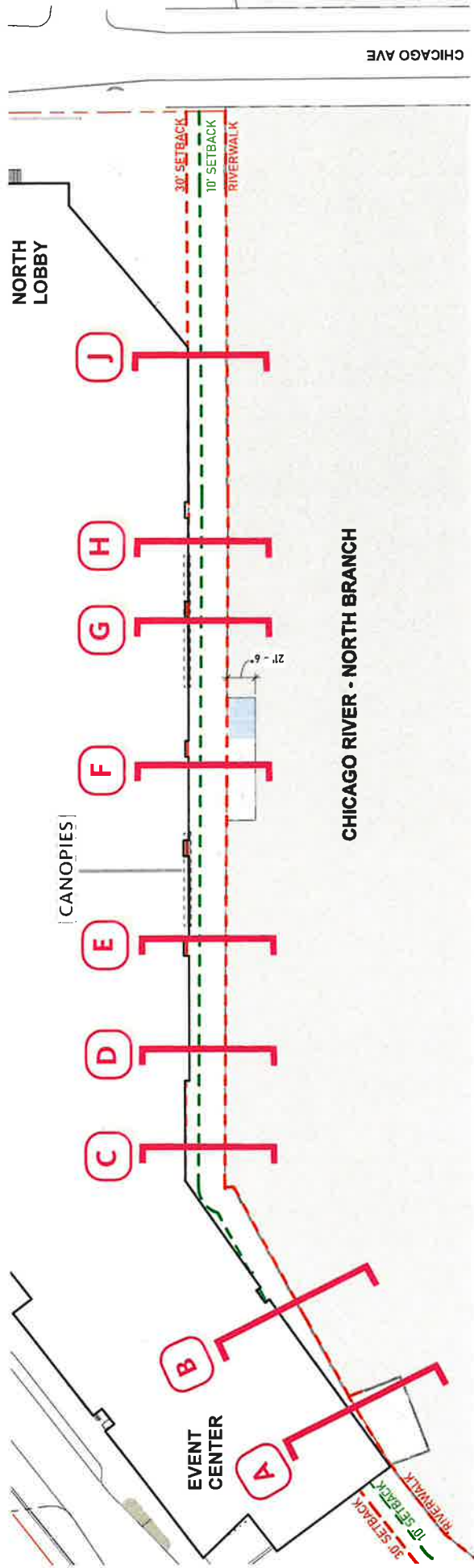
PERCENTAGE: **57%**



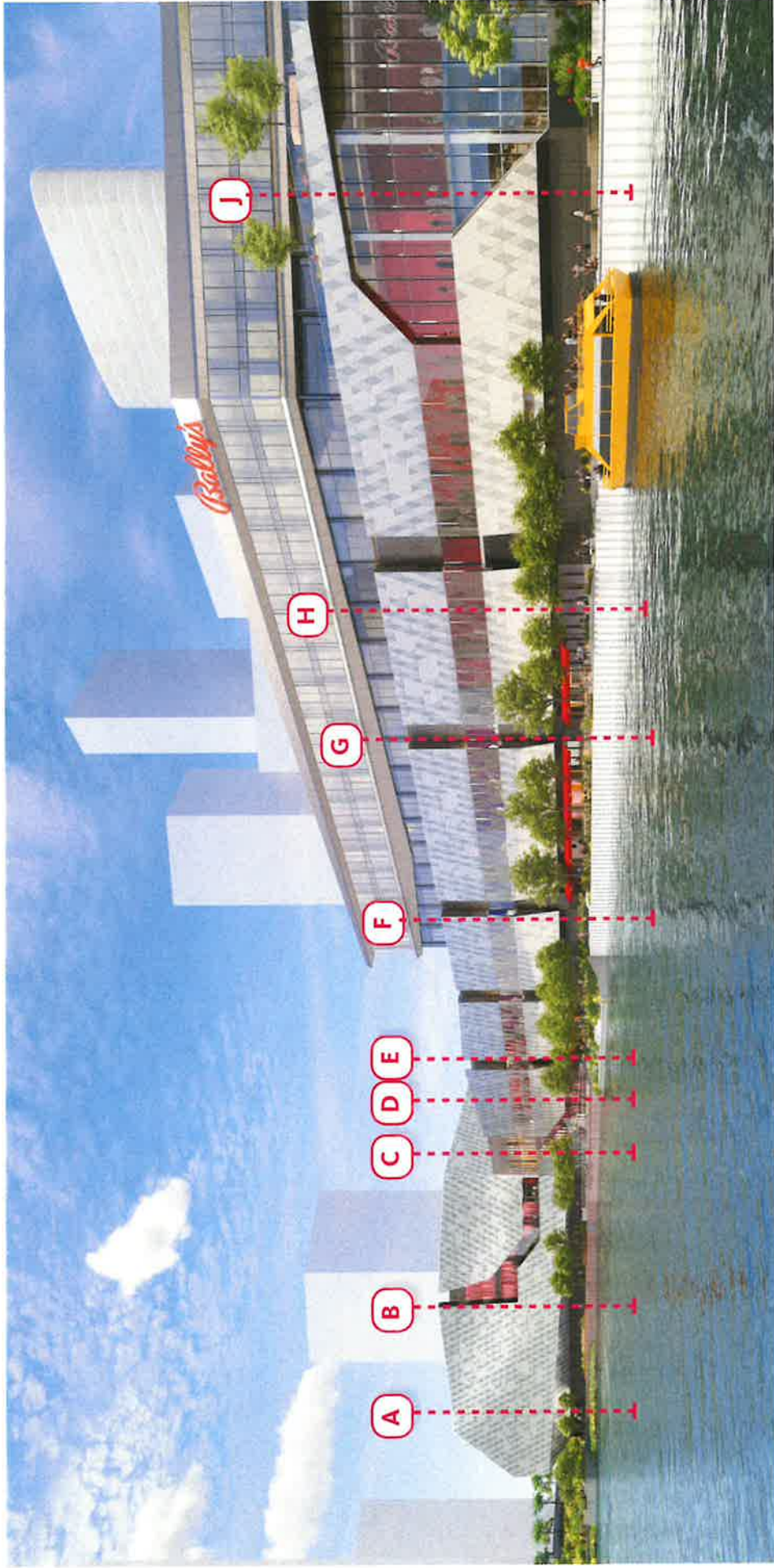
CALCULATING OPEN SPACE
(EXCLUDING SOUTH PARK)

FLOATING WETLANDS	5,789
ERIE EMBANKMENT	2,200
PLATFORM EXTENSION	2,022
RIVERWALK BEYOND 30' SETBACK	158
BALLY'S LANDING	11,028
TOTAL	21,197 SF

SETBACKS | OVERVIEW



SETBACKS | OVERVIEW



SETBACKS | AT SOUTH PARK



DEC 13, 2023 | RIVERWALK SETBACKS EXHIBITS

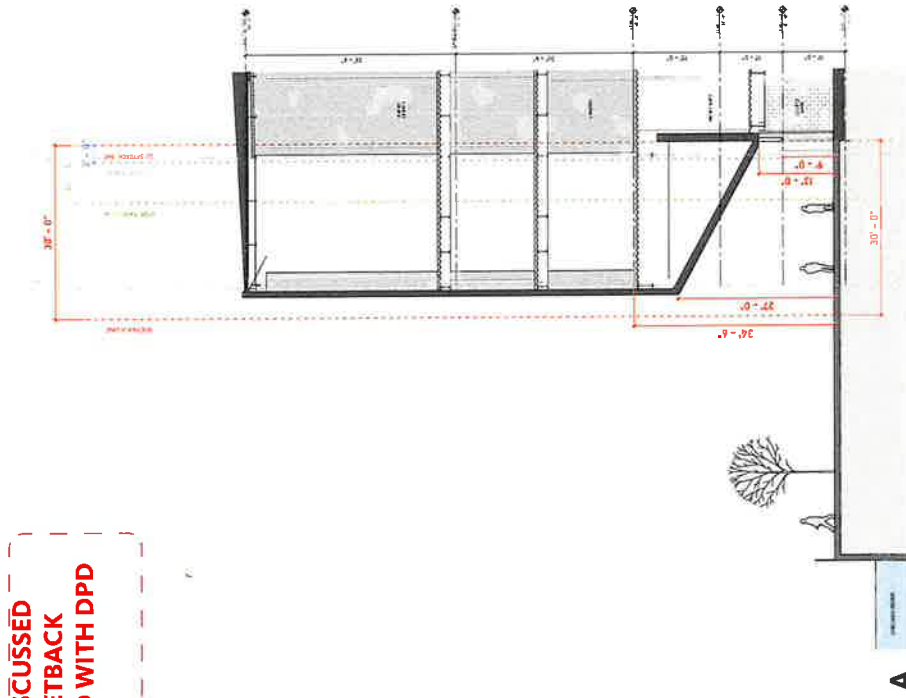
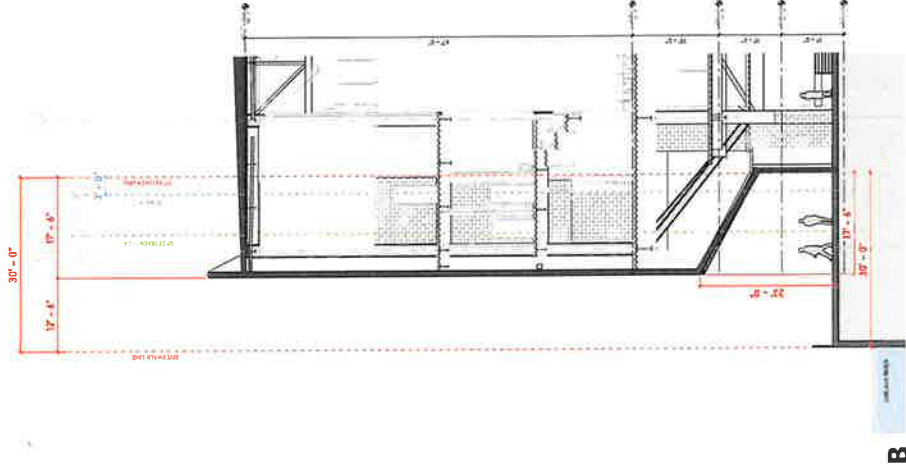
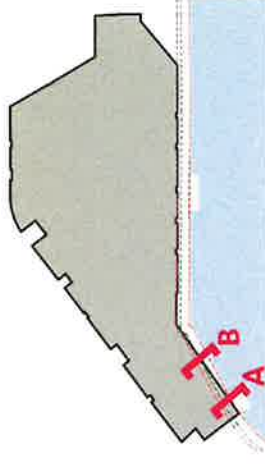
SETBACKS | AT EVENT CENTER



DEC 13, 2023 | RIVERWALK SETBACKS EXHIBITS

SETBACKS | AT EVENT CENTER

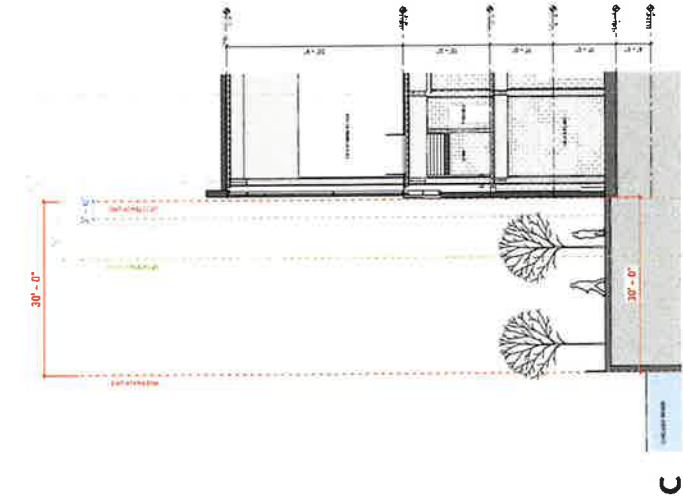
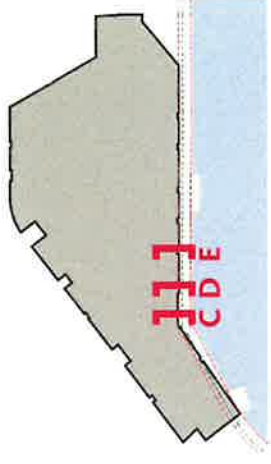
PREVIOUSLY DISCUSSED
EVENT CENTER SETBACK
AND CONFIRMED WITH DPD
JANUARY 6, 2023



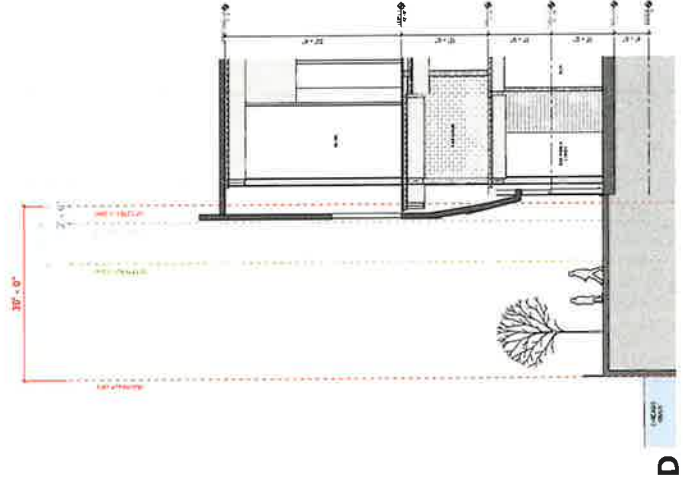
SETBACKS | AT RIVERWALK



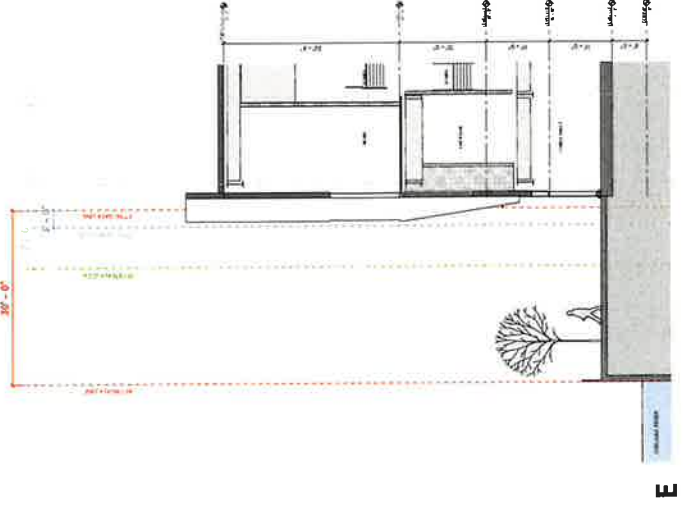
SETBACKS | AT RIVERWALK ENTRY



C

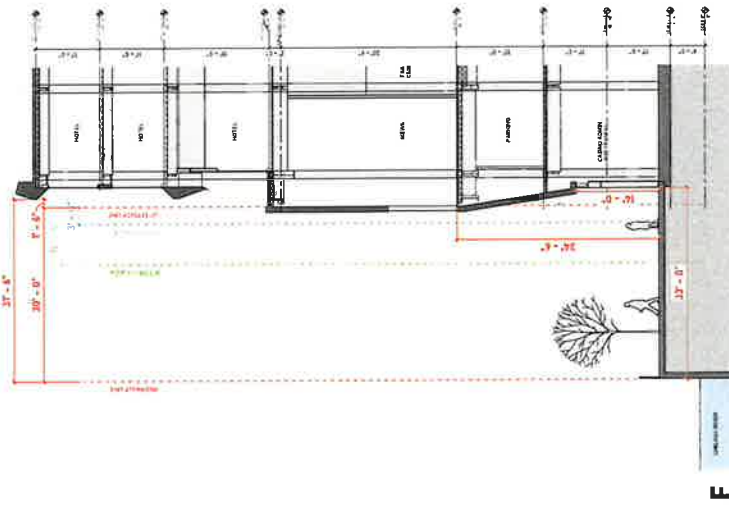
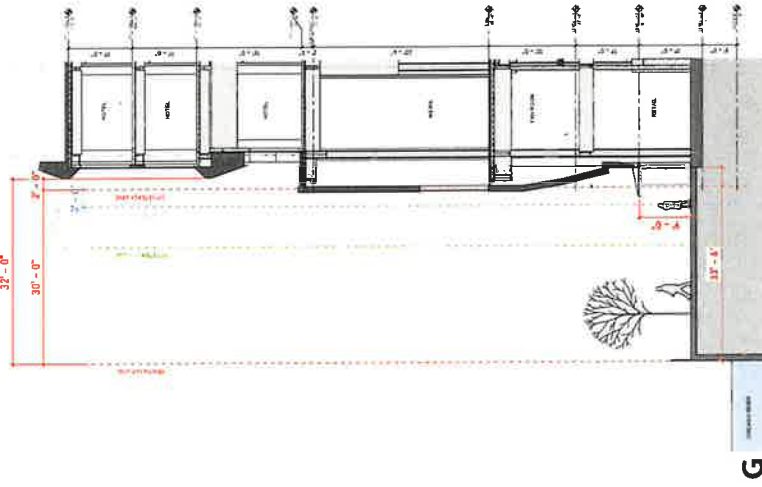
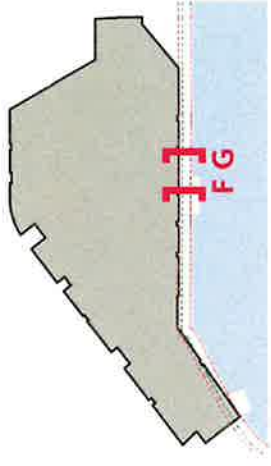


D

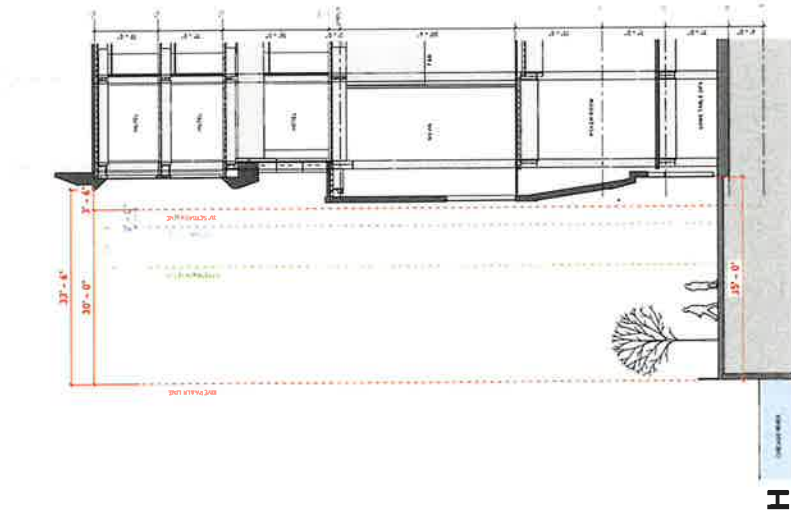
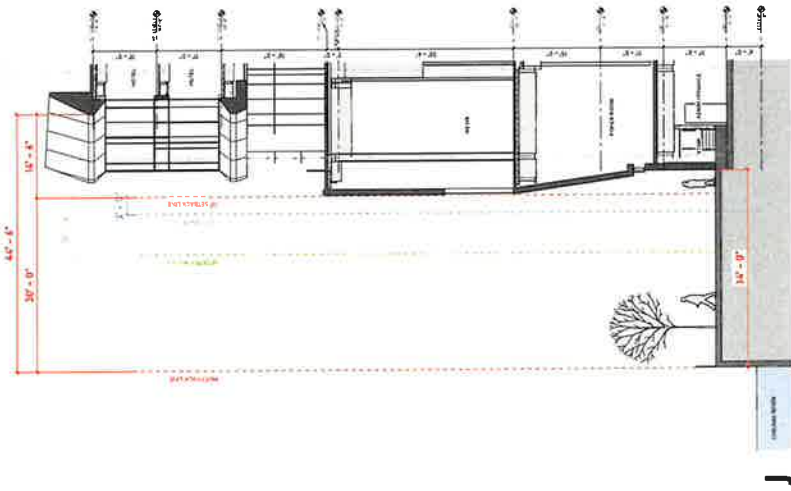
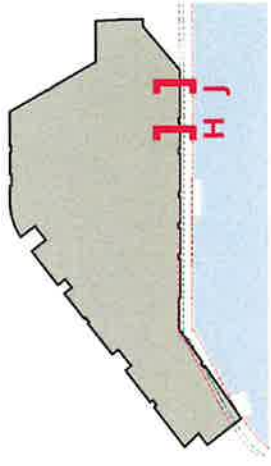


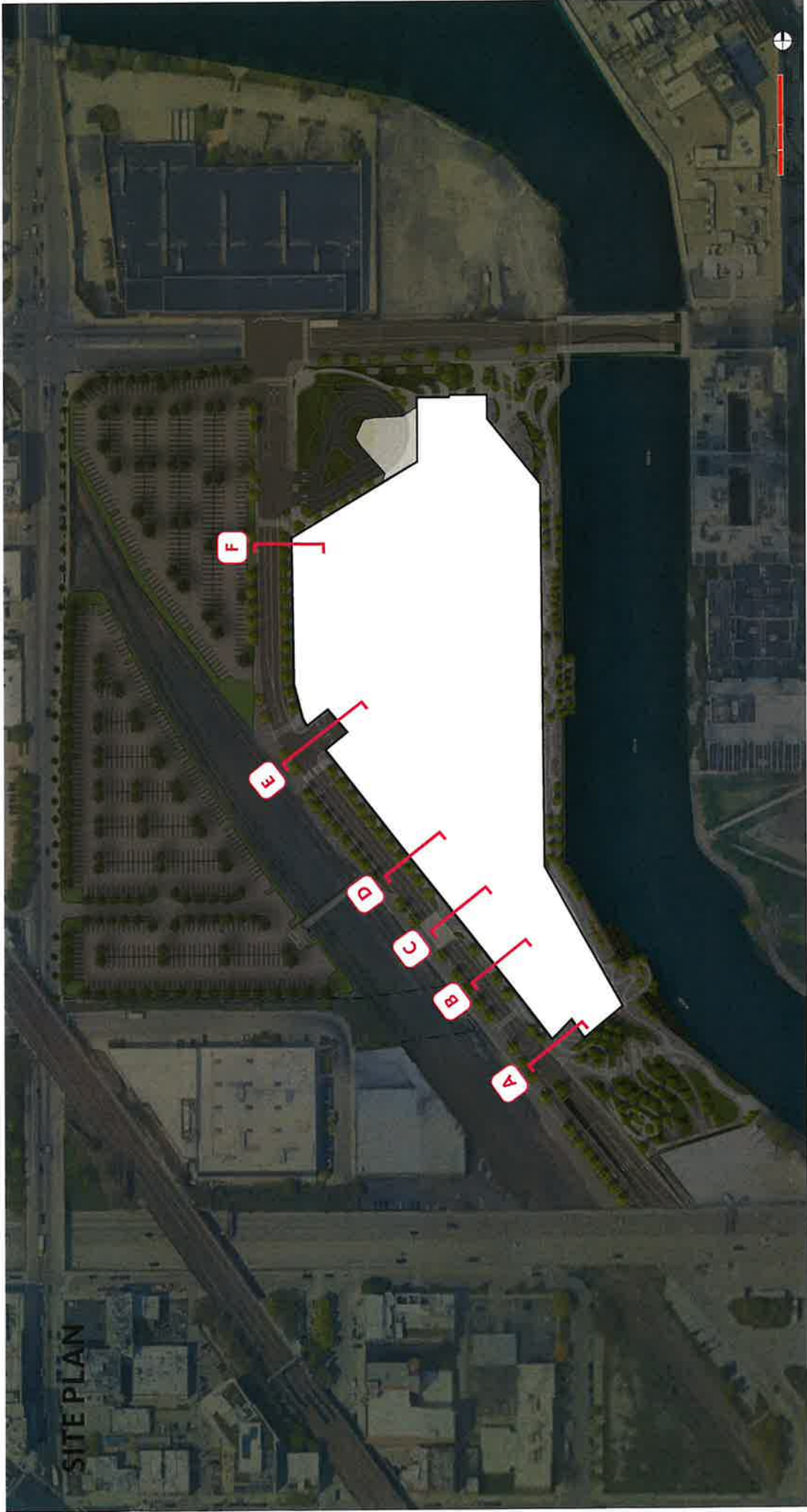
E

SETBACKS | AT RIVERWALK



SETBACKS | AT RIVERWALK (NEAR BALLY'S LANDING)

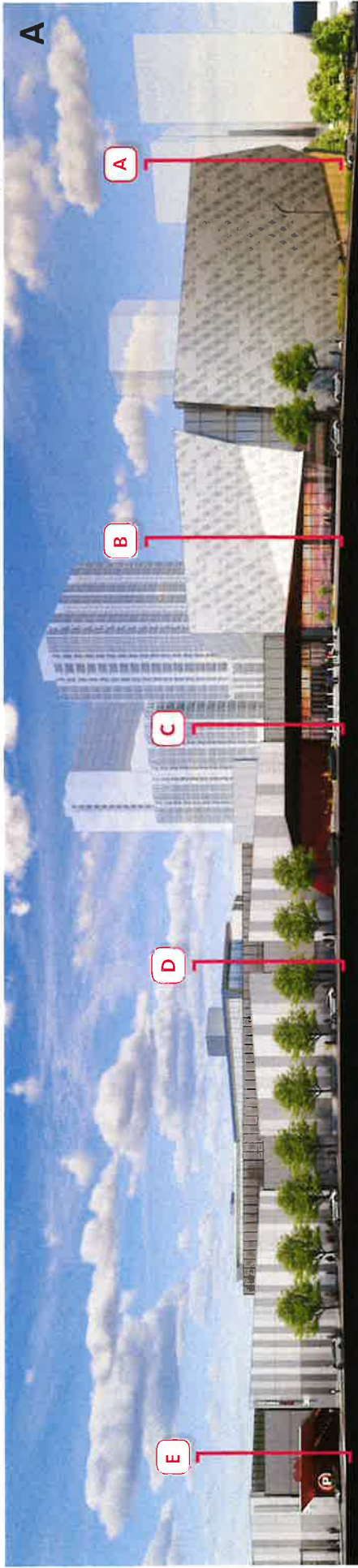
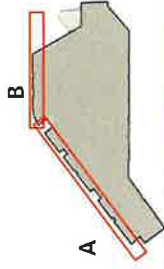




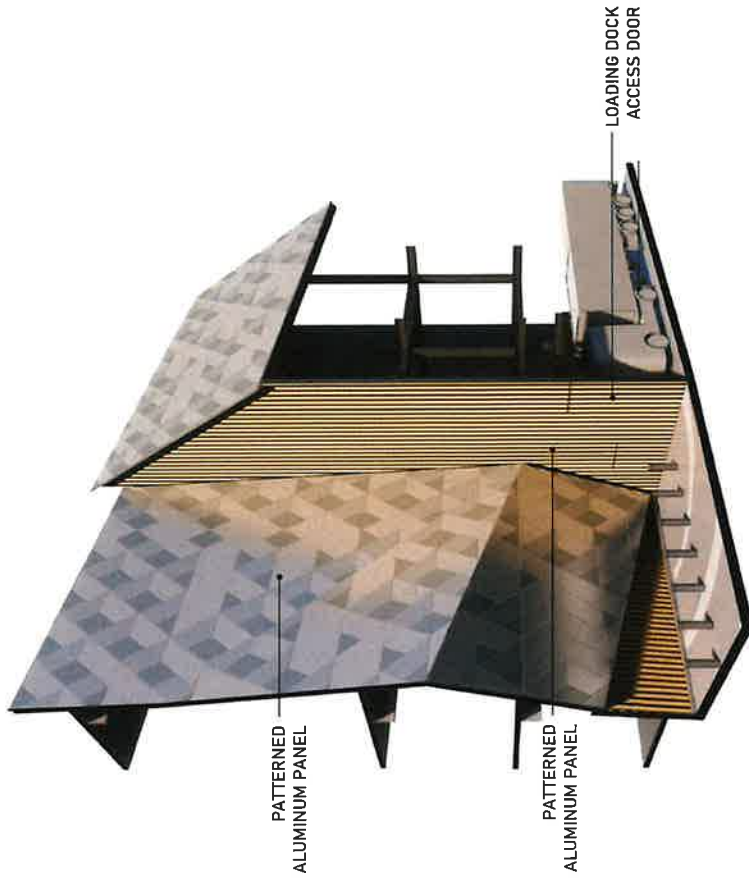
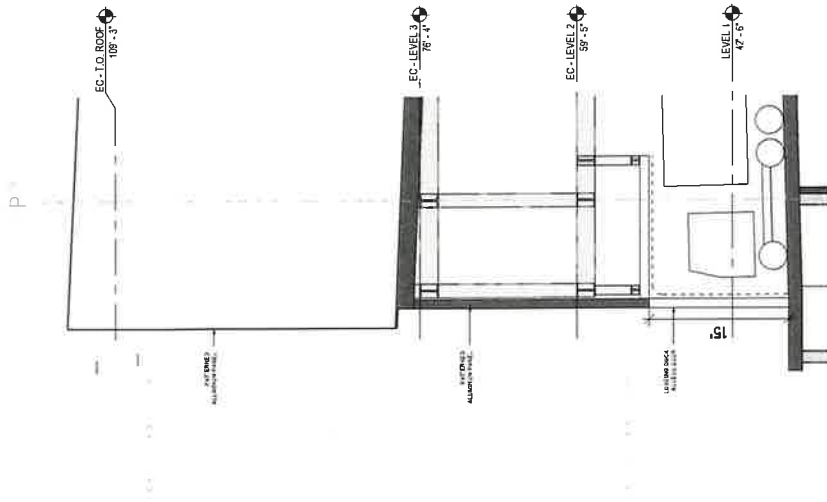
SITE PLAN

Dec 13, 2023 | JEFFERSON ST EXHIBIT
UPDATED | JANUARY 17, 2024

BALLY'S WAY | ELEVATIONS



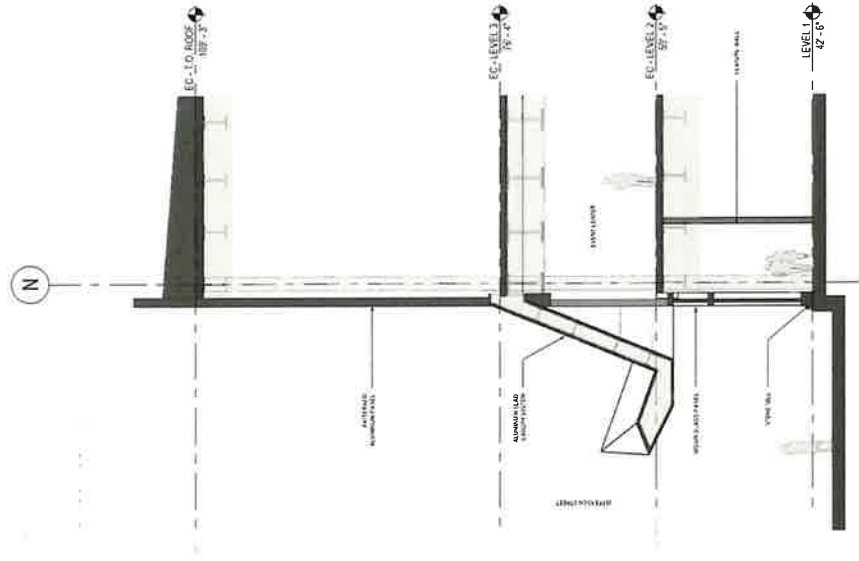
A | EVENT CENTER LOADING DOCK | SECTION + VIGNETTE



VIGNETTE

SECTION AT EVENT CENTER LOADING DOCK

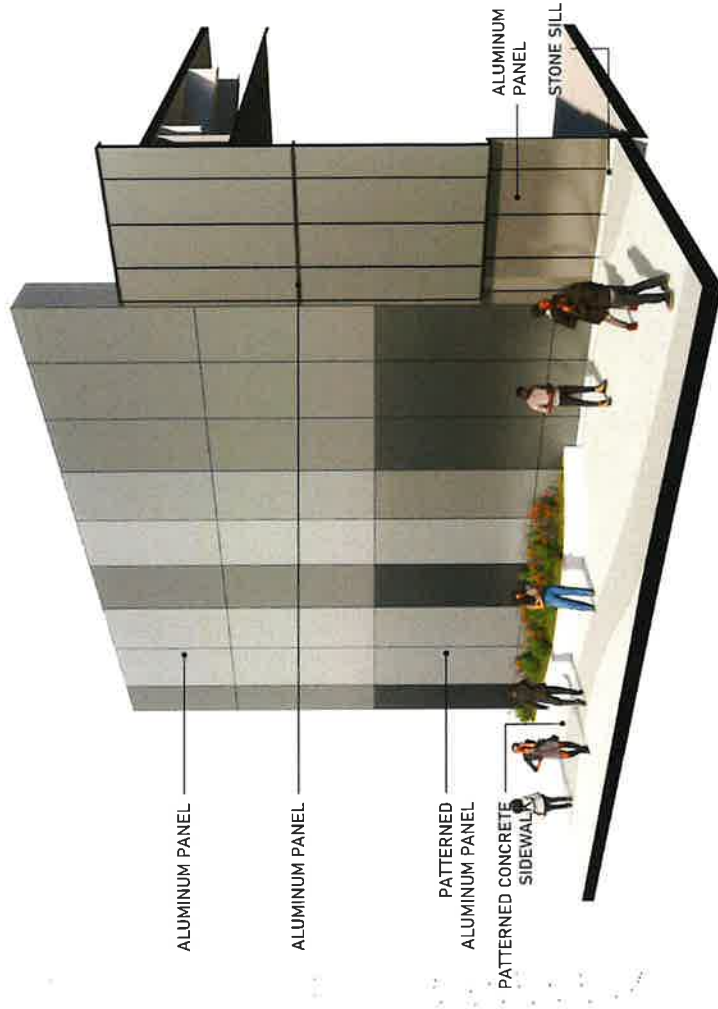
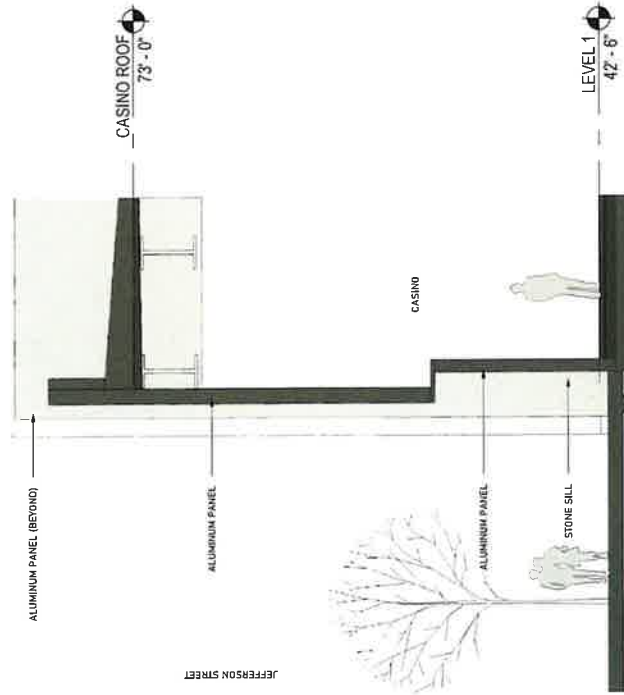
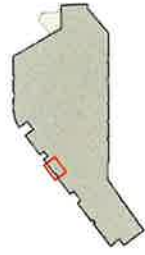
B | EVENT CENTER | SECTION + VIGNETTE



VIGNETTE

SECTION AT EVENT CENTER

D | JEFFERSON STREET TYPICAL | SECTION + VIGNETTE



TYPICAL SECTION

VIGNETTE

E | PARKING ENTRY | SECTION + VIGNETTE



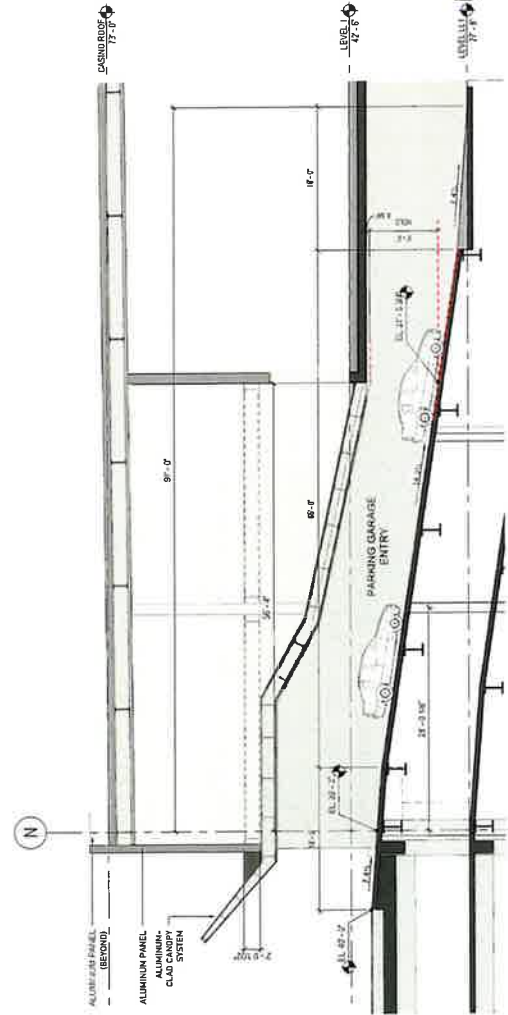
PARKING SIGNAGE

ALUMINUM PANEL

PATTERNED ALUMINUM PANEL

ALUMINUM-CLAD CANOPY SYSTEM

CONCRETE SIDEWALK



CANOPY ROOF
17'-0"

ALUM. CLAD
17'-0"

LEVEL 1
17'-0"

ALUMINUM PANEL
9'-0"

ALUMINUM-CLAD CANOPY SYSTEM

18'-7"

17'-0"

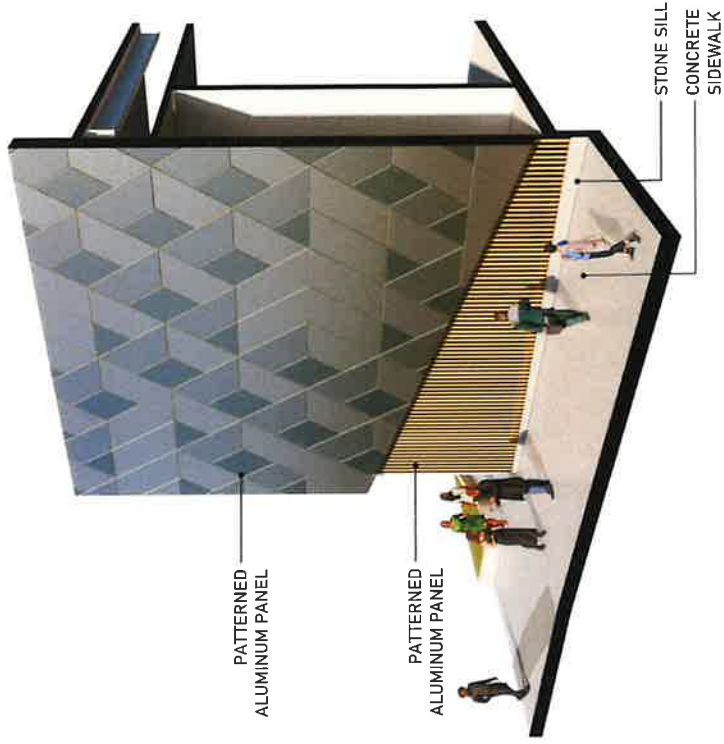
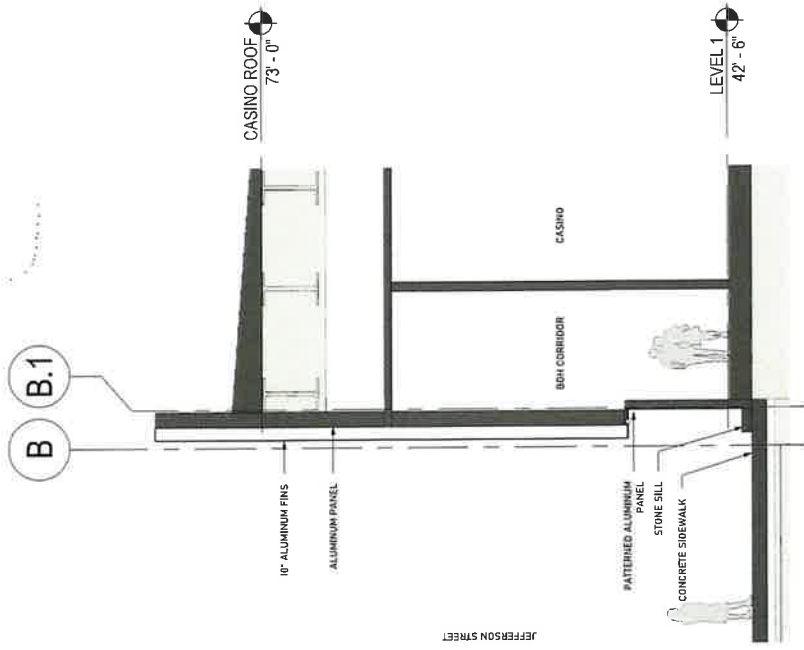
17'-0"

17'-0"

SECTION AT PARKING ENTRY

Dec 13, 2023 | JEFFERSON ST EXHIBIT
UPDATED | JANUARY 17, 2024

F | DROP-OFF AREA | SECTION + VIGNETTE



SECTION AT DROP-OFF AREA

VIGNETTE



Genster
177 West Chicago Avenue
Chicago, IL 60610
312.467.1100

STANDARD COLORS

Color swatches and logos for various materials and finishes, including Bally, Genster, and other brands.

NOT FOR CONSTRUCTION
CONFIDENTIAL - Bally Chicago

CL2.00



PLANNING SCHEDULE
Lynn
K. K. K.

Table with columns: Item Code, Quantity, Unit Name, Common Name, Size, Finishing, Notes. Contains detailed material specifications for various items like 'Solid Pine', 'Oak Ply', 'Maple Ply', etc.

MULLER'S PLANT SCHEDULE



**DEPARTMENT OF PLANNING AND DEVELOPMENT
CITY OF CHICAGO**

May 10, 2023

Meg George
Akerman LLP
444 W. Lake St., Suite 900
Chicago, IL 60606

Re: Air Rights Waterway Business Residential Planned Development No.1426, Site Plan Approval Request for Subarea A, Bally's Casino.

Dear Ms. George:

Please be advised that your request for site plan approval to Planned Development No. 1426 ("PD 1426"), Subarea A, has been considered by the Department of Planning and Development pursuant to Section 17-13-0800 of the Chicago Zoning Ordinance and Statement Number 12 of PD 1426. You are requesting on behalf of your clients, Bally's Chicago Operating Company LLC., the property owners of Subarea A, a site plan approval for development at the subject site.

You are requesting site plan approval specifically within Sub-Area A to allow for the construction of a mixed-use entertainment complex to be approximately located at 777 W. Chicago Avenue. The development as proposed includes a casino, a 3000-seat entertainment venue, and various eating and drinking and retail establishments. In addition, the proposal contains a multi-level parking garage below grade, and a river walk in compliance with the River Design Guidelines that extends the entire length of the property from Grand Avenue north to Chicago Avenue, in Phase 1 the Riverwalk will be completed to full build out from Chicago Avenue to the southern edge of the park area with an interim connection continuing south to Grand Avenue; this remaining portion will be completed to full once the adjacent sub-area is approved via site plan approval request through the Department. The proposed bulk, uses, and density of the proposal all remain compliant with the planned development documents already reviewed and approved by the Chicago City Council and published in the City Council Journal dated December 14, 2022.

Pursuant to Statement No. 12 of the planned development, it was acknowledged that the final design of the proposed development was in a state of evolution and as a result would be finalized and memorialized via this process. Further it should be clarified that the two interim surface parking lots as shown for reference purposes in the revised exhibits will each need to request and received their own site-plan approvals in the future – this can be done individually or coupled as one submission. Lastly, reference has been made in several documents to inclusion of public art as part of the proposed casino development, none of that art is finalized or included as part of the site plan approval letter issued here today; the inclusion of public art will be done in coordination between the applicant, the Department of Planning and Development, the Department of Cultural Affairs and Special Events and, when any public right-of-way is involved the Department of Transportation.

We have reviewed the following drawings and exhibits and those are made part of this planned development and accompany this letter:

- Existing Zoning Map – Unchanged but included for reference.
- Existing Land Use Map – Unchanged but included for reference.
- General Land Use Map
- Planned Development Boundary and Property Line Map - Unchanged but included for reference.
- Right of Way Adjustment Map
- Site Sections - Unchanged but included for reference.
- Development Plan Associated with Infrastructure Requirements
- Sub-Area and Parcelization Plan
- Development Phasing Diagram
- Phase I: Interim Site Plan for Parking and Riverwalk
- Phase I: Interim Site Plan for Parking – Reference Only – The proposed interim surface parking lots will be the subject of a future site plan approvals at the applicant’s future request.
- Phase I: Employee Parking Lot South– Reference Only – The proposed interim surface parking lots will be the subject of a future site plan approvals at the applicant’s future request.
- Phase I: Employee Parking Lot North– Reference Only – The proposed interim surface parking lots will be the subject of a future site plan approvals at the applicant’s future request
- Site Access and Loading Plan Level 1
- Site Access and Loading Plan Level 2
- Site Level Plan – Level 1
- Site Level Plan – Level 2
- Site Level Plan – Level 3
- Site Sections – 1 of 2 - Unchanged but included for reference.
- Site Sections – 2 of 2
- Public Open Space Plan
- Generalized Landscape Plan
- Bike and Pedestrian Connectivity Plan – Full Build-Out
- Bike and Pedestrian Connectivity Plan – Phase 1
- Transitway Accomodation Plan - Unchanged but included for reference.
- Design Guidelines - Unchanged but included for reference.
- Phase 1 – Green Roof Plan
- Phase 1 – Floor Plan L1
- Phase 1 – Floor Plan P1
- Phase 1 – Floor Plan P2
- Phase 1 – Floor Plan P3
- Phase 1 – Typical Hotel Floor Plans - Unchanged but included for reference.
- Phase 1 – Building Section 1 of 2
- Phase 1 – Building Section 2 of 2
- Phase 1 – Proposed Landscape Plan
- Phase 1 – Riverwalk Site Sections
- Phase 1 – Riverwalk Site Section I-I (Event Center)
- Phase 1 – Building Elevations E, W, N, and S.
- Rendered Images of the Proposed Development (12)
- Update River Design Guidelines Checklist and 12 illustrative exhibits
- (7) Section and Vignette Exhibits detailing proposed building construction

While the above list is extensive it is still the obligation of the development team to remain compliant with the planned development document, the Host Community Agreement, and the Municipal Code of Chicago. Additional review of submitted documents will occur at the time of permitting as if the case with all development proposals to ensure compliance.

The Department of Planning and Development has worked closely with the Department of Transportation on this Site Plan Review request. As a result of these reviews CDOT has reviewed and approved the phase 1 site plan and companion traffic memo, finding it acceptable for approval, conditional upon ongoing coordination on the Jefferson St. cross section and treatment of the western edge, ongoing coordination on the design of signalized intersections design on Grand and Chicago Ave., ongoing coordination on east-west connection streets on Erie, and future Huron, Ancona & Desplaines, and on the finalization and approval of the complete landscape plans.

Regarding your request, the Department of Planning and Development has determined that allowing the proposed development in Sub-Area A will not create an adverse impact on the remainder of the Planned Development or the surrounding neighborhood, will not result in an increase in the height, bulk, or density of Subarea A as approved, and will not change the character of the development.

Accordingly, pursuant to the authority granted by the Chicago Zoning Ordinance and PD 1426, I hereby approve the foregoing site plan approval request, but no other changes to this Planned Development. The attached exhibits are in accordance with and satisfy the requirements of PD 1426. Accordingly, this site plan approval request is hereby approved.

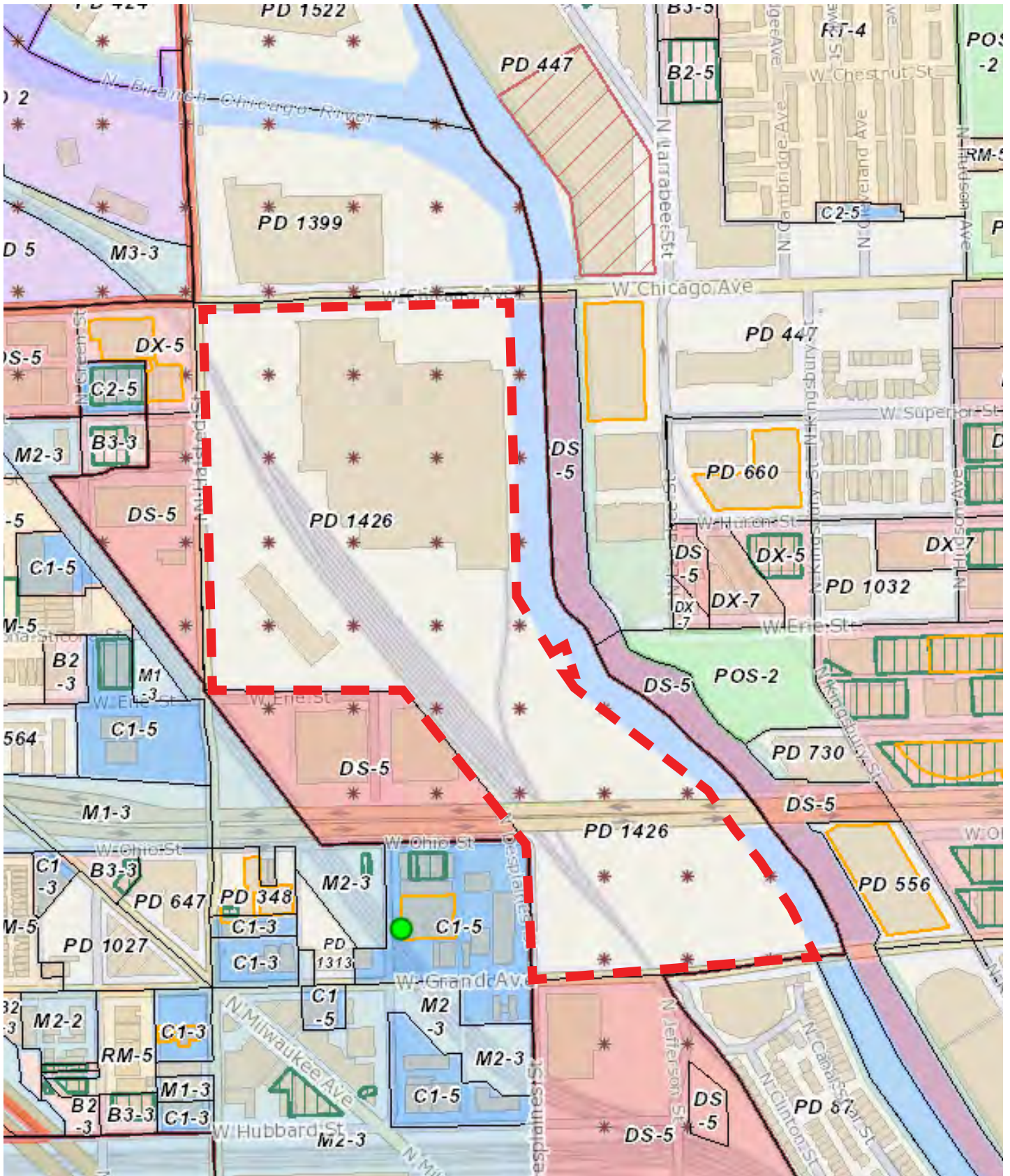
Sincerely

A handwritten signature in black ink, appearing to read 'Patrick Murphey', with a long horizontal flourish extending to the right.

Patrick Murphey
Zoning Administrator

CC: Cindy Roubik, Noah Szafraniec, Ethan Gillman, Mike Marmo, Erik Glass, Main file

121 NORTH LASALLE STREET, ROOM 1101, CHICAGO ILLINOIS 60602

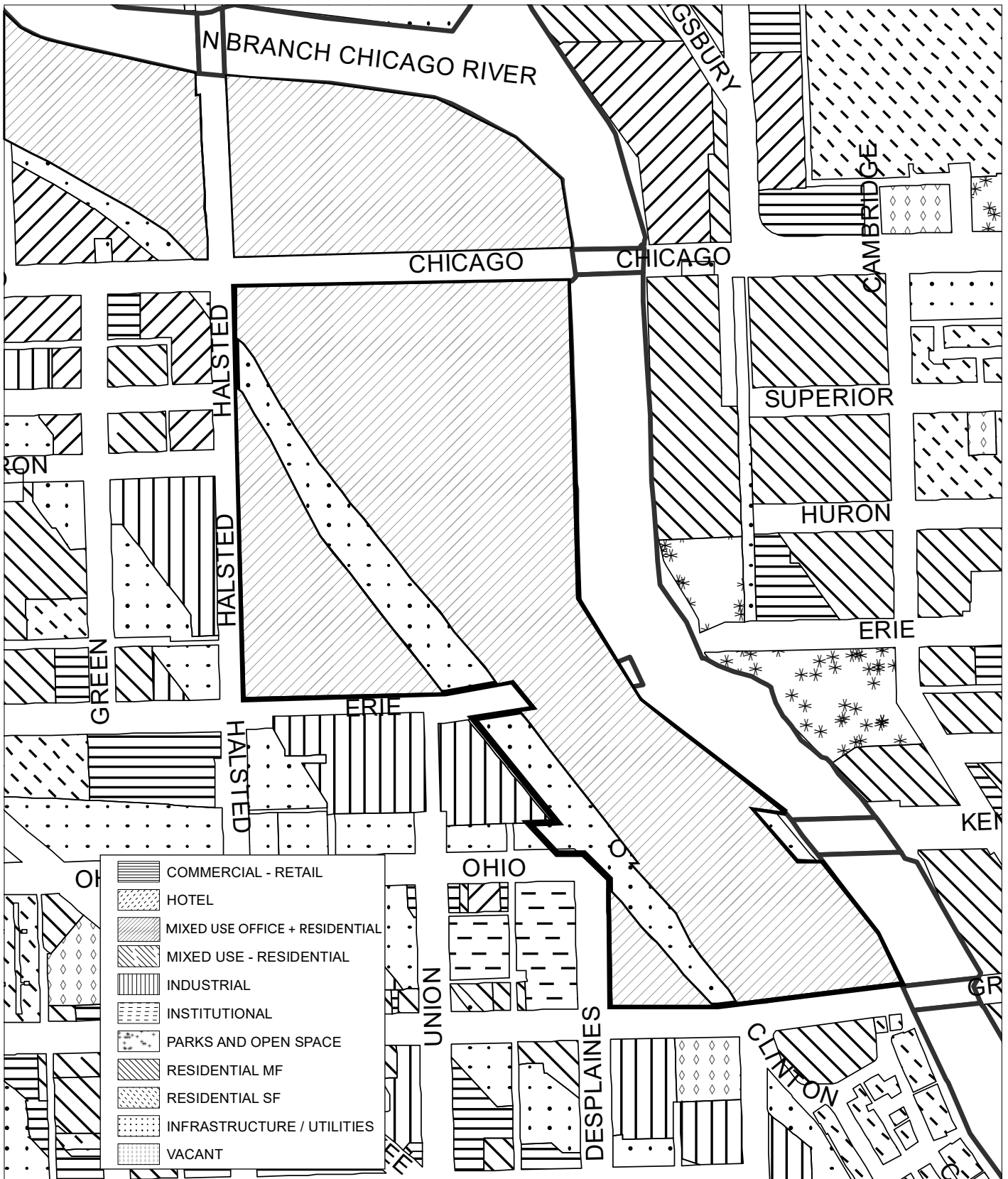


EXISTING ZONING MAP

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **May 03, 2023**



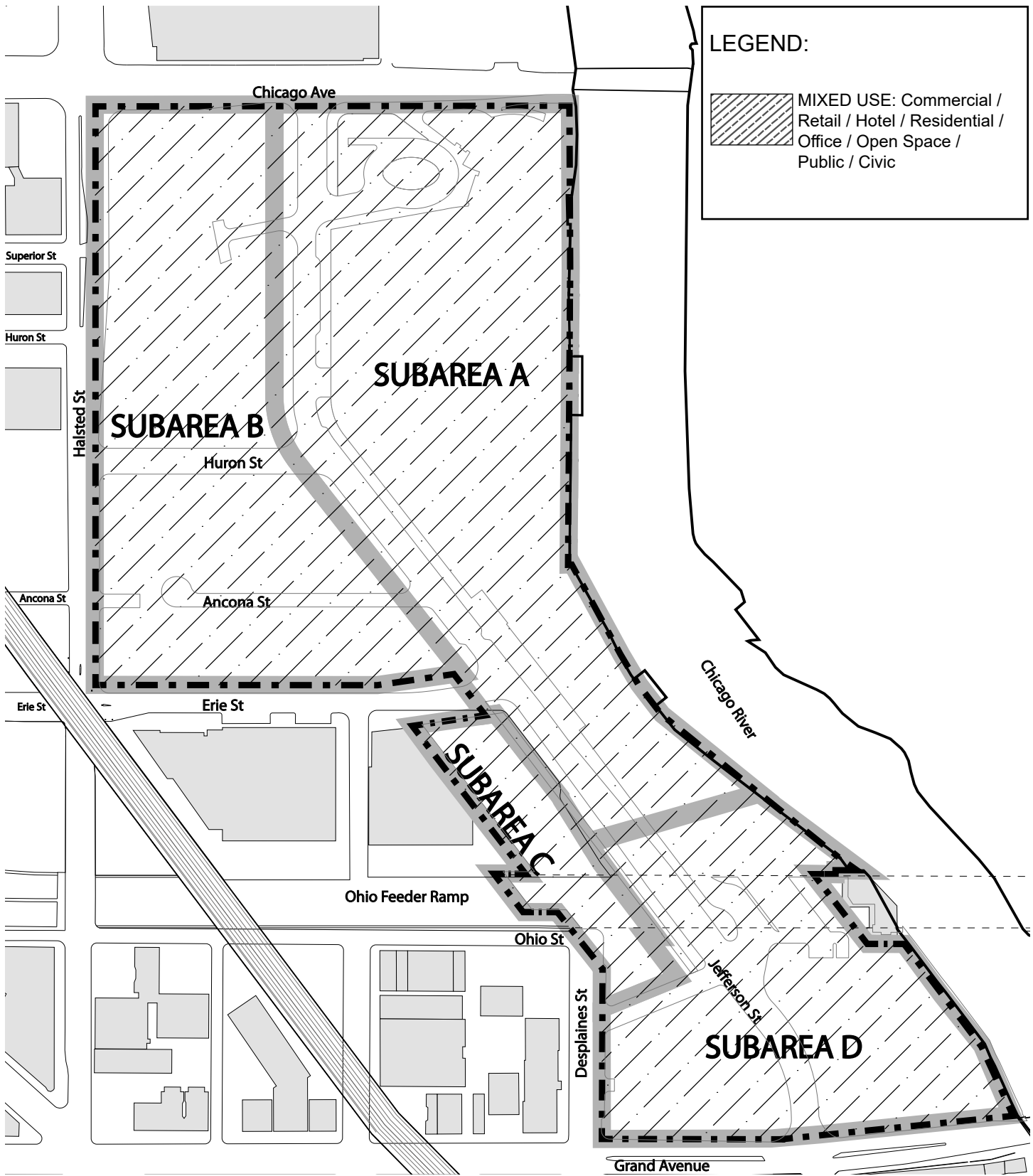


EXISTING LAND USE MAP

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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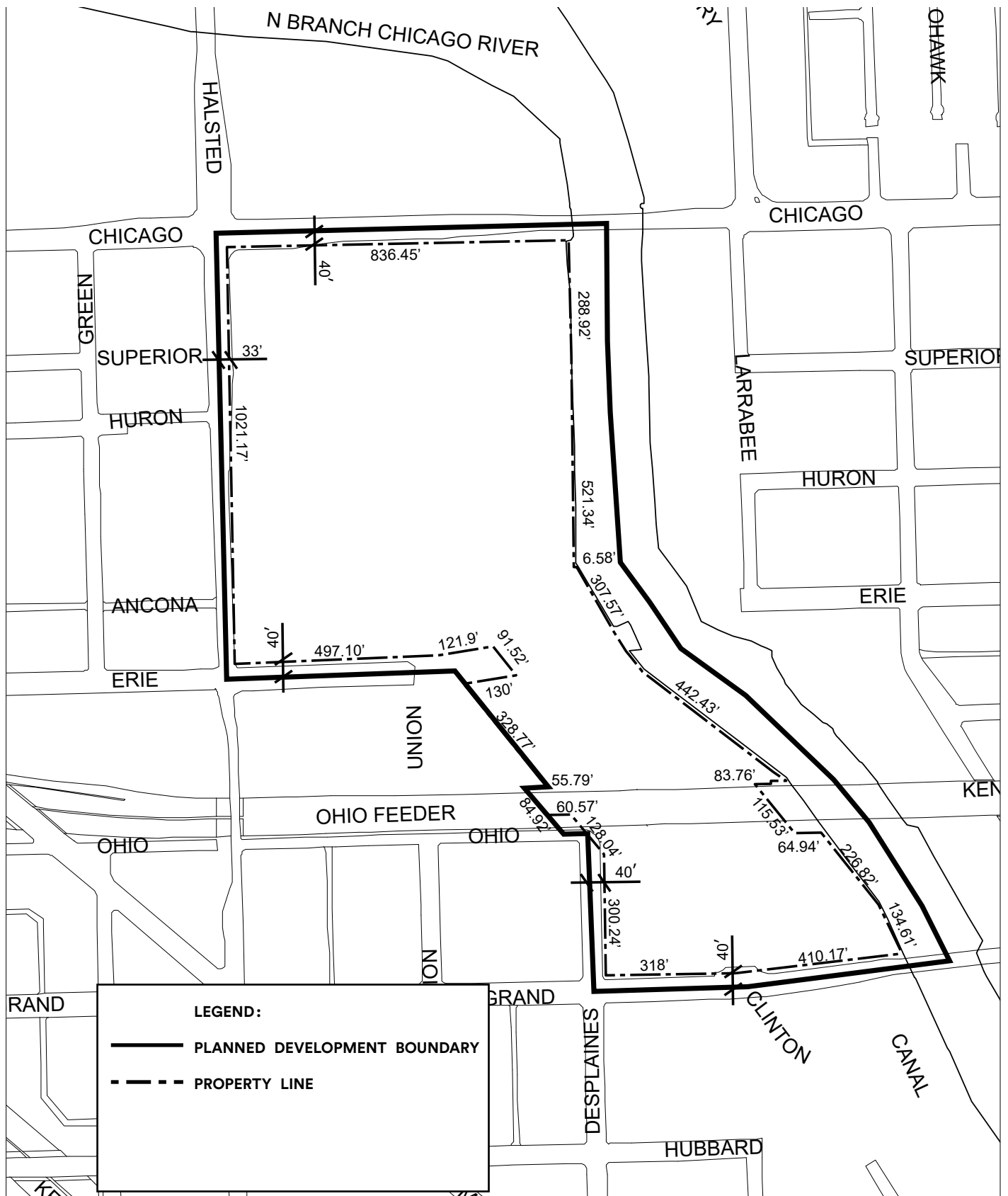


GENERAL LAND USE MAP

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE...; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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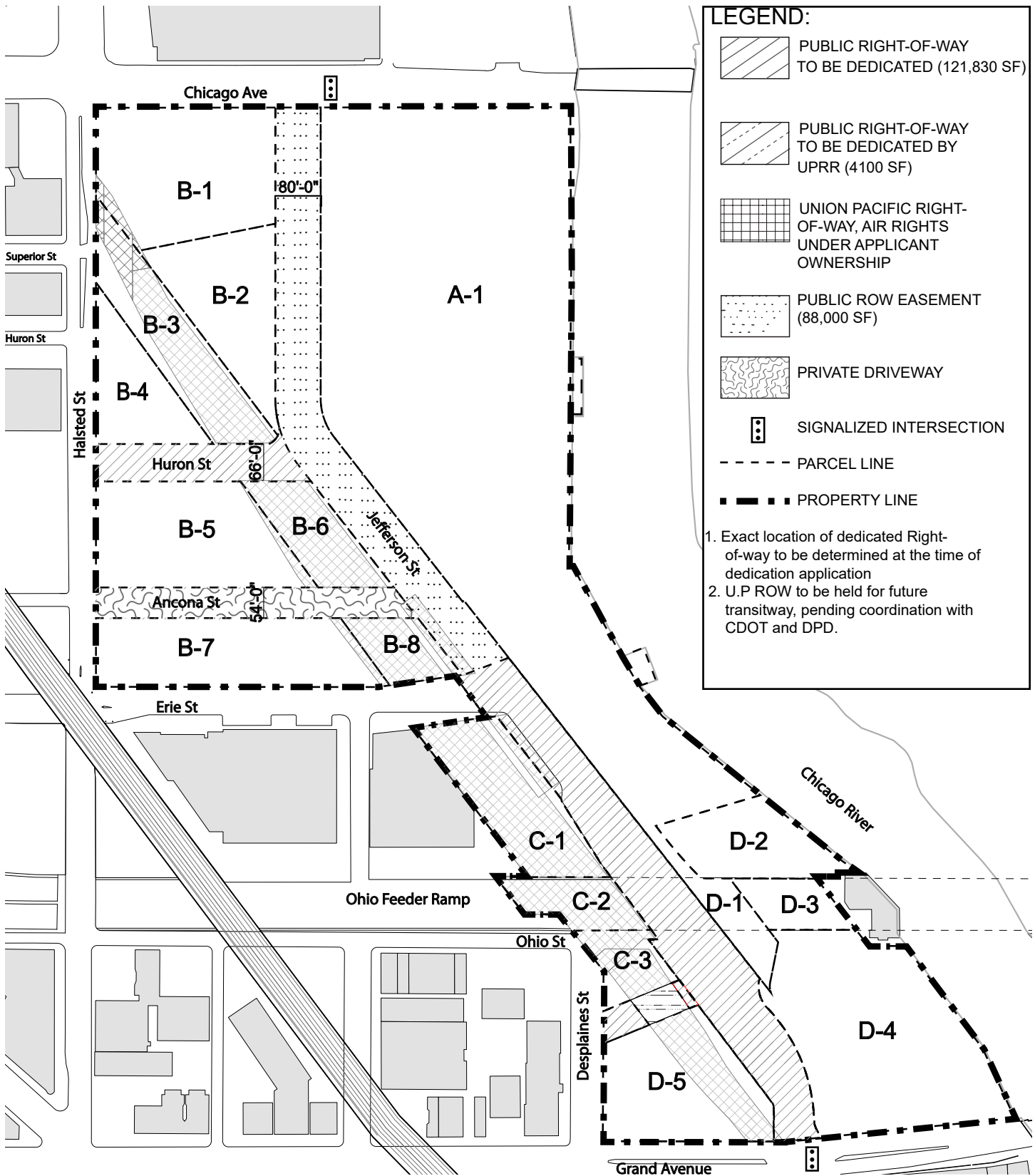


PLANNED DEVELOPMENT BOUNDARY AND PROPERTY LINE MAP

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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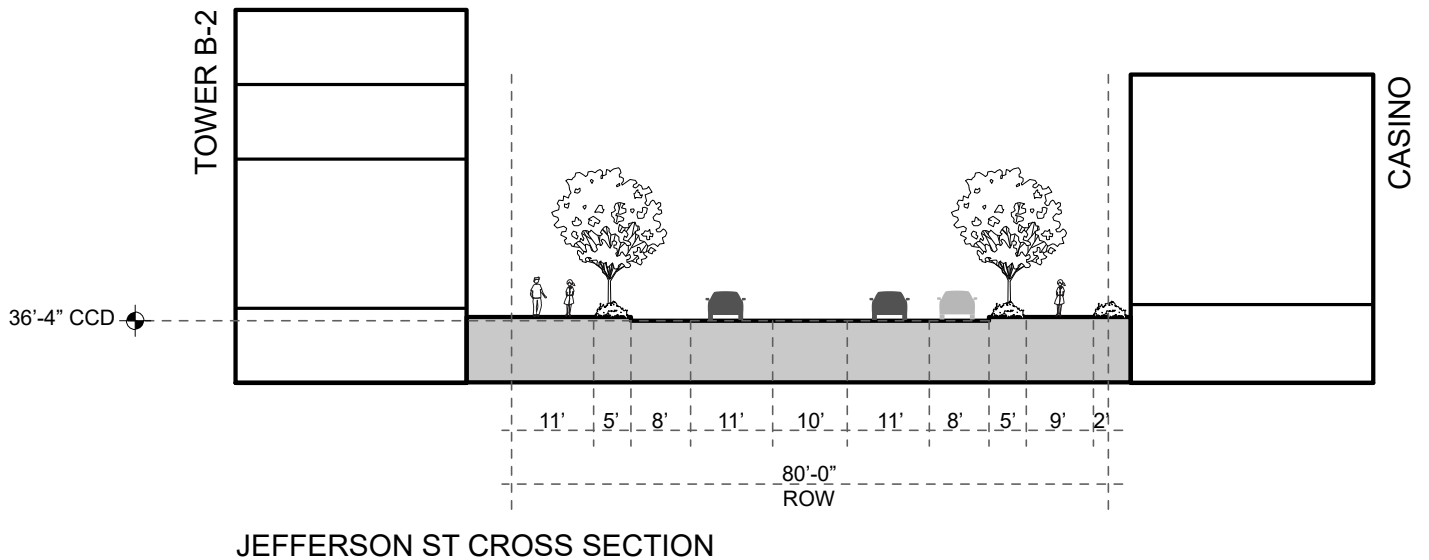
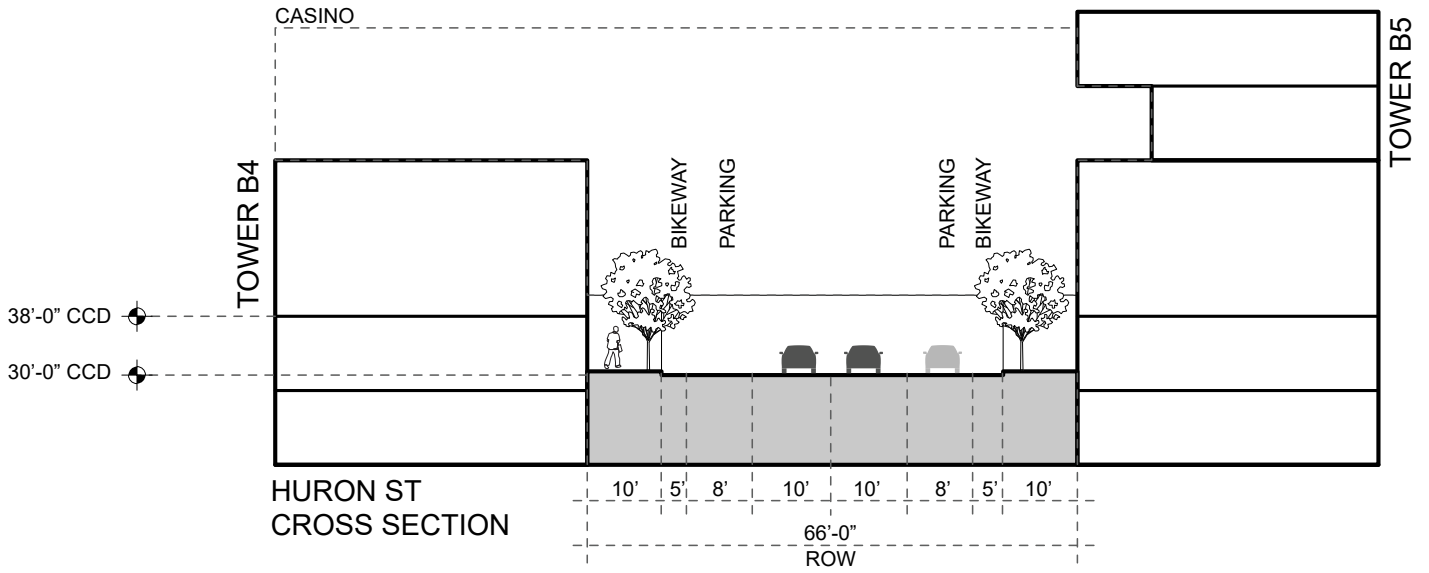
Note: City of Chicago ROW intended to be 66'-0"

RIGHT OF WAY ADJUSTMENT PLAN

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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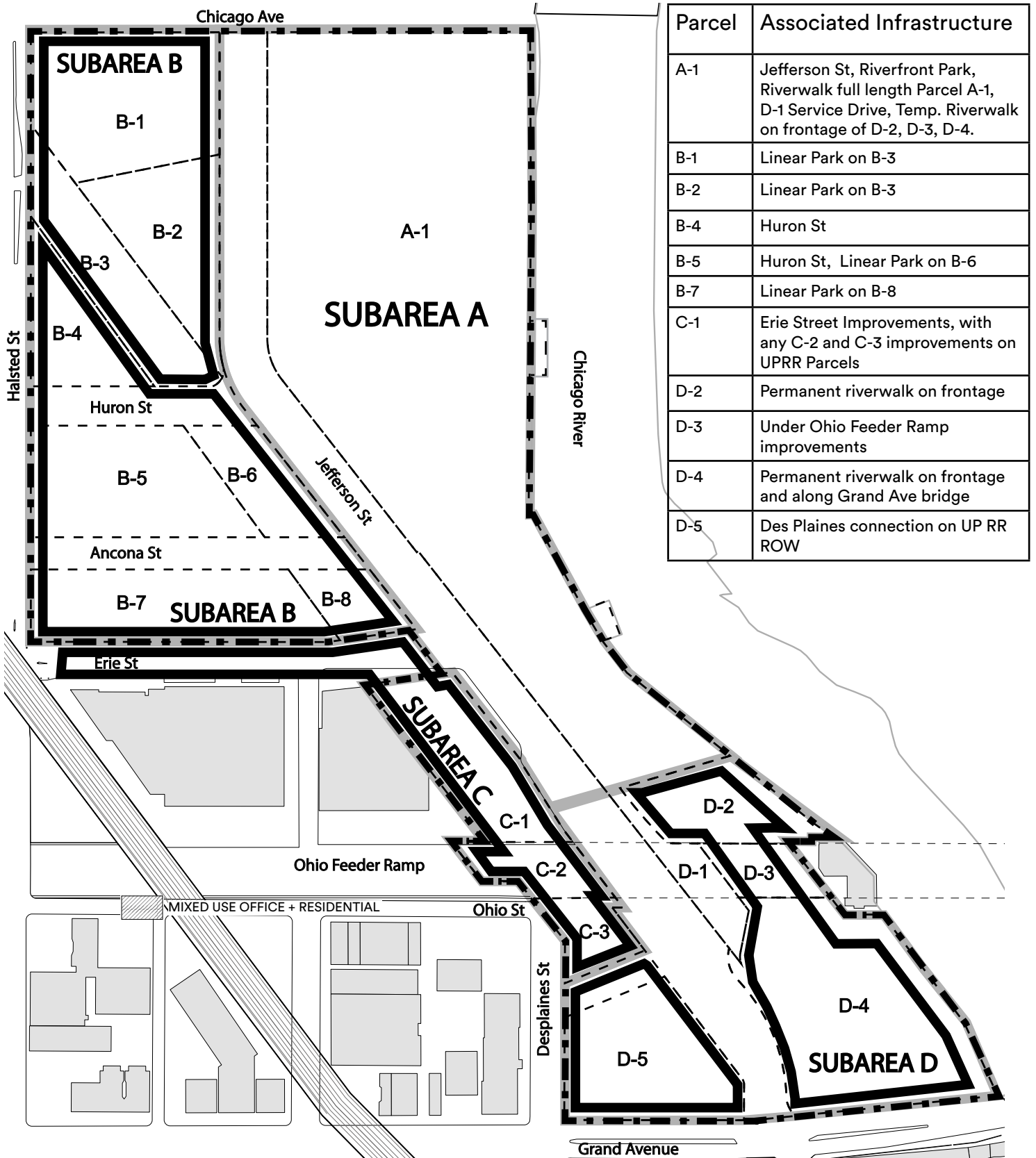
SITE SECTIONS

SCHEMATIC DESIGN FOR AT-GRADE LEVEL IMPROVEMENTS ONLY.



Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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Parcel	Associated Infrastructure
A-1	Jefferson St, Riverfront Park, Riverwalk full length Parcel A-1, D-1 Service Drive, Temp. Riverwalk on frontage of D-2, D-3, D-4.
B-1	Linear Park on B-3
B-2	Linear Park on B-3
B-4	Huron St
B-5	Huron St, Linear Park on B-6
B-7	Linear Park on B-8
C-1	Erie Street Improvements, with any C-2 and C-3 improvements on UPRR Parcels
D-2	Permanent riverwalk on frontage
D-3	Under Ohio Feeder Ramp improvements
D-4	Permanent riverwalk on frontage and along Grand Ave bridge
D-5	Des Plaines connection on UP RR ROW

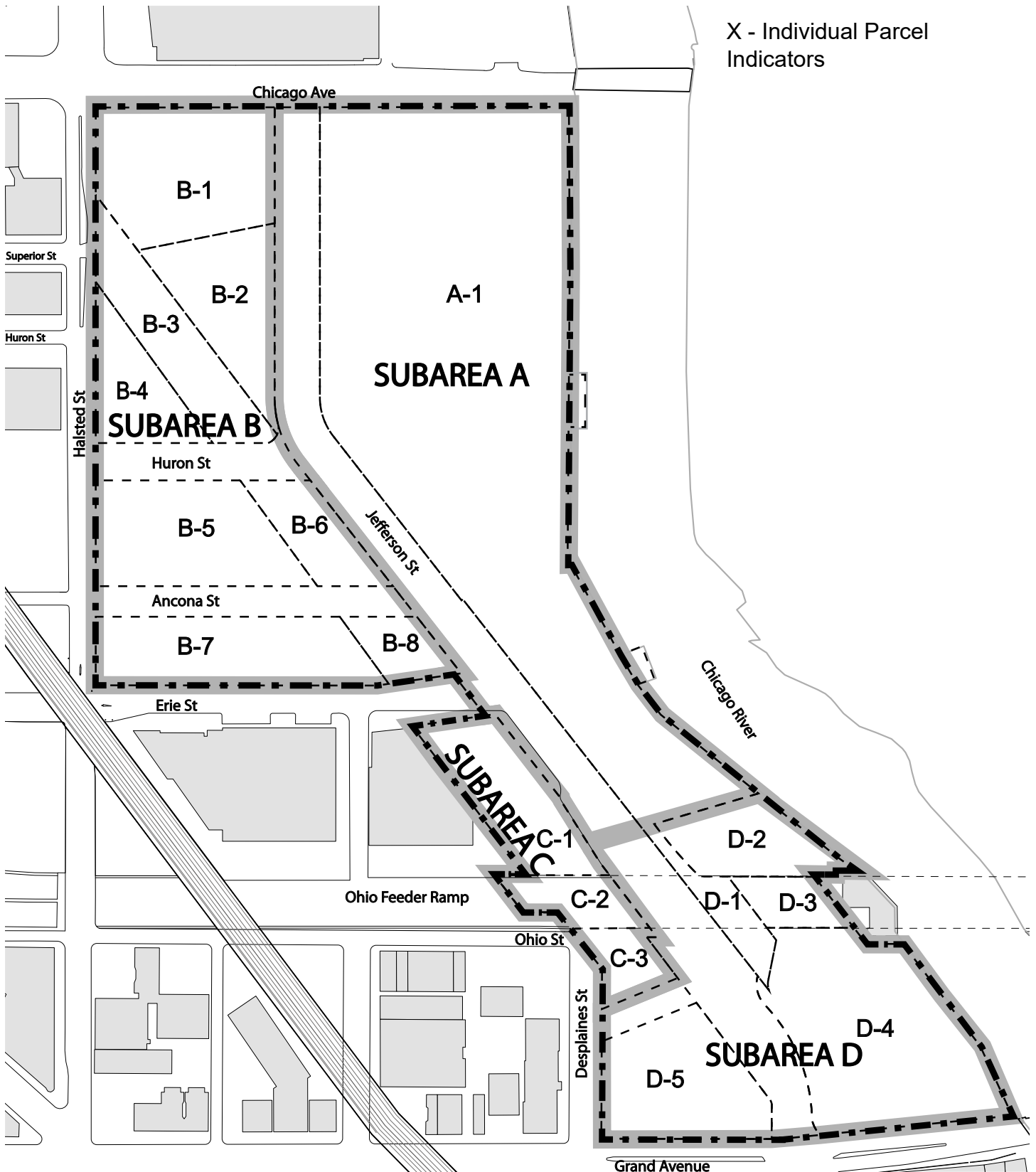
DEVELOPMENT PLAN ASSOCIATED WITH INFRASTRUCTURE REQUIREMENTS

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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X - Individual Parcel Indicators

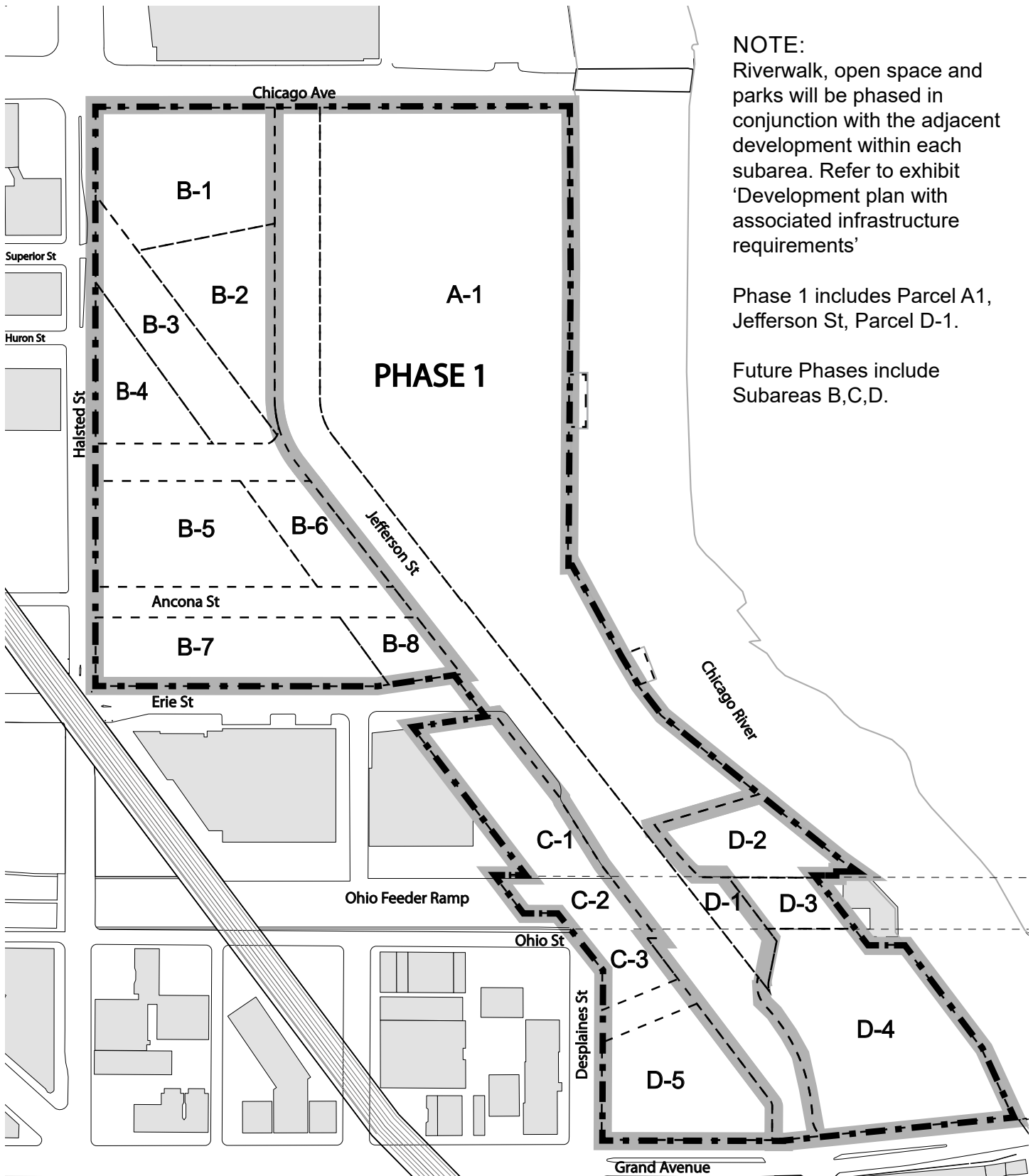


SUB AREA AND PARCELIZATION PLAN

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: September 21, 2022
Plan Commission: December 12, 2022
Updated: May 03, 2023





NOTE:
 Riverwalk, open space and parks will be phased in conjunction with the adjacent development within each subarea. Refer to exhibit 'Development plan with associated infrastructure requirements'

Phase 1 includes Parcel A1, Jefferson St, Parcel D-1.

Future Phases include Subareas B,C,D.

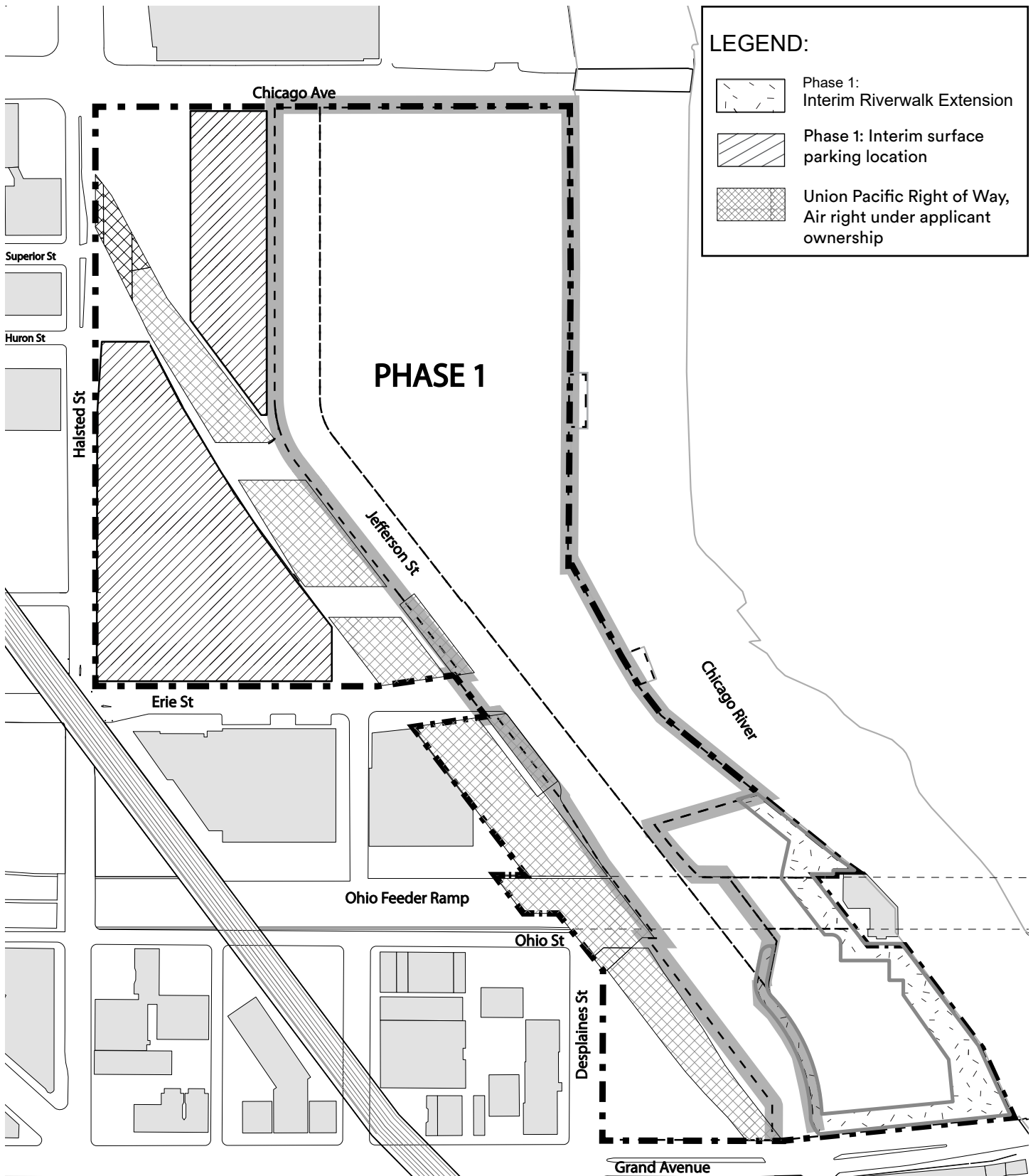
DEVELOPMENT PHASING DIAGRAM

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.




Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **May 03, 2023**

0' 150' 300'

Gensler SCB site V



LEGEND:

-  Phase 1: Interim Riverwalk Extension
-  Phase 1: Interim surface parking location
-  Union Pacific Right of Way, Air right under applicant ownership

Note: Refer to detailed interim parking site plan

PHASE 1: INTERIM SITE PLAN FOR PARKING AND RIVERWALK

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: September 21, 2022
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0' 150' 300'



Gensler SCB site V



TOTAL PROJECT PARKING COUNT

Parking	2,820 Total
Patron Spaces	1,927 spaces
Employee Spaces	478 spaces
Valet Spaces	415 spaces

PHASE 1: INTERIM SITE PLAN FOR PARKING

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

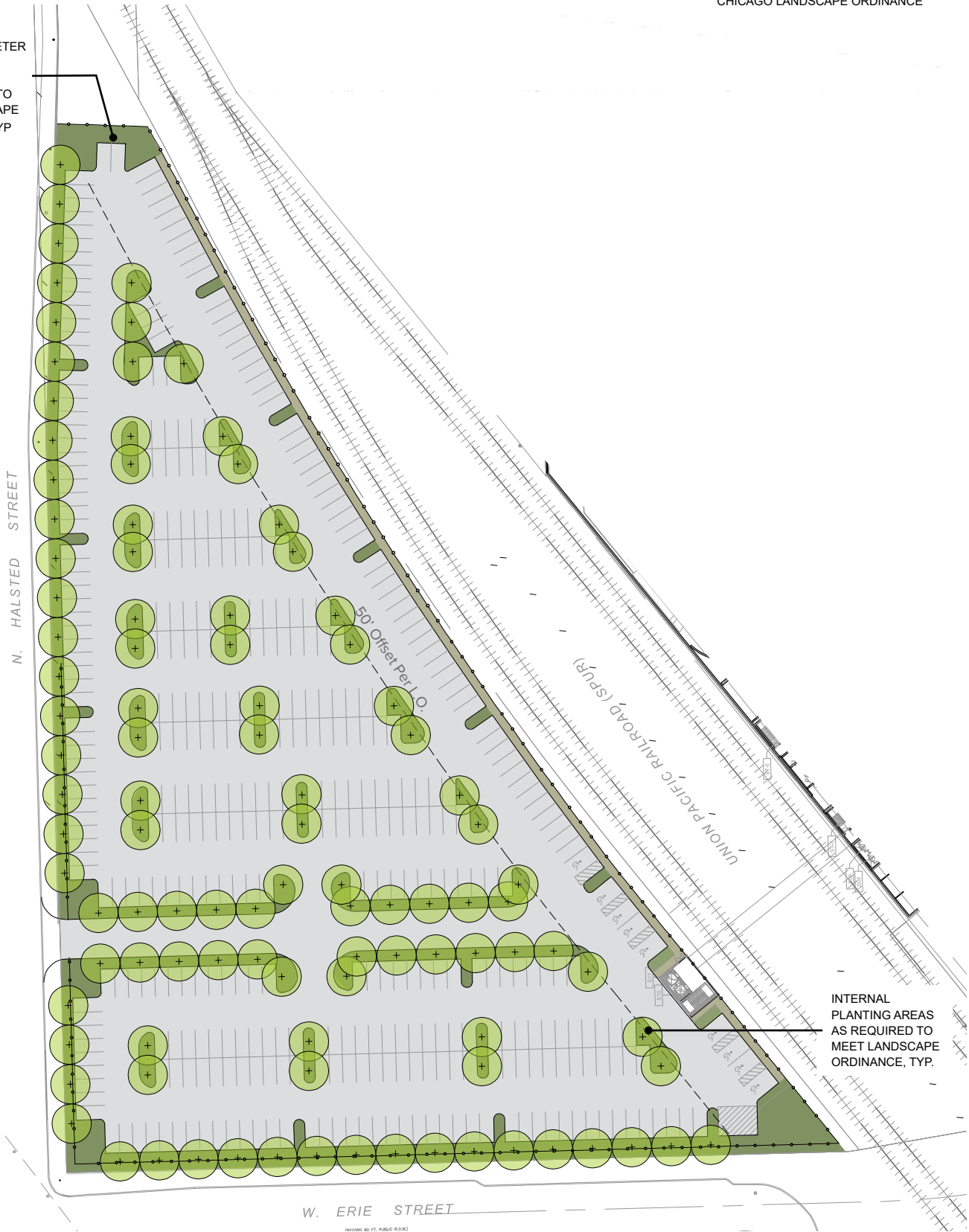
Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **May 03, 2023**

0' 68' 135'



NOTE: INTERIM PARKING LOTS TO COMPLY WITH CITY OF CHICAGO LANDSCAPE ORDINANCE

TREES, PERIMETER LANDSCAPING, AND FENCING AS REQUIRED TO MEET LANDSCAPE ORDINANCE, TYP



INTERNAL PLANTING AREAS AS REQUIRED TO MEET LANDSCAPE ORDINANCE, TYP.

PHASE 1: EMPLOYEE PARKING LOT - SOUTH

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
Plan Commission: **December 12, 2022**
Updated: **May 03, 2023**

0' 45' 90'



TREES, PERIMETER LANDSCAPING, AND FENCING AS REQUIRED TO MEET LANDSCAPE ORDINANCE, TYP.



INTERNAL PLANTING AREAS AS REQUIRED TO MEET LANDSCAPE ORDINANCE, TYP.

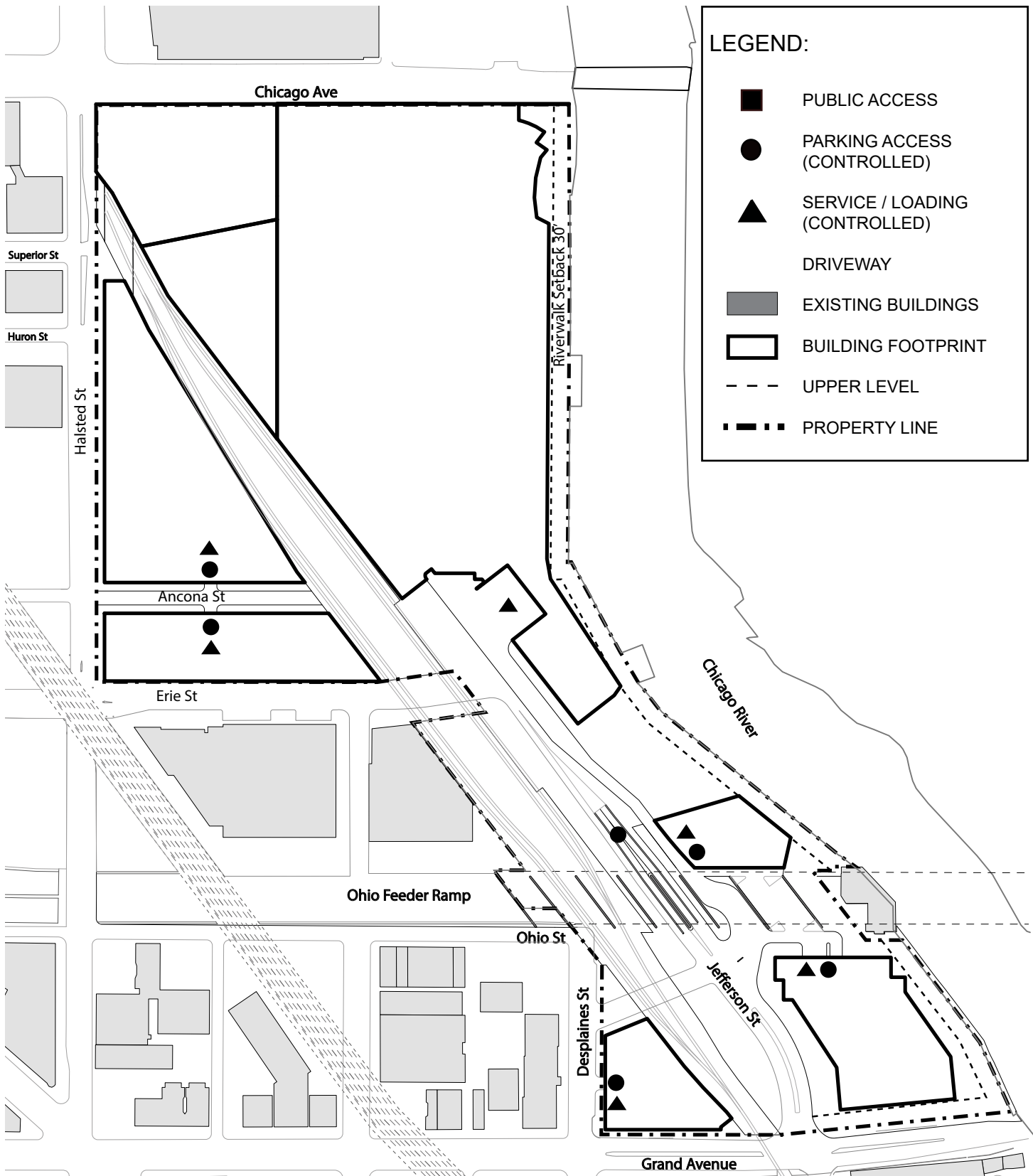
PHASE 1: EMPLOYEE PARKING LOT - NORTH

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
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0' 45' 90'



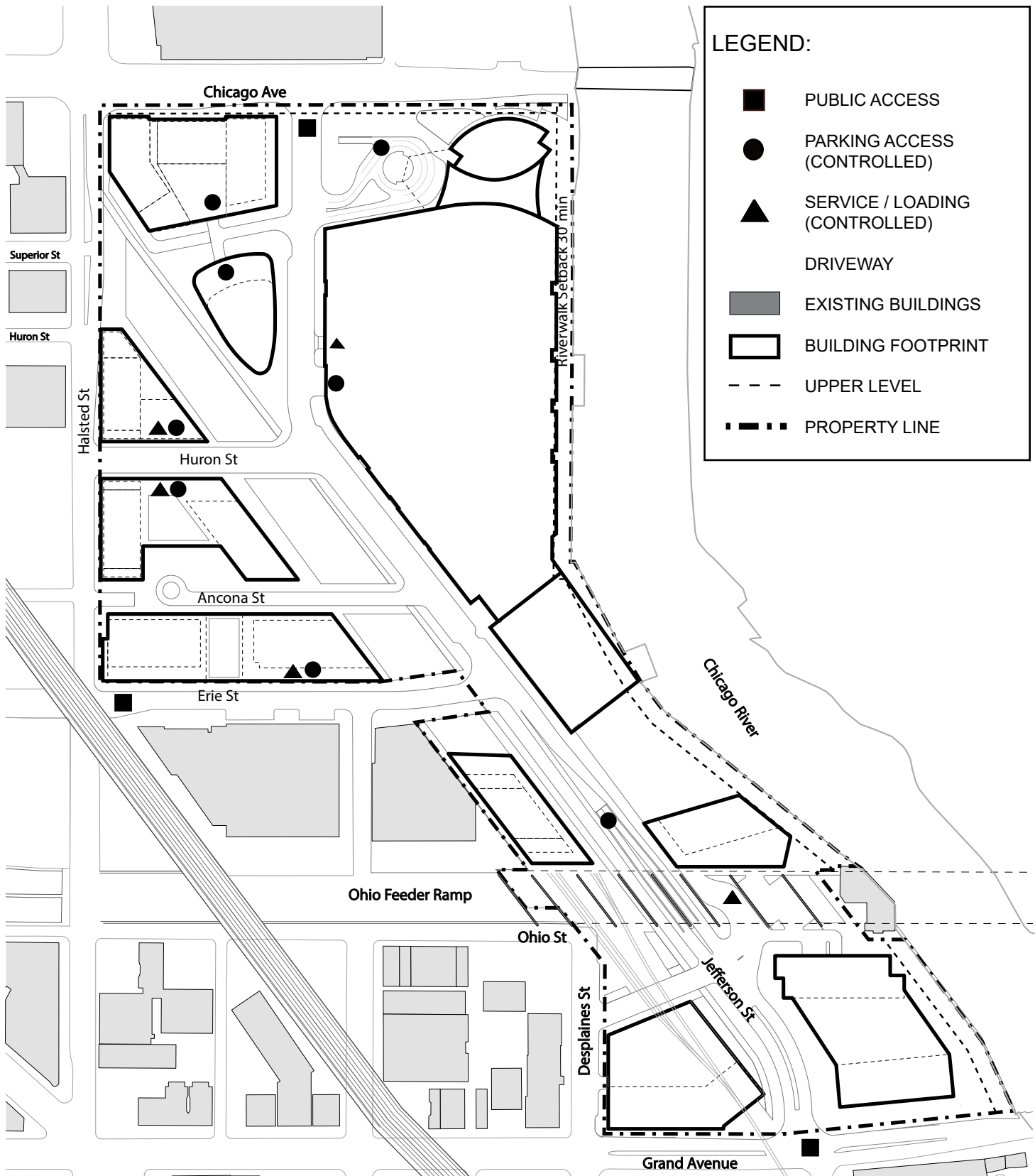


**SITE ACCESS AND LOADING PLAN: LEVEL 01
(+0.0 +30.0 CCD)**

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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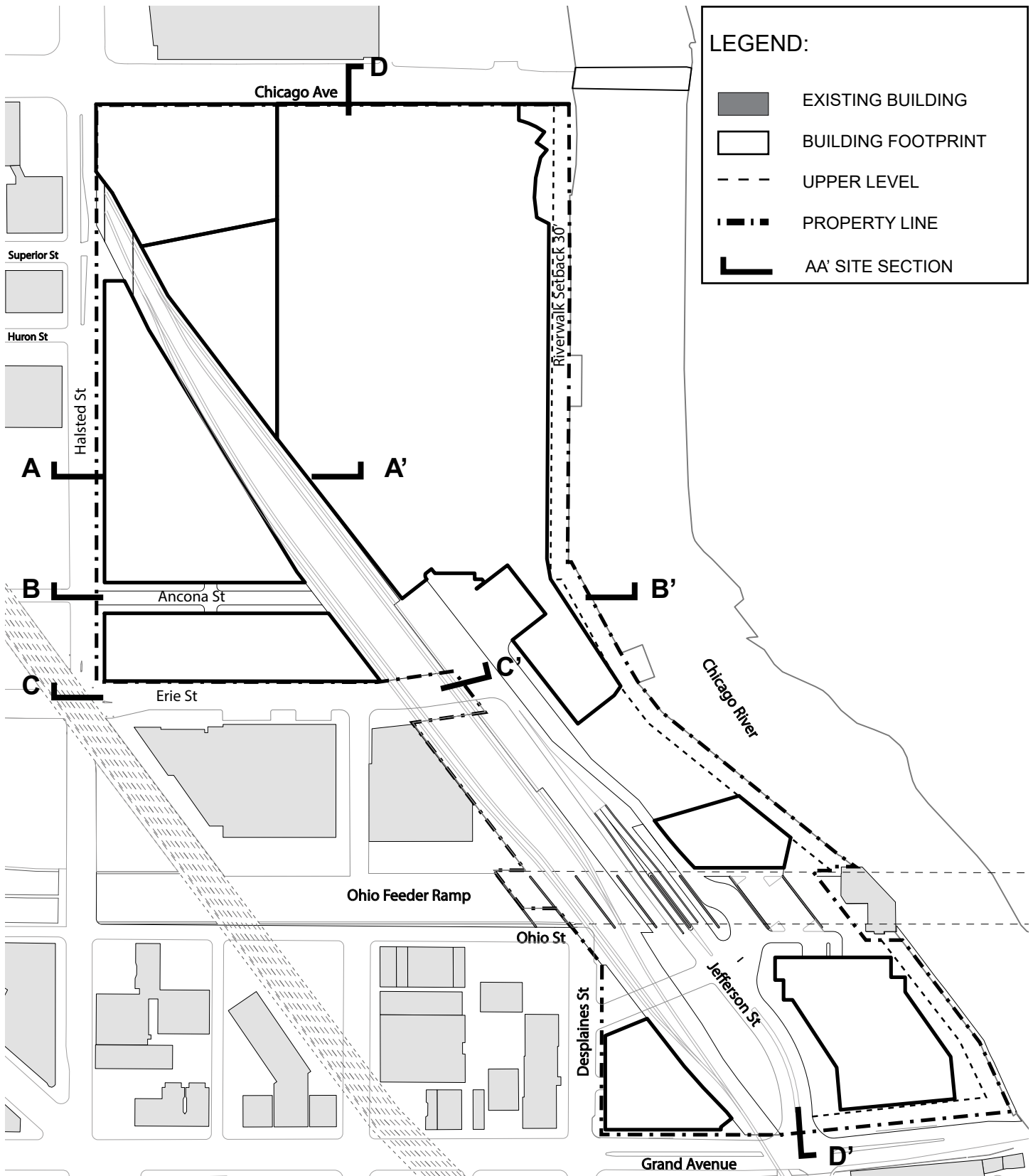


**SITE ACCESS AND LOADING PLAN: LEVEL 02
(+30.0 +45.0 CCD)**

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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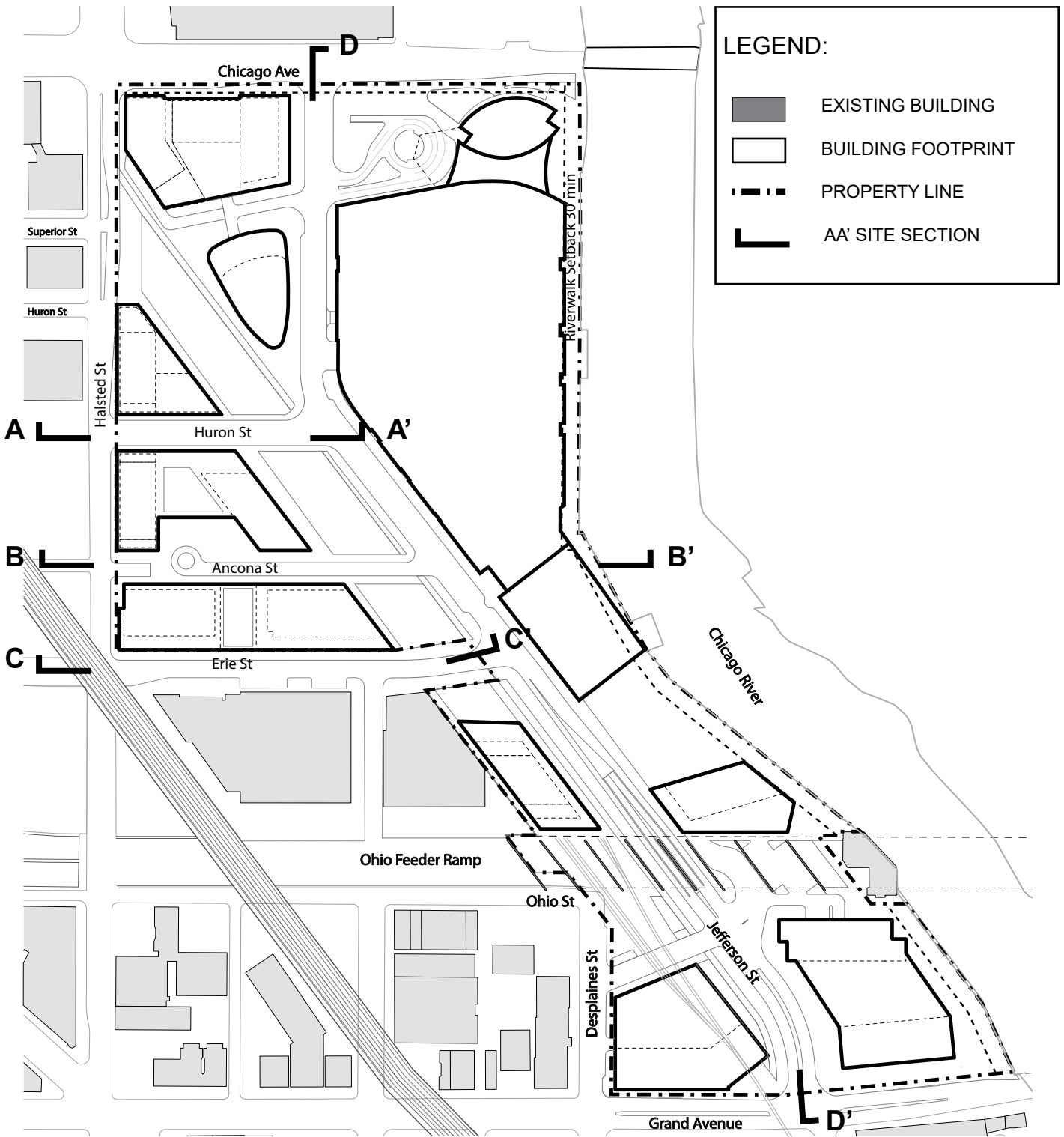


SITE LEVEL PLAN: LEVEL 01 (+0.0 TO +30.0 CCD)

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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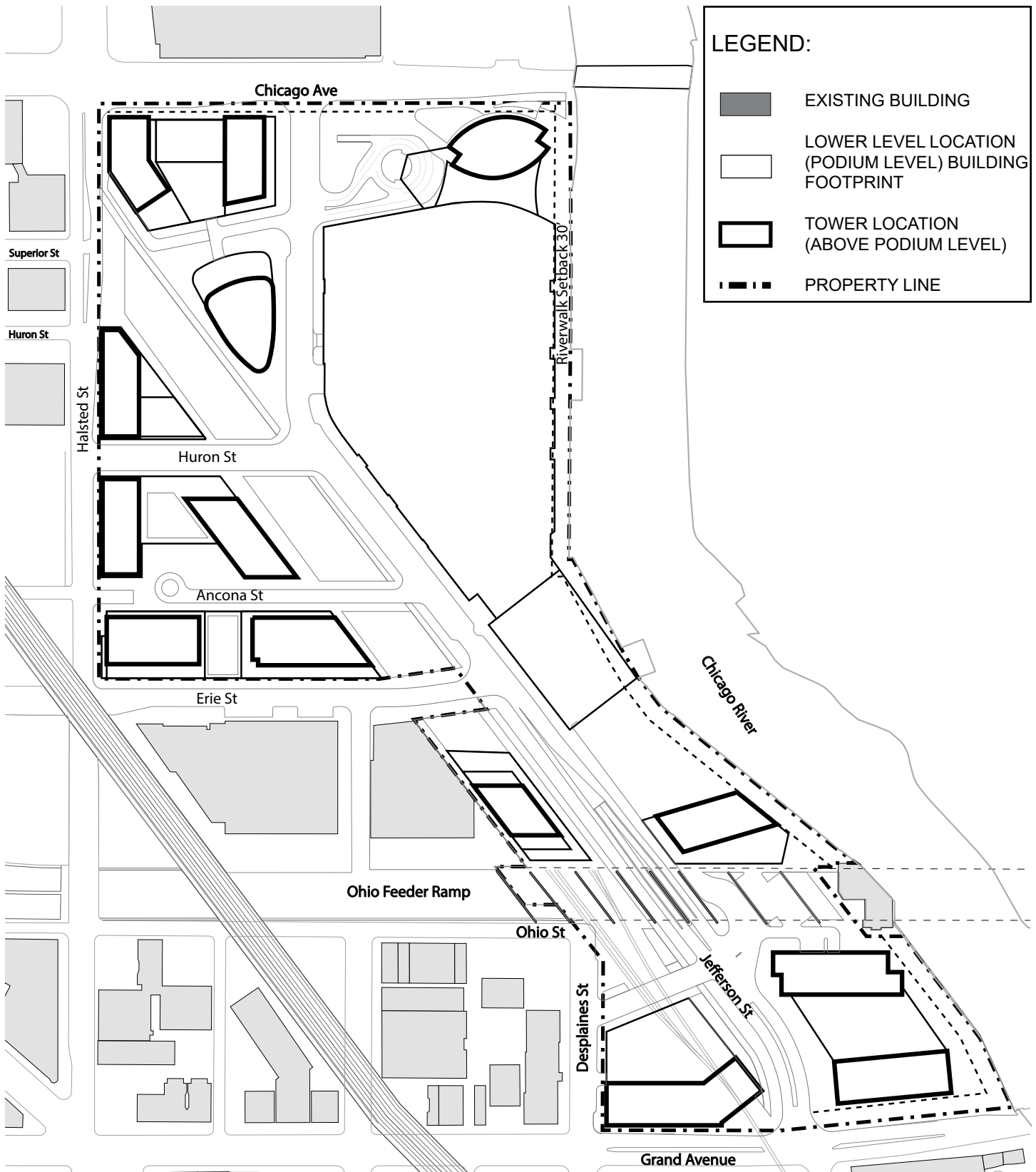


SITE LEVEL PLAN: LEVEL 02 (+38.0 TO +45.0 CCD)

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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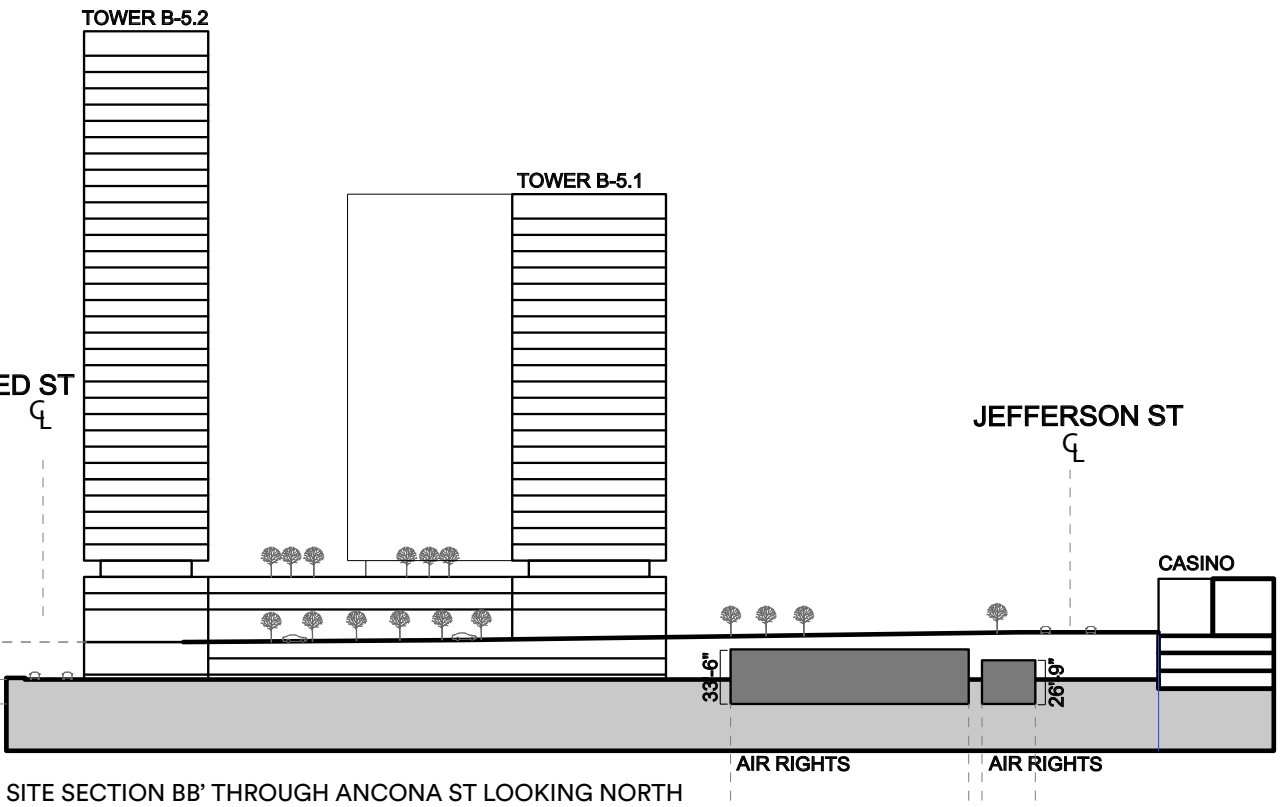
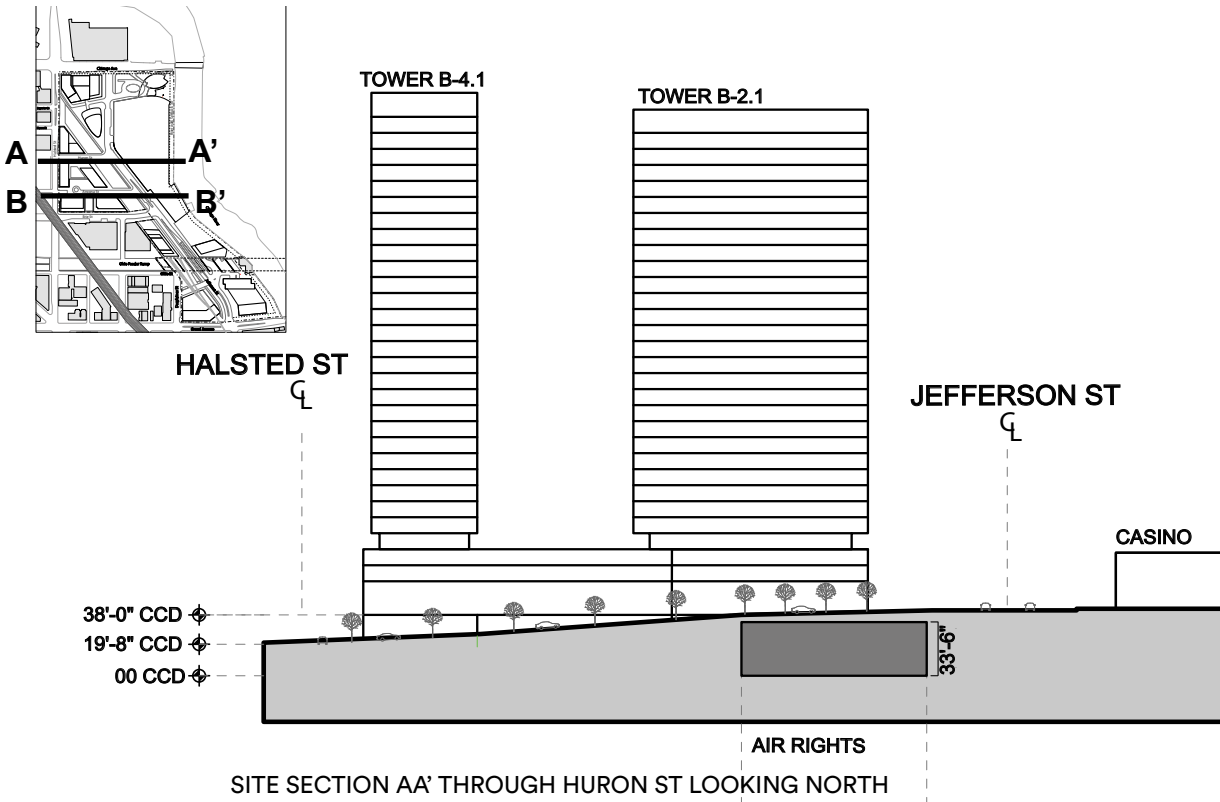


SITE LEVEL PLAN: LEVEL 03 (+45.0 CCD)

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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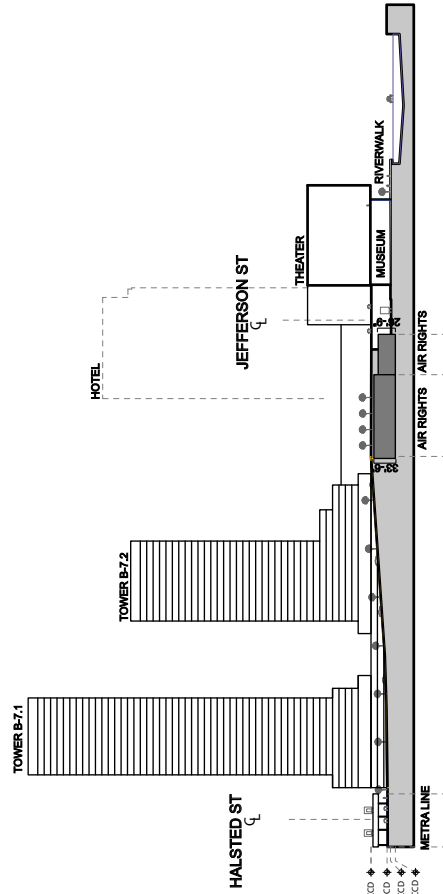
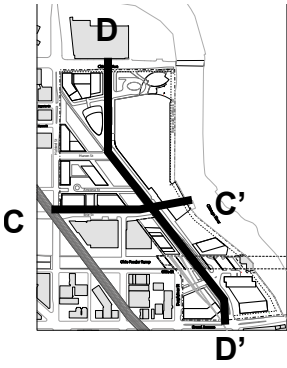


SITE SECTIONS (1 OF 2)

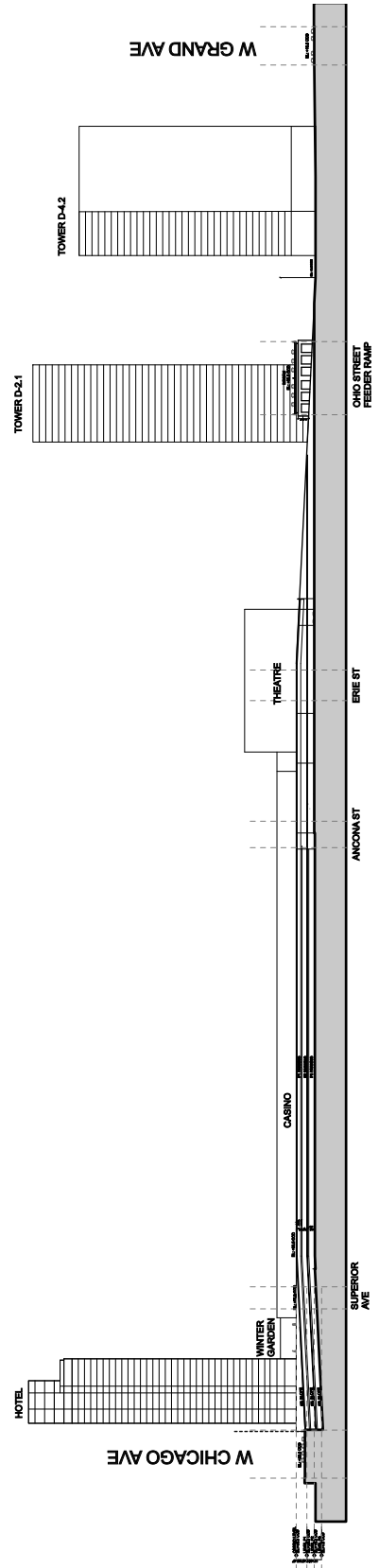
Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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SITE SECTION CC' THROUGH ERIE ST LOOKING NORTH



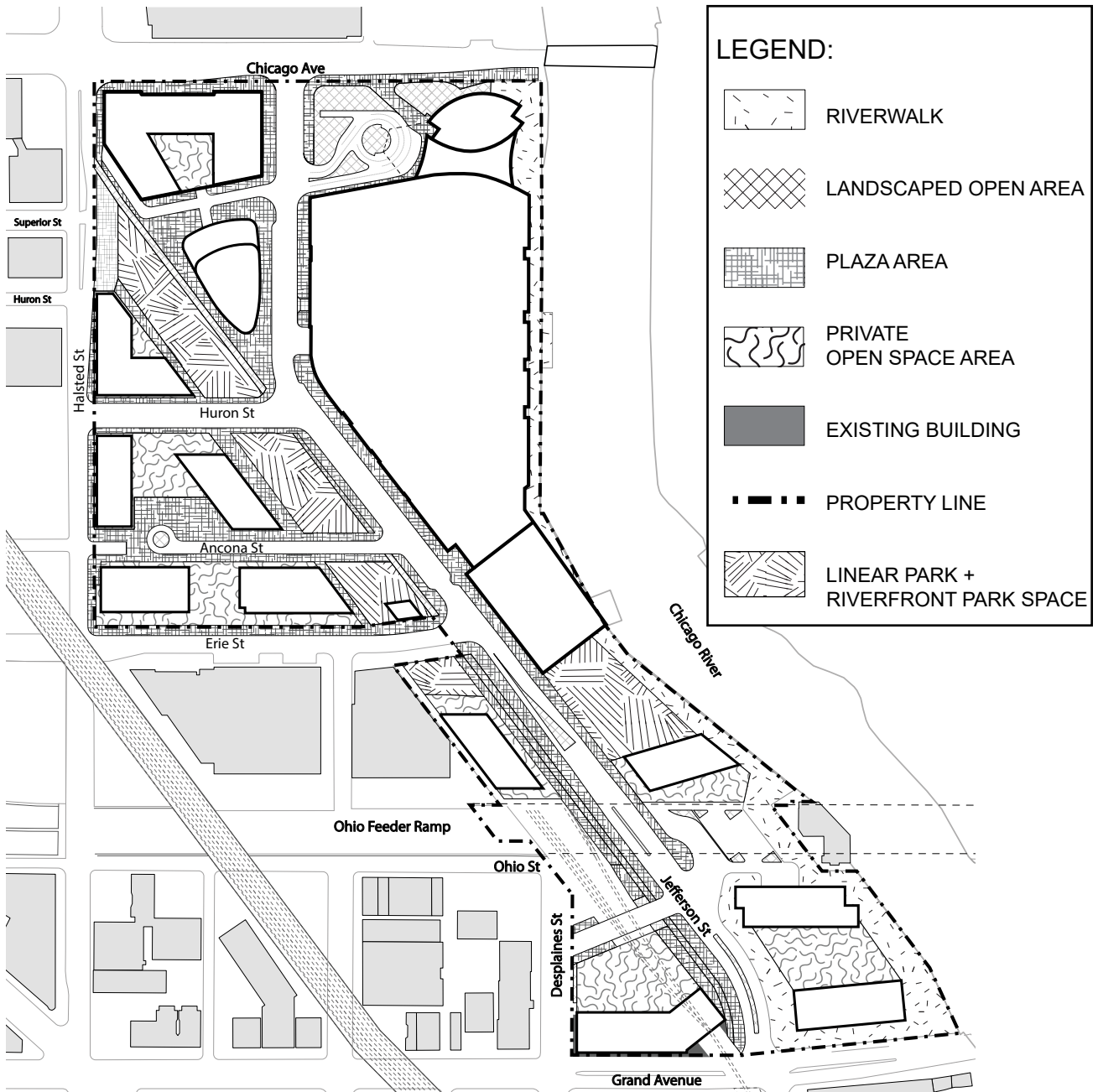
SITE SECTION DD' THROUGH JEFFERSON ST LOOKING EAST

SITE SECTIONS (2 OF 2)

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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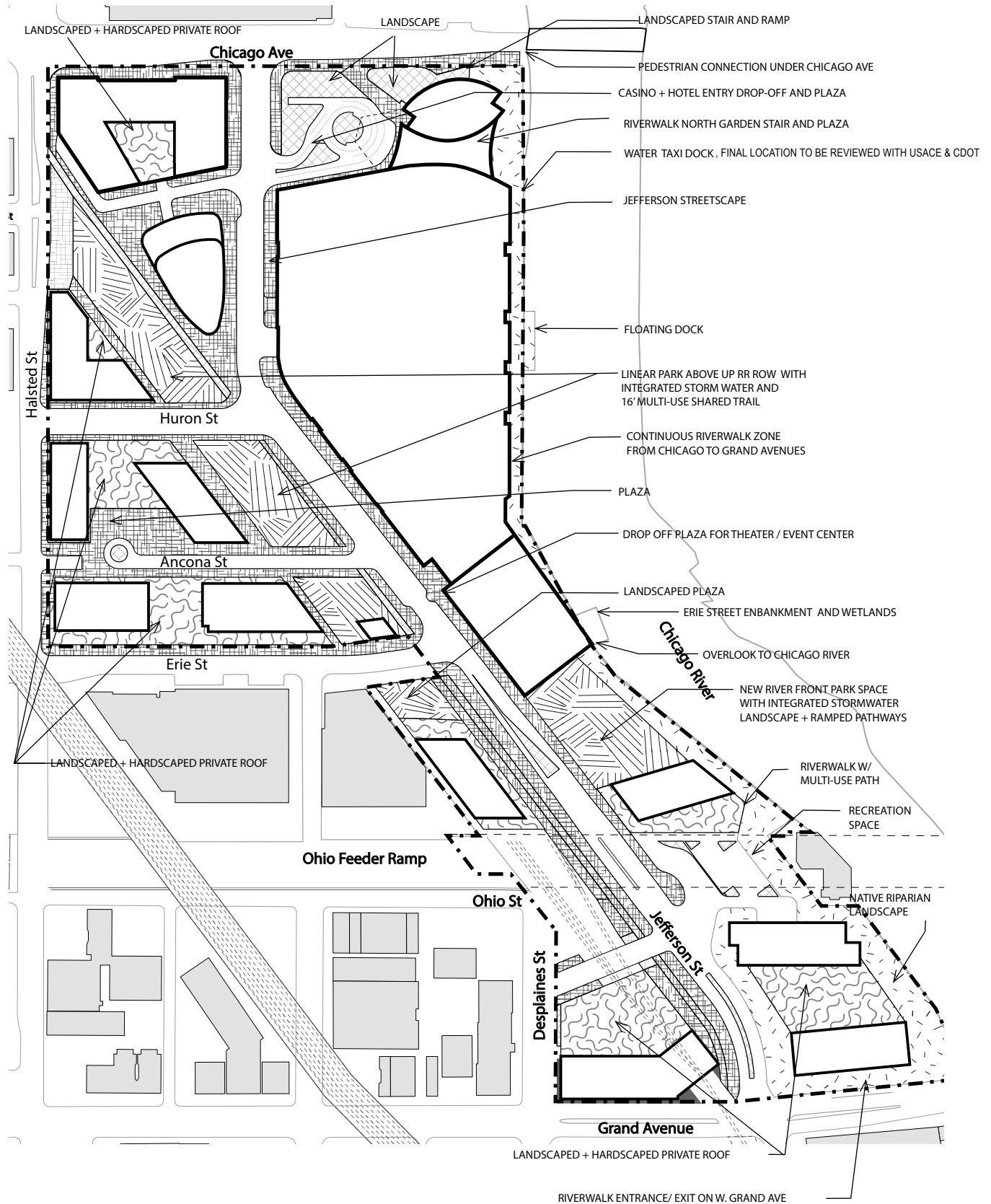
OPEN SPACE CALCULATIONS	GSF	Acres	
Publicly Accessible Open Space			
- Riverwalk (includes natural areas and landscaped stormwater features)	146,550	3.36	31% of Public Open Space
- Landscaped Open Area (includes natural areas and landscaped stormwater features)	77,307	1.77	17% of Public Open Space
- Plaza Area/Elevated Plaza (includes natural areas and landscaped stormwater features)	242,573	5.57	52% of Public Open Space
Subtotal: Publicly Accessible Open Space	466,430	10.71	36% of Total Site Area
Subtotal: Private and Common Open Space	123,678	2.84	9% of Total Site Area
Total Open Space (Public & Private Space)	590,108	13.55	45% of Total Site Area

PUBLIC SPACE OPEN PLAN

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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 Plan Commission: **December 12, 2022**
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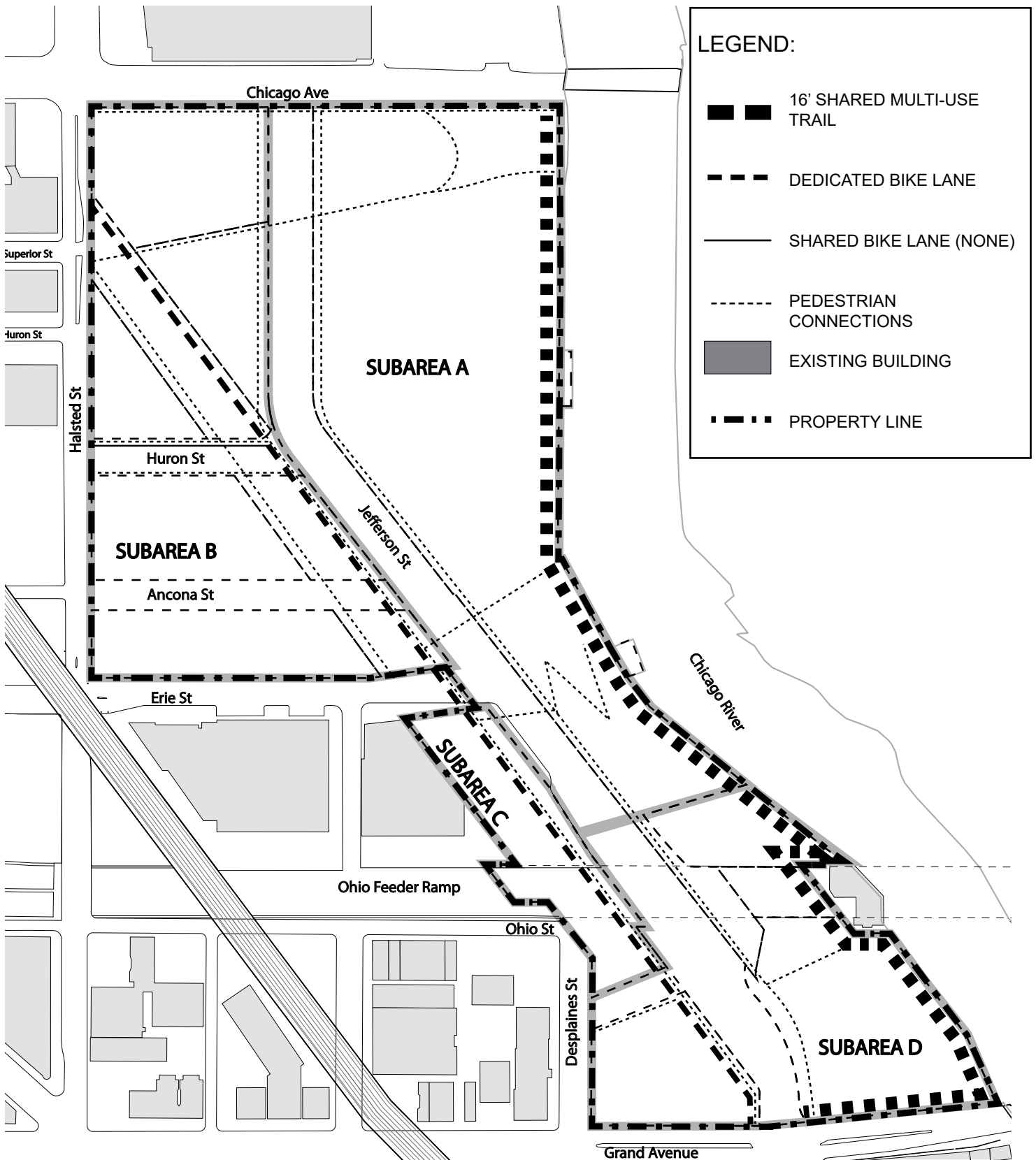
GENERALIZED LANDSCAPE PLAN

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
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0' 150' 300'



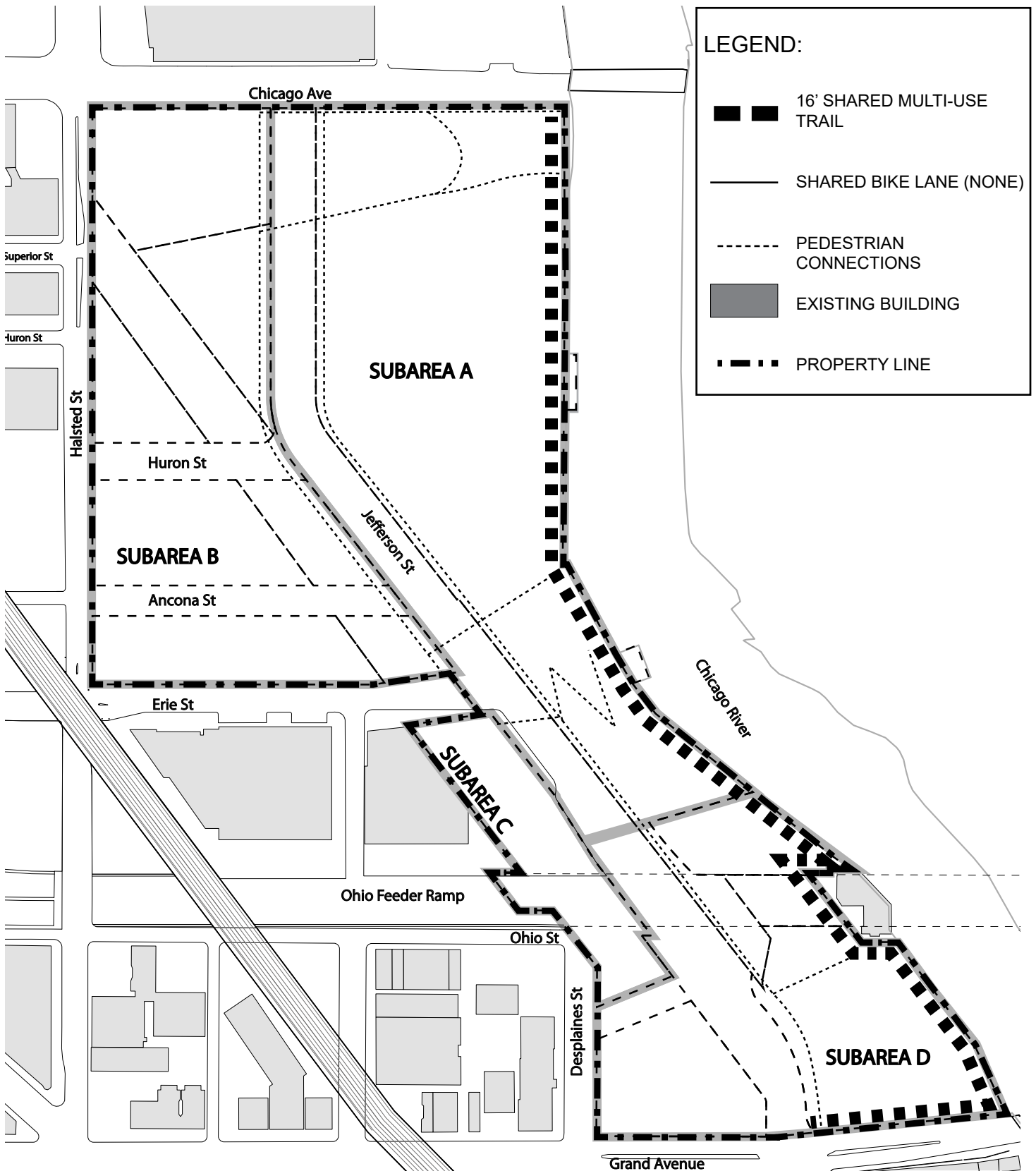


**BIKE AND PEDESTRIAN CONNECTIVITY PLAN
- ULTIMATE BUILD OUT**

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **May 03, 2023**



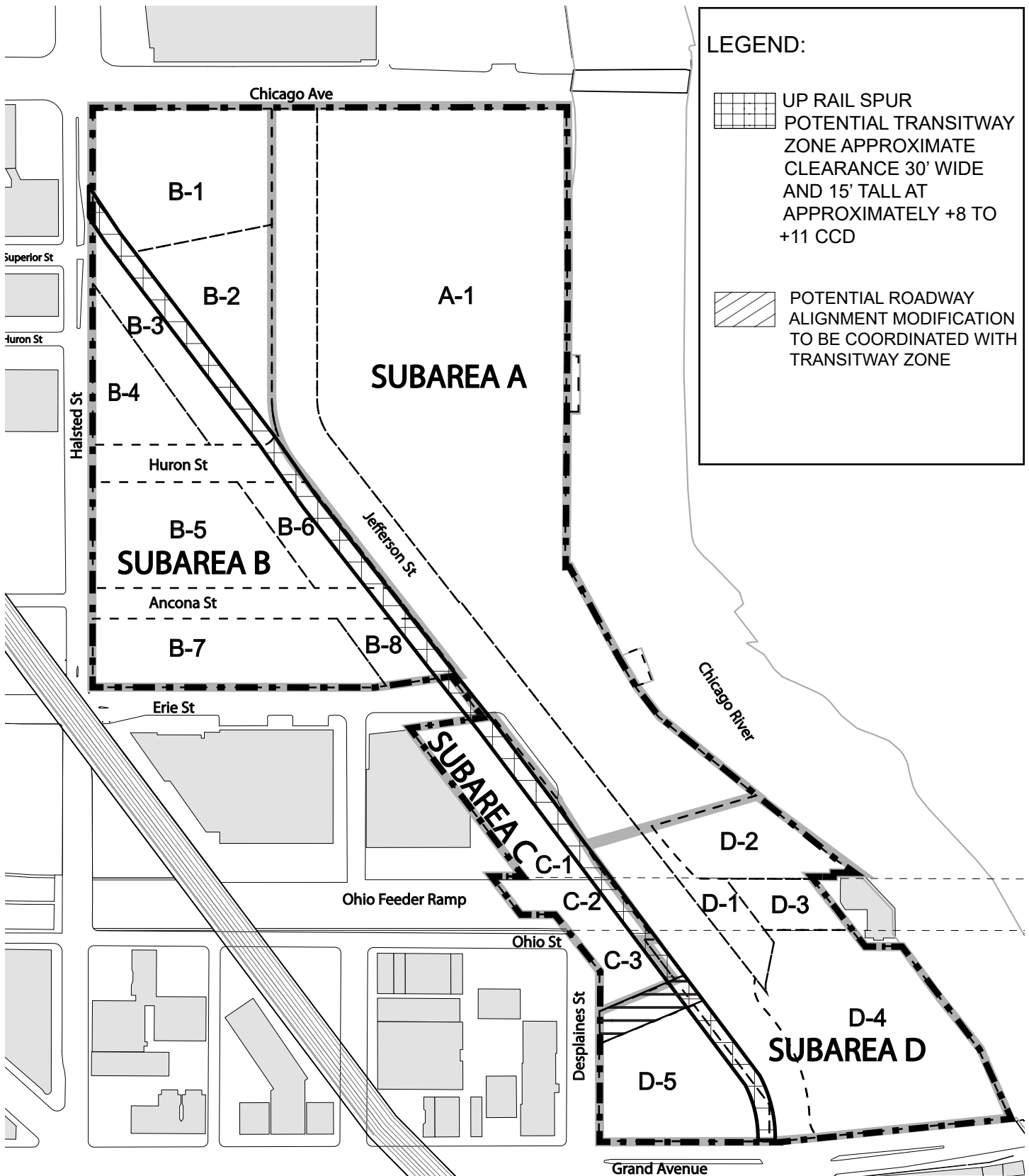


PHASE 1: BIKE AND PEDESTRIAN CONNECTIVITY PLAN

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **May 03, 2023**





TRANSITWAY ACCOMODATION PLAN

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
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All development within this planned development must substantially comply with the Design Guidelines of the North Branch Framework adopted by the Chicago Plan Commission on May 18, 2017, or as amended, the Chicago River Corridor Design Guidelines and Standards, or as amended, and with the design standards and guidelines outlined in the Zoning Ordinance, Section 17-8-0900 Standards and Guidelines. The guidelines listed below provide additional standards for buildings and public spaces to complement the specific context of this planned development:

GENERAL DEVELOPMENT GUIDELINES

- Building siting and base design shall define the street walls along Jefferson Street, Chicago Avenue, and Halsted Street, with setbacks allowed for plazas and open spaces.
- Jefferson Street shall be developed as a central street with wide sidewalks, street trees and landscaping to promote stormwater management.
- Active ground floor uses shall be integral to buildings along the west side of Jefferson Street, Chicago Avenue, and Halsted Street, and shall include active uses such as retail, lobbies, or office space.
- Active ground-floor uses shall have direct access to publicly-accessible open spaces and the riverwalk where feasible.
- Buildings at the corner of Halsted Street and Chicago Avenue shall incorporate massing, siting, and façade designs to express and reinforce a district gateway.
- In general, vehicular pick-up and drop-off shall be oriented away from Chicago Avenue and Grand Avenue. Drop off zones on Halsted to be coordinated with CDOT and CTA. Parking and service entrances for the casino and other buildings along Jefferson Street will be provided via access drives or driveways. Pick-up and drop-off locations within the public way may be incorporated only where curbside space exists for that use.
- Curb-cuts should not exceed 20' in width and parking entries should be integrated with the building's architecture through the use of similar materials, patterns, textures, and color. Curb cuts shall be coordinated with CDOT and may be widened within CDOT's standards if required to provide adequate access for service, loading, and shuttle services.
- To preserve access to light, a minimum separation of 40' between towers is required. Where site constraints permit, towers will be oriented to maximize passive cooling, natural lighting, and energy efficiency.
- Undeveloped parcels may support interim uses including, but not limited to recreational open space, dog friendly areas, and surface parking. Interim use plan improvements and time frame will require review and approval by DPD.

BUILDING ENVELOPE DESIGN GUIDELINES

- Building designs shall achieve a varied and distinctive skyline.
- Mechanical penthouses and any upper level parking floors shall be screened with high-quality architectural elements consistent with the overall building facade.
- Parking levels shall be screened adequately to obscure car headlights and sound from neighboring buildings.
- Podium and tower designs shall relate to each other to provide a cohesive expression.
- Podium roofs will incorporate active amenity decks and landscaping as appropriate to building uses.
- Balconies and any required ventilation shall be integrated within the design of the building façade.
- Tenant signage for each building should be considered as part of the façade design to ensure consistency of placement, size, materials, and method of illumination.

MATERIAL GUIDELINES FOR BUILDINGS

- Buildings will not be clad with low quality materials and materials with low aesthetic value such as Exterior Insulation and Finish System (EIFS), exterior exposed Concrete Masonry Units (CMU), thin brick, or residential-type vinyl and metal siding.
- Buildings will employ architectural materials consistent with contemporary building practices, such as high quality wall systems in glass, metal, masonry, high-quality architectural concrete, or hardwood. Glazing shall not be highly reflective or mirrored and be bird friendly design.
- Building envelopes will support environmentally responsible design by reducing heat loads, improving energy efficiency, maximizing occupant comfort, and using sustainable materials.
- Podium and ground-floor level materials and design facing the streets, publicly accessible open spaces, and the riverwalk will be detailed to enhance the pedestrian environment and will be complimentary to the context.
- Ground-floor facades will have a high percentage of clear / un-tinted glazing along the primary street frontages.

LANDSCAPE GENERAL GUIDELINES

- A riverfront publicly accessible park will connect directly to the riverwalk and include gathering places, a lawn, passive play spaces, native landscaping, opportunities for direct viewing of river, and stormwater infiltration.
- Landscaped stairs and ramps will connect the riverwalk and Jefferson Street, and provide view corridors from Halsted Street to the river. Programming will include a variety of uses such as seating, play areas, a location for public art, and a dog park.
- The space under the Ohio Feeder Ramp will be activated with recreational uses that connect to the riverwalk park spaces.
- Publicly accessible open spaces will be designed to the applicable standards of Chicago Park District Parks.
- The riverwalk will connect to the north side of Chicago Avenue using space provided by CDOT under the new Chicago Avenue Bridge.
- The riverwalk will connect to the street level intersection of Grand Avenue and Jefferson Street, and not preclude space for future Riverwalk connection under Grand Avenue bridge.
- Landscaping throughout will incorporate stormwater management best practices to detain, clean, and reduce the volume of stormwater discharge.
- Landscaped areas will enhance local habitat for wildlife through use native plantings.
- In key locations, floating wetlands can be located along the riverwalk where feasible.

DESIGN GUIDELINES

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
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 Updated: **May 03, 2023**



ROOF AREAS

Theater Roof	37,256 SF
Casino Roof	242,068 SF
Hotel Roof	14,141 SF
TOTAL ROOF AREA	293,465 SF

ROOF AREA USED FOR MEP 72,815.56 SF

GREEN ROOF AREA 88,259.78 SF

40% OF LEFTOVER AREA 88,259.78 SF

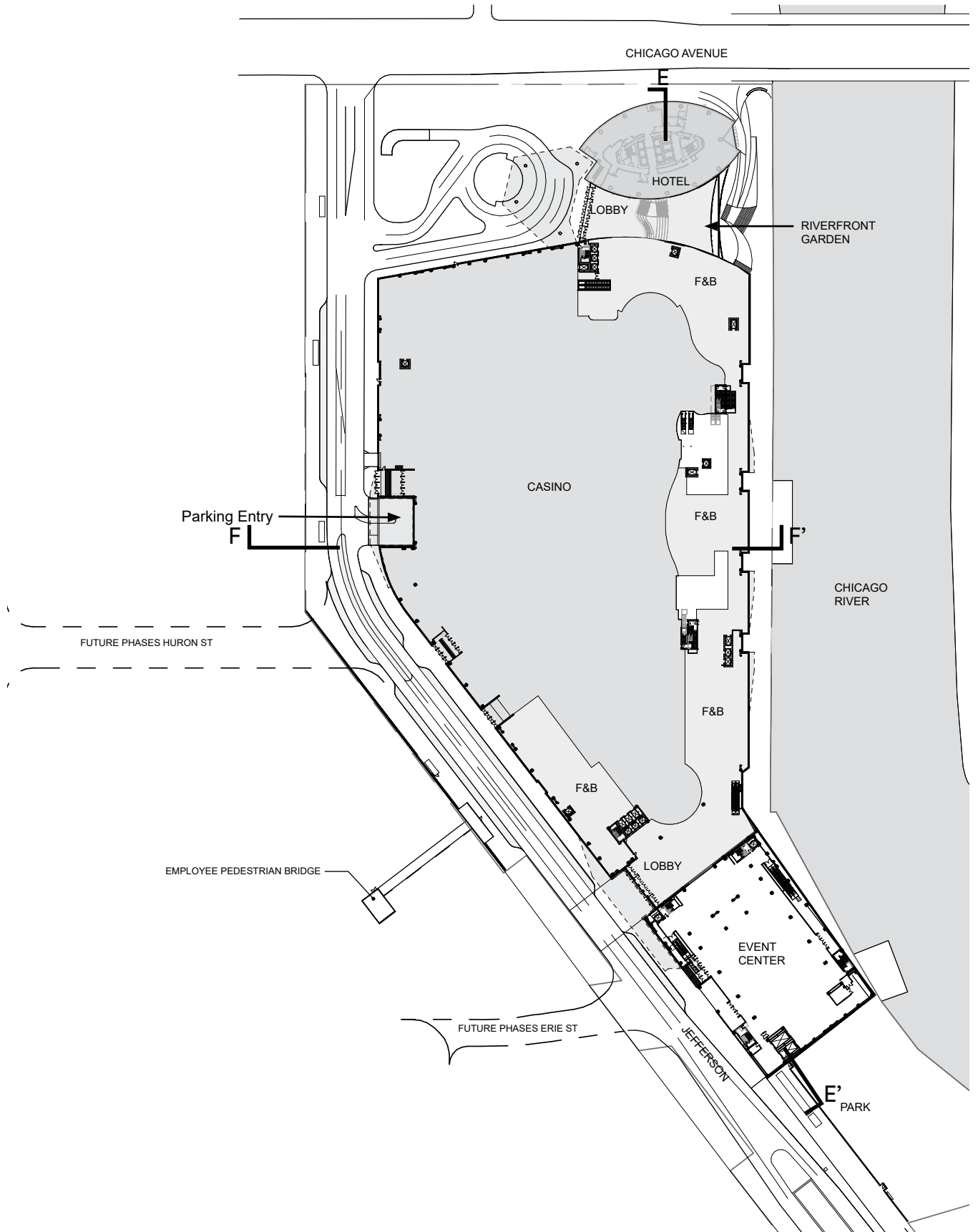
PHASE 1: GREEN ROOF PLAN

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE...; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **May 03, 2023**

SCALE: 1" = 160'- 0"
 0' 95' 190'





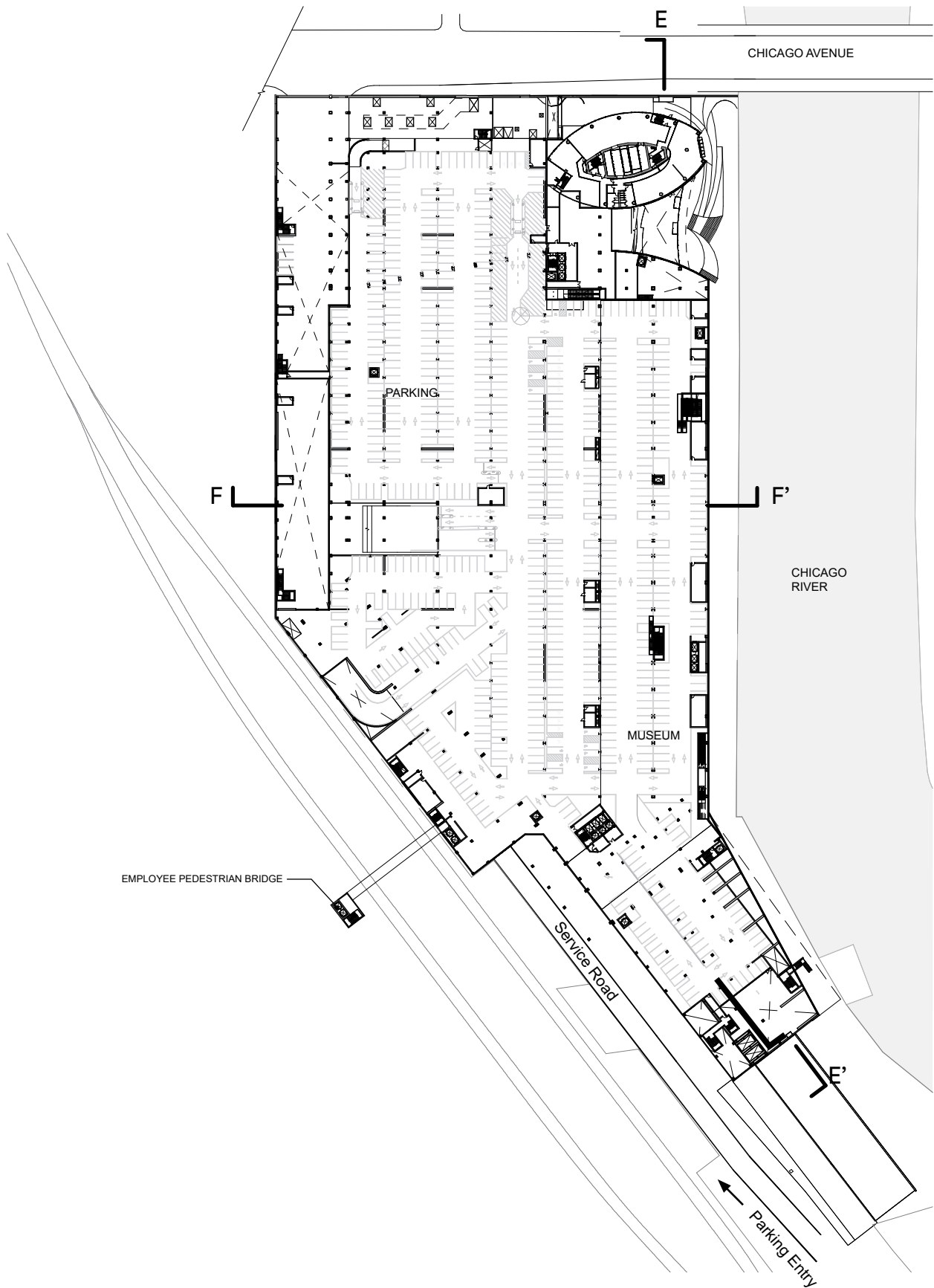
PHASE 1: FLOOR PLAN - L1

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE...; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **May 03, 2023**

SCALE: 1" = 160'-0"
 0' 95' 190'





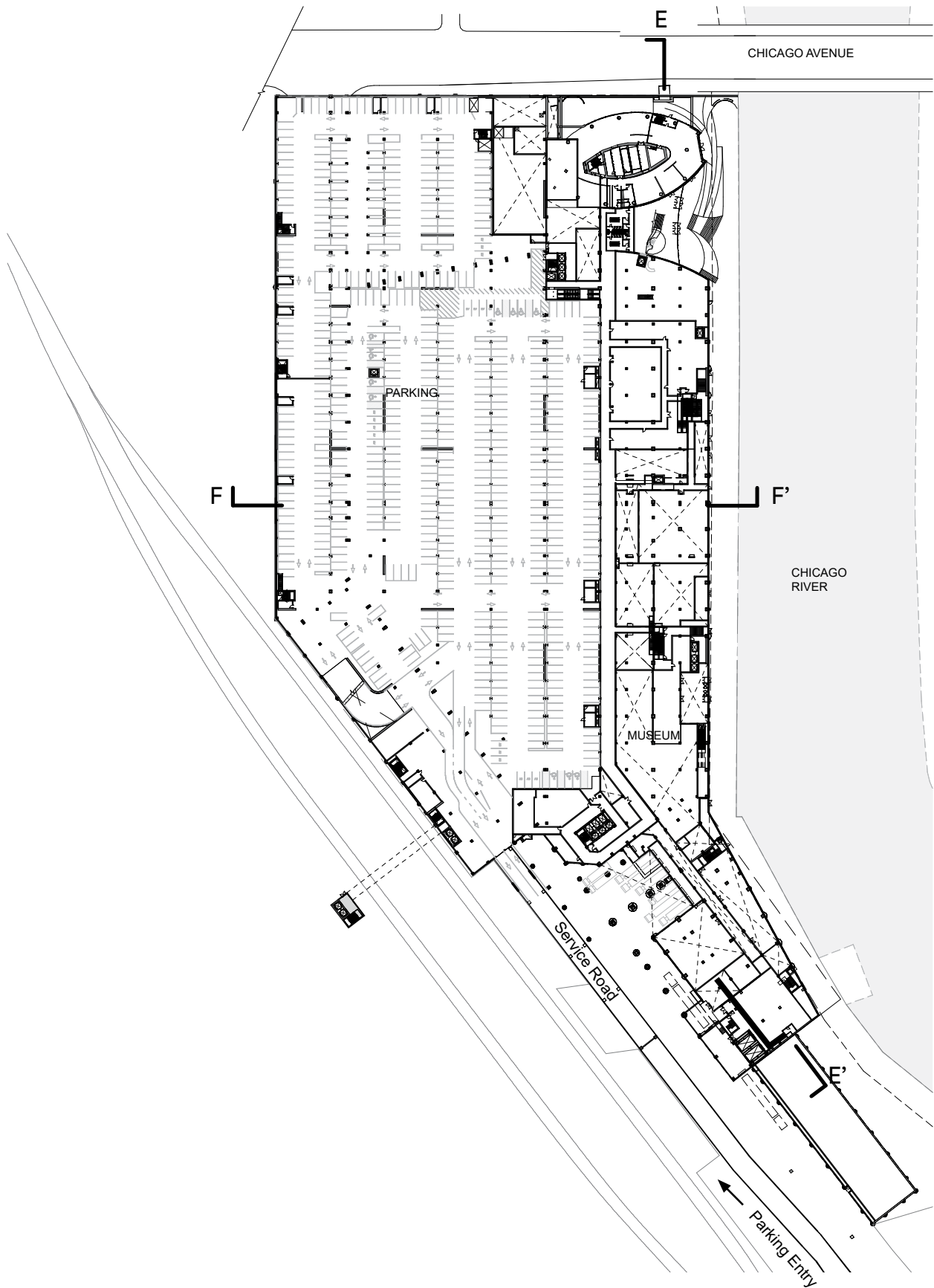
PHASE 1: FLOOR PLAN - P1

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
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SCALE: 1" = 160'-0"
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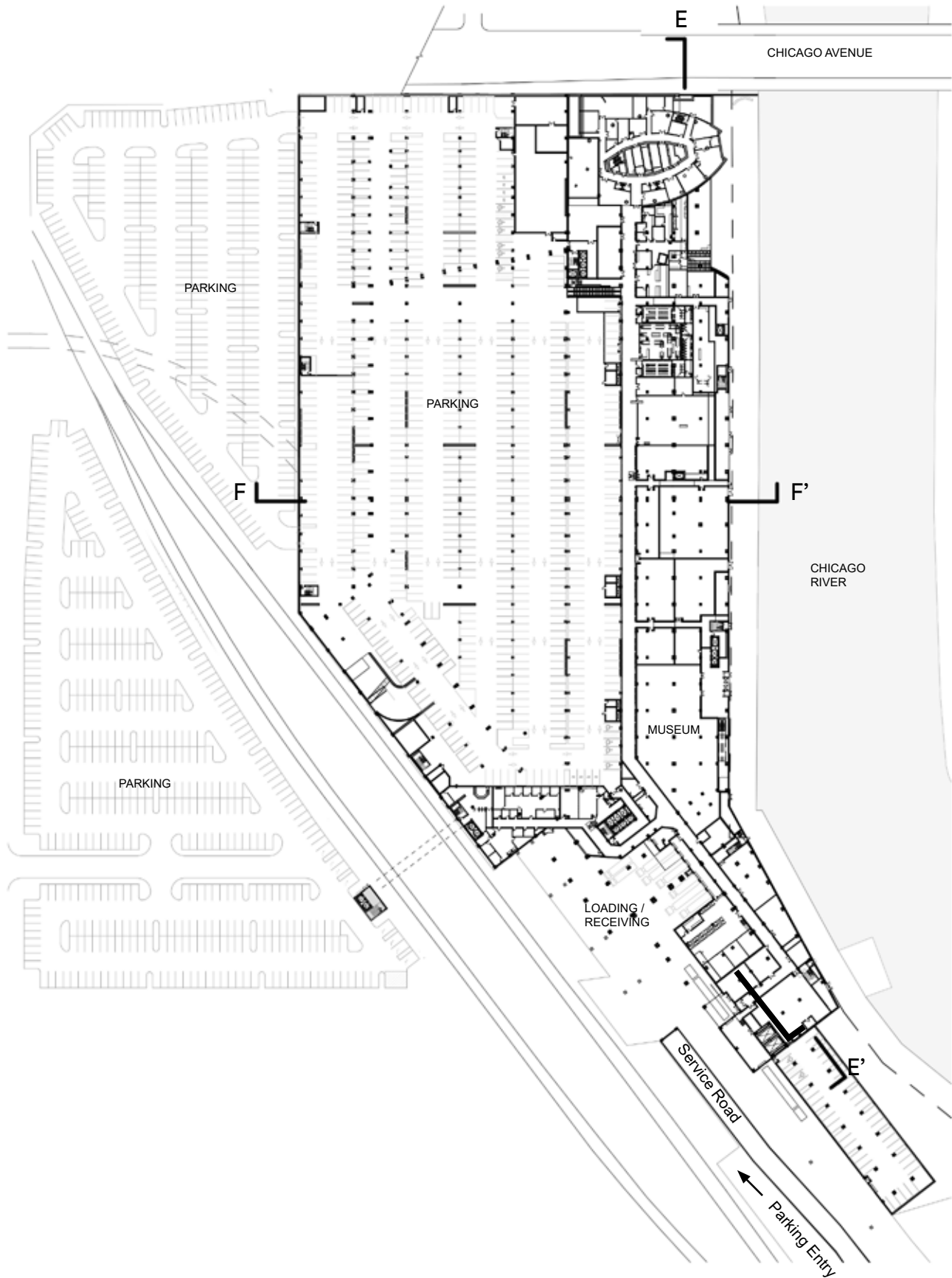
PHASE 1: FLOOR PLAN - P2

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **May 03, 2023**

SCALE: 1" = 160'-0"
 0' 95' 190'





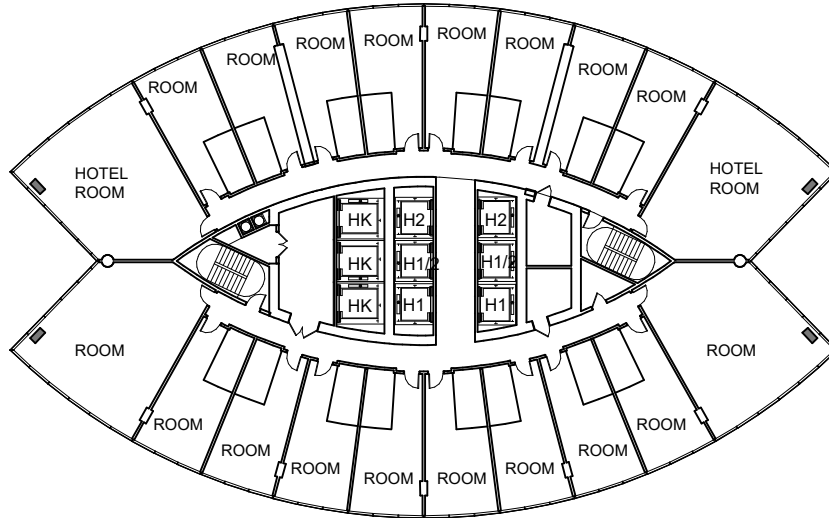
PHASE 1: FLOOR PLAN - P3

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

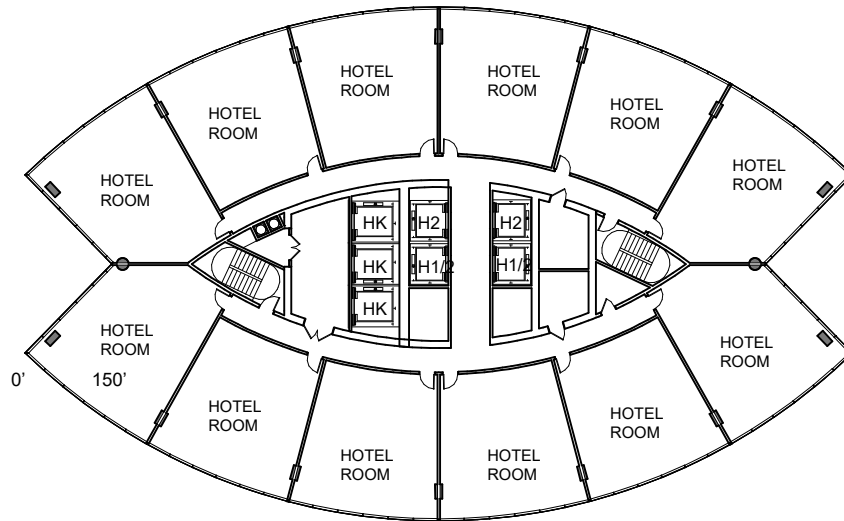
Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **May 03, 2023**

SCALE: 1" = 160'-0"
 0' 95' 190'





HOTEL FLOOR PLAN 1



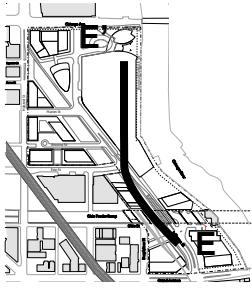
HOTEL FLOOR PLAN 2

TYPICAL HOTEL PLANS

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

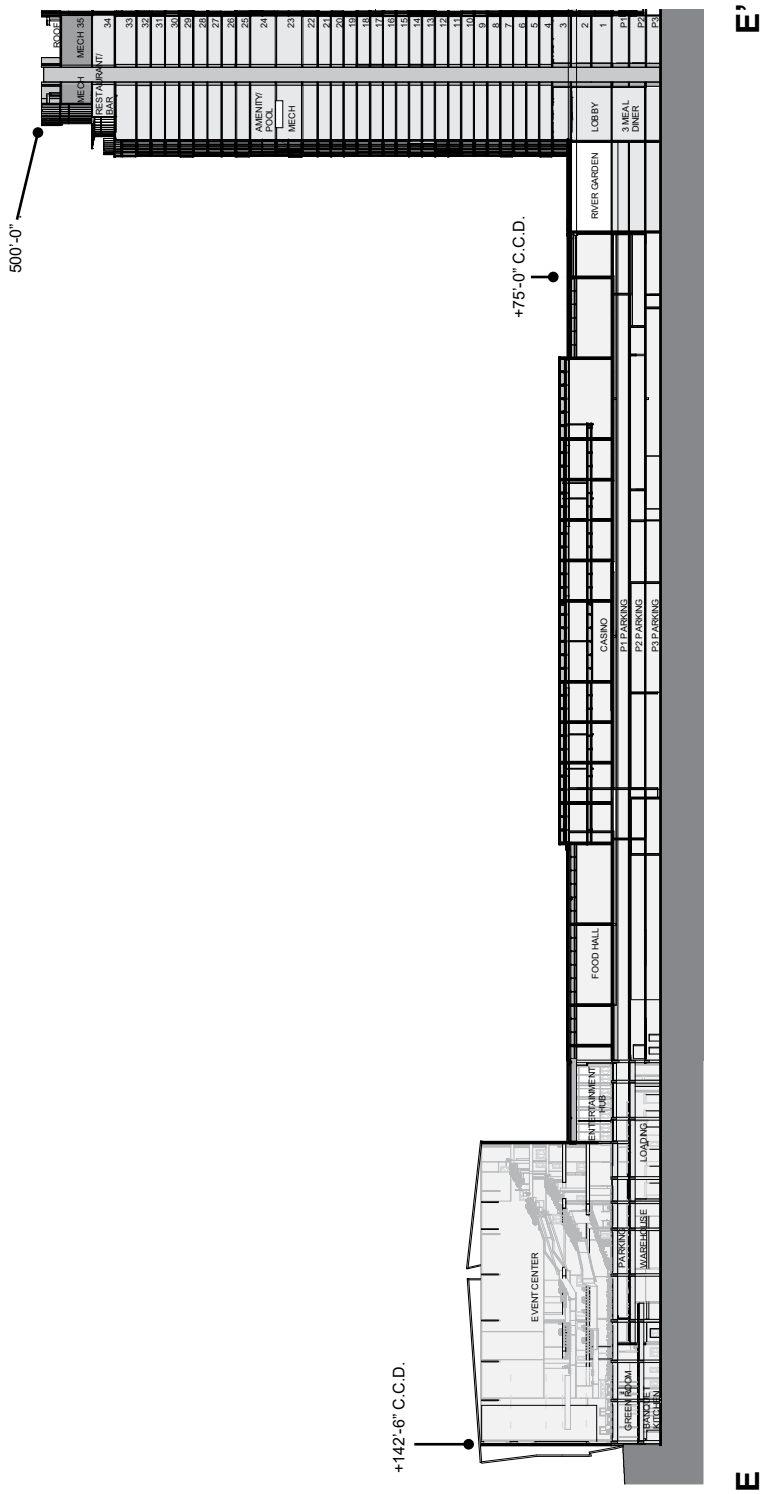
Introduced: **September 21, 2022**
 Plan Commission: **December 12, 2022**
 Updated: **May 03, 2023**





KEYPLAN

KEYPLAN

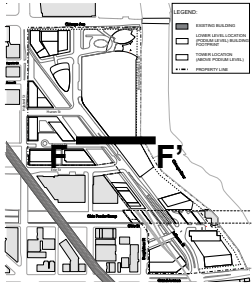


PHASE 1: BUILDING SECTION - 1 (OF 2)

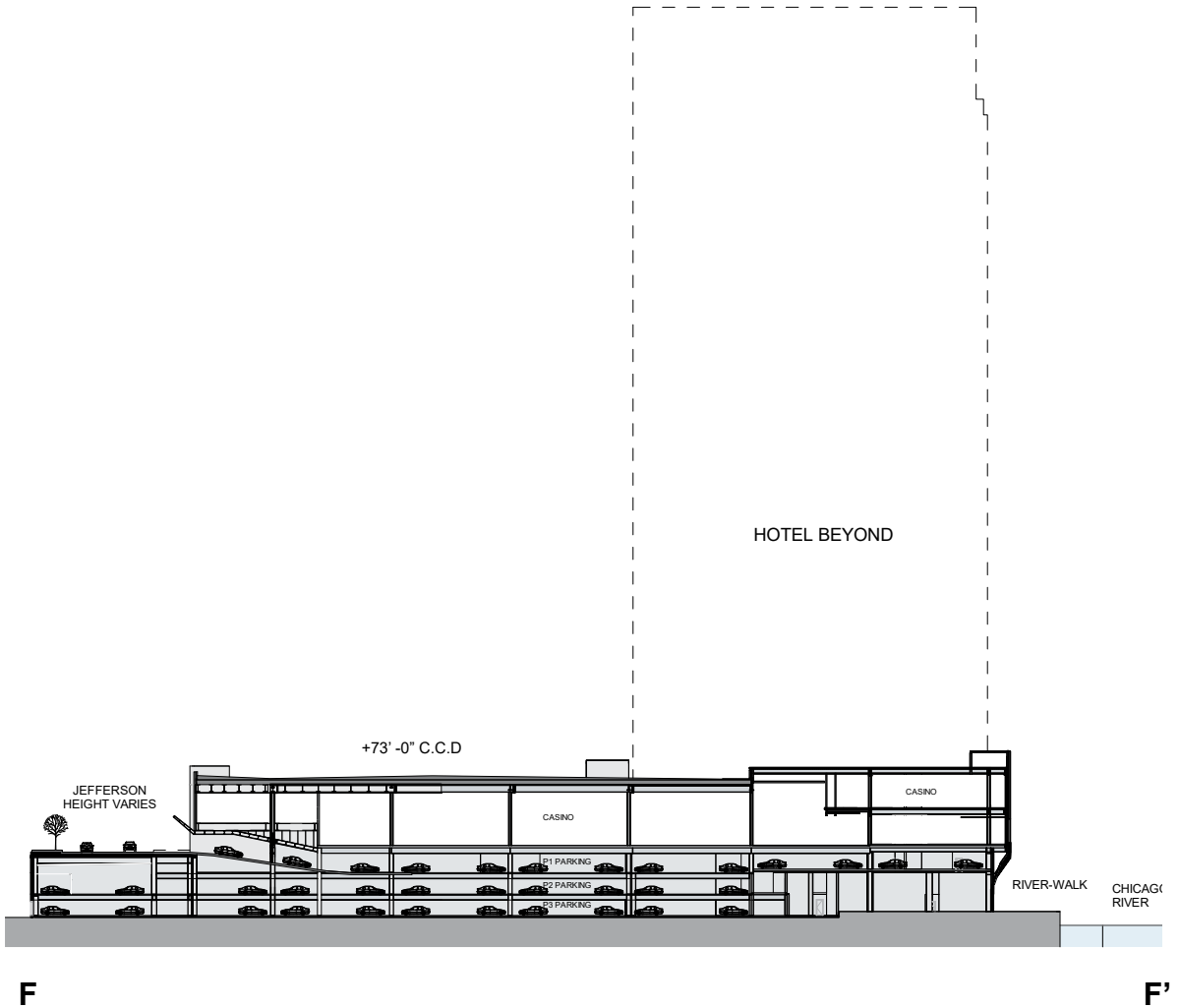
Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE...; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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KEYPLAN

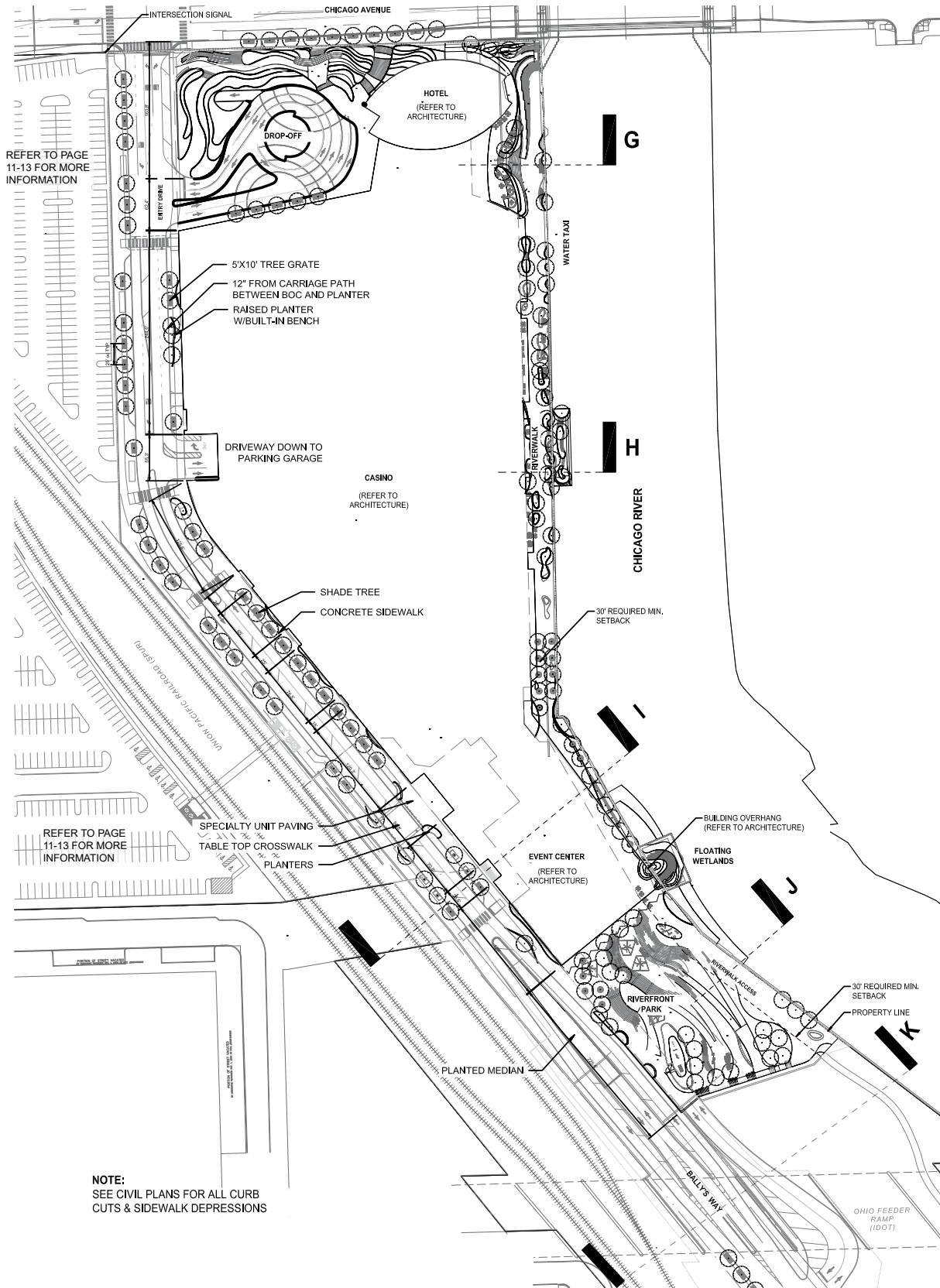


**PHASE 1: BUILDING SECTION - 2 (OF 2)
FF EAST / WEST BUILDING SECTION**

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE...; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: **September 21, 2022**
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REFER TO PAGE 11-13 FOR MORE INFORMATION

REFER TO PAGE 11-13 FOR MORE INFORMATION

NOTE:
SEE CIVIL PLANS FOR ALL CURB CUTS & SIDEWALK DEPRESSIONS

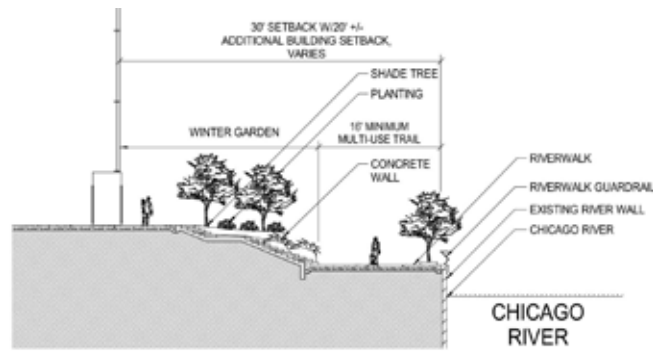
PHASE 1: PROPOSED LANDSCAPE PLAN

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

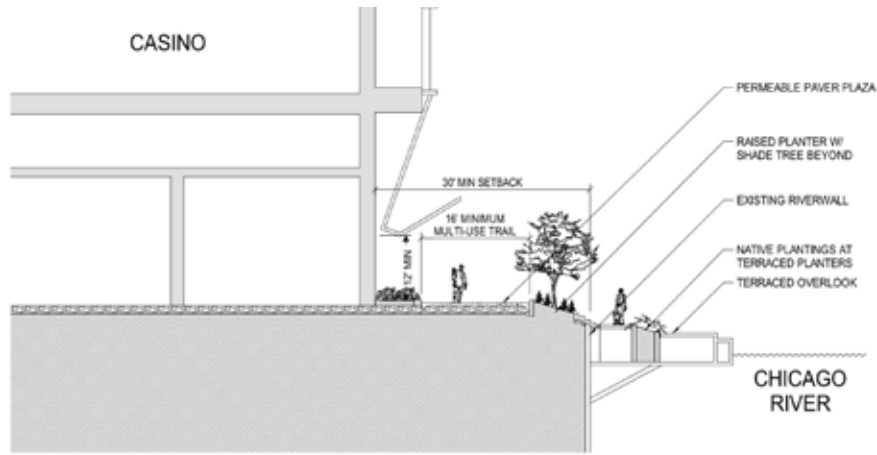
Introduced: September 21, 2022
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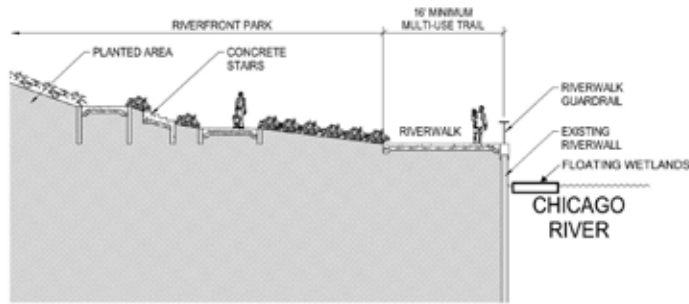
SECTION G



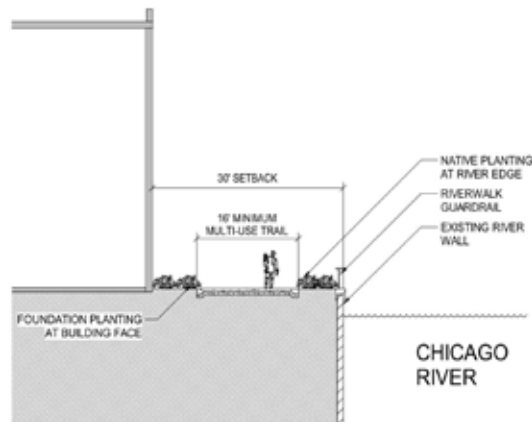
SECTION H



SECTION J



SECTION K

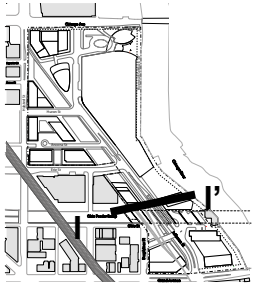


PHASE 1: RIVERWALK SITE SECTIONS

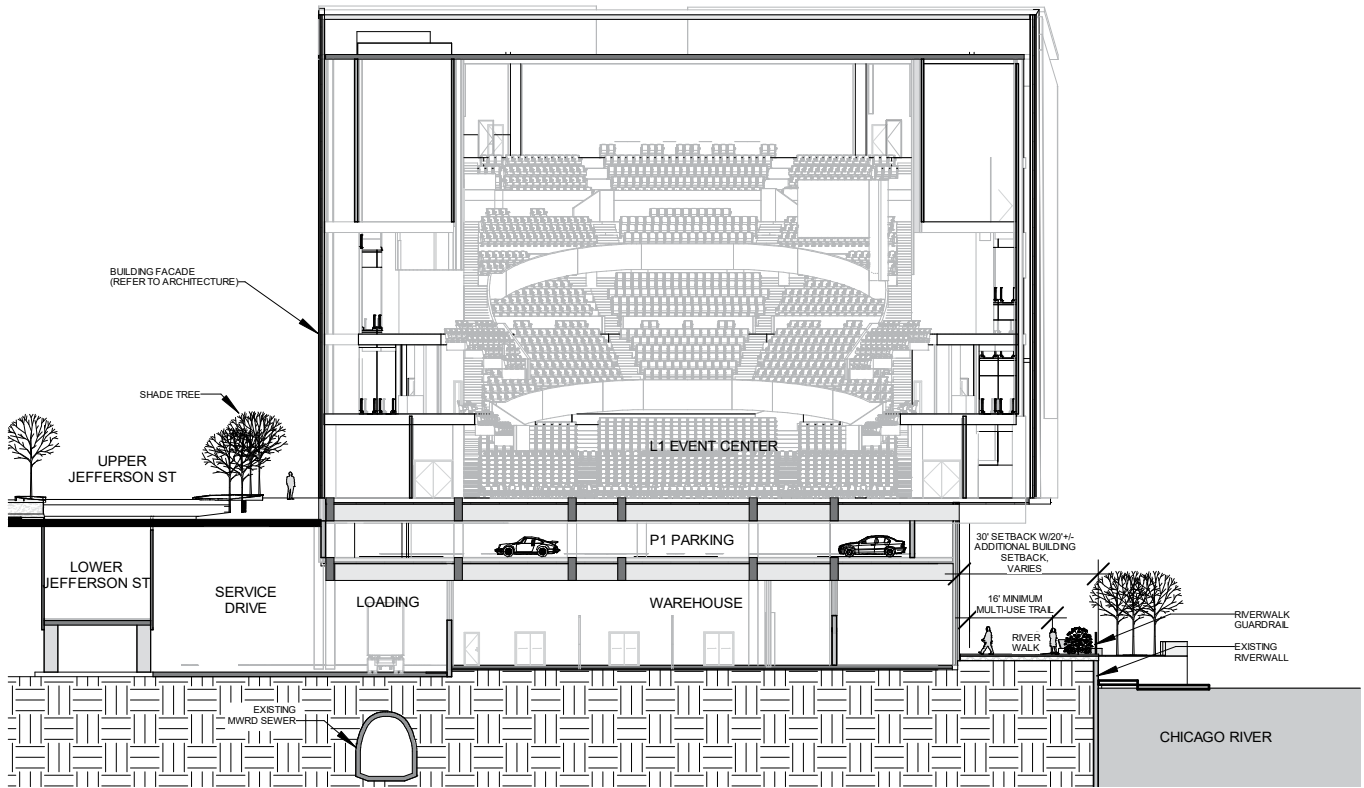
Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE...; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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KEYPLAN



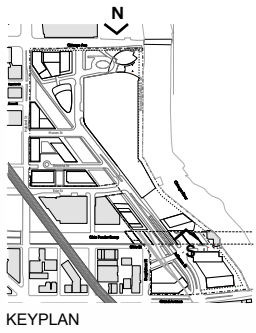
SECTION I-I'

PHASE 1: RIVERWALK SITE SECTION I-I'

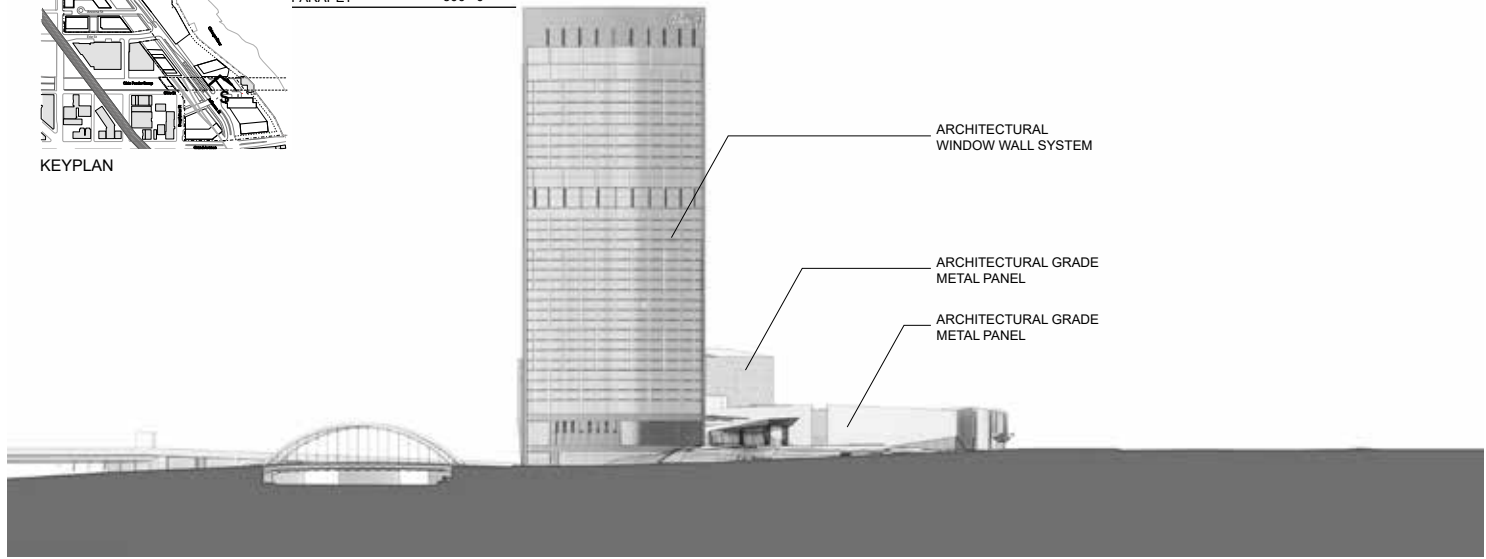
Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE...; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: September 21, 2022
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TOP OF PARAPET 500'-0"



ARCHITECTURAL WINDOW WALL SYSTEM

ARCHITECTURAL GRADE METAL PANEL

ARCHITECTURAL GRADE METAL PANEL

NORTH ELEVATION

TOP OF PARAPET 500'-0"

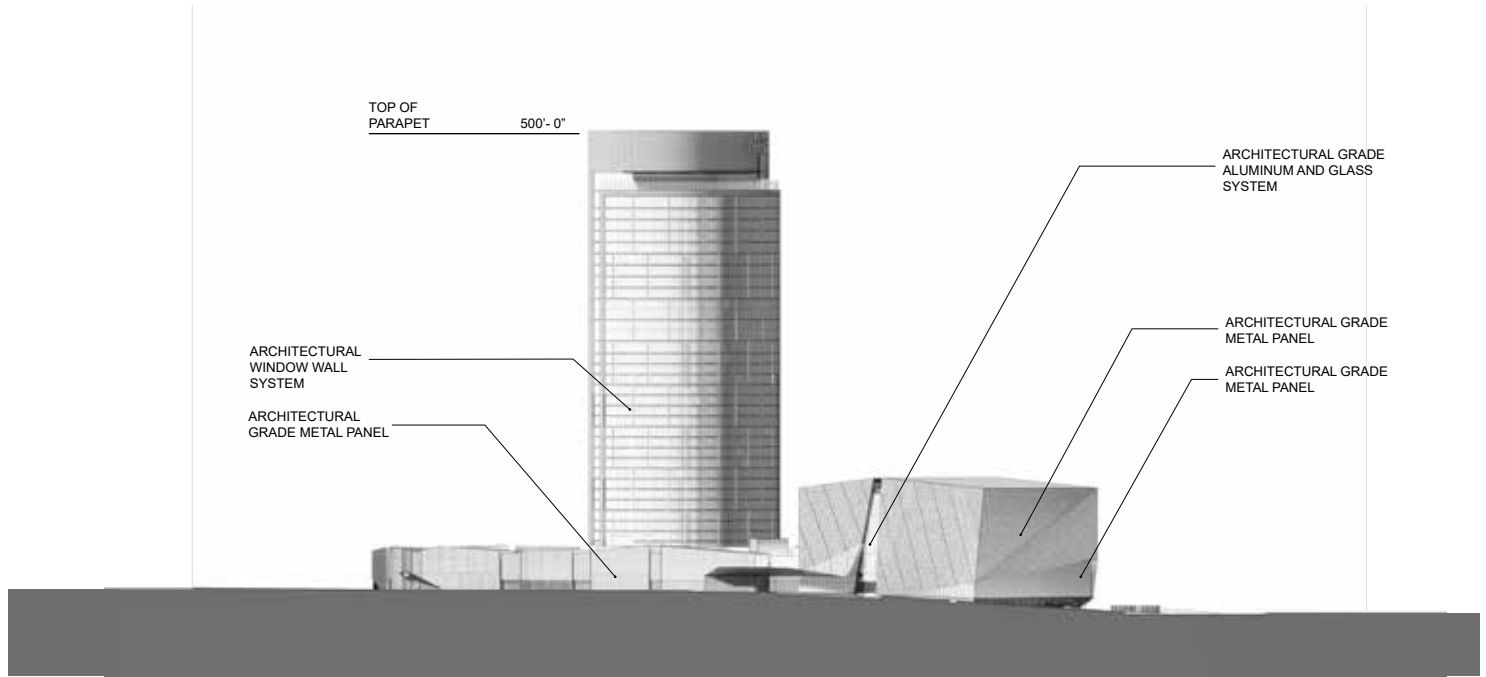
ARCHITECTURAL WINDOW WALL SYSTEM

ARCHITECTURAL GRADE METAL PANEL

ARCHITECTURAL GRADE ALUMINUM AND GLASS SYSTEM

ARCHITECTURAL GRADE METAL PANEL

ARCHITECTURAL GRADE METAL PANEL



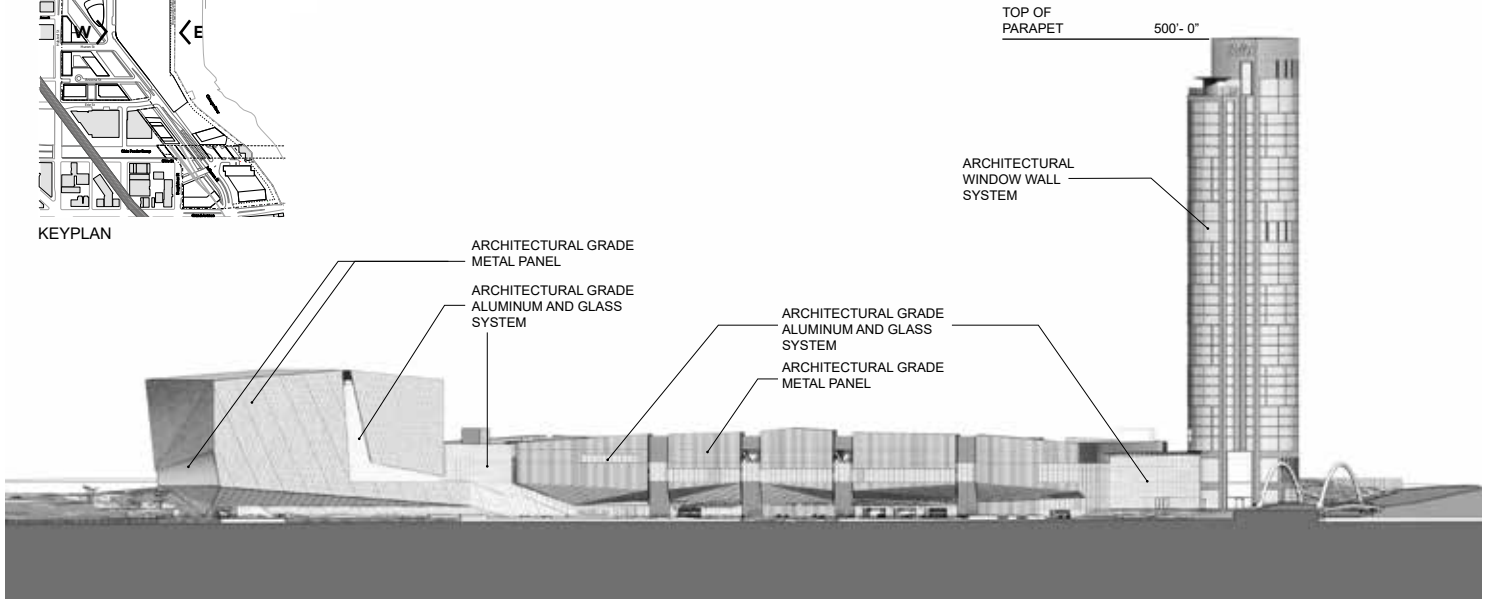
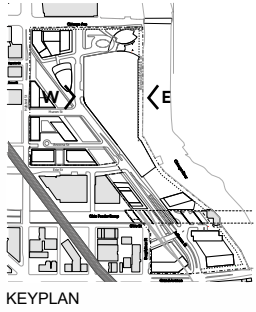
SOUTH ELEVATION

PHASE 1: BUILDING ELEVATIONS - NORTH / SOUTH

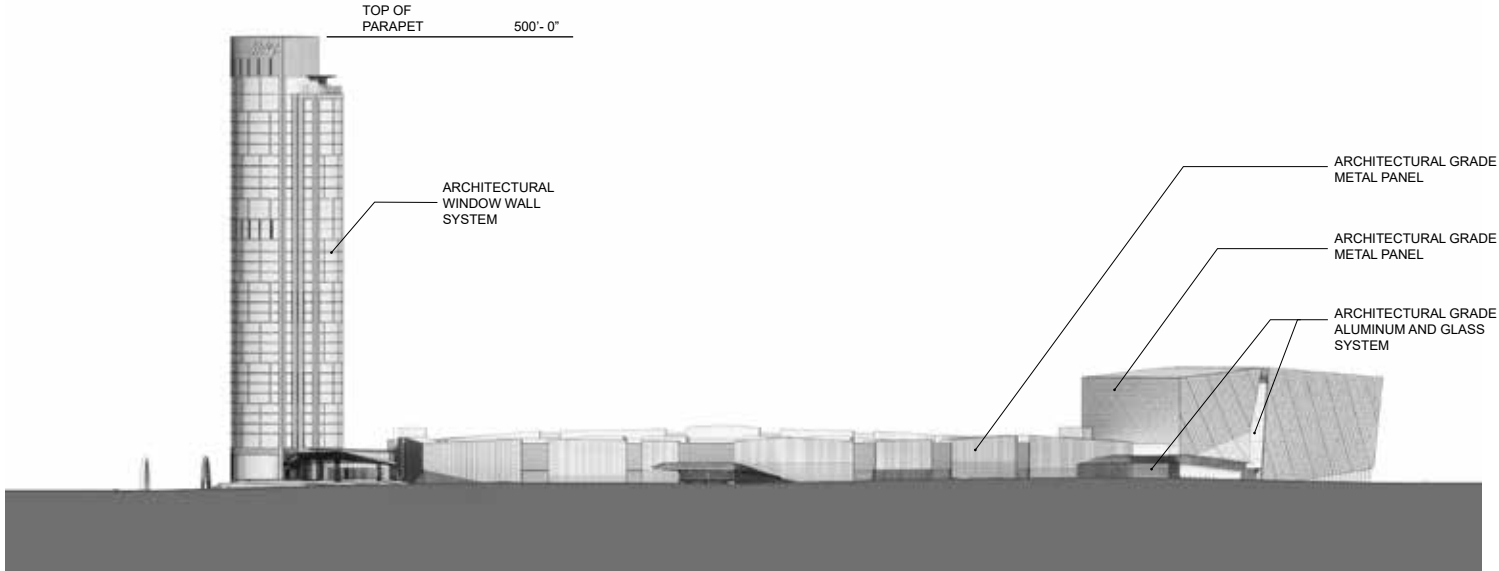
Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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EAST ELEVATION



WEST ELEVATION

PHASE 1: BUILDING ELEVATIONS - EAST / WEST

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE...; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

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PHASE 1



PHASE 2 (OVER YEARS)



CITY'S EDGE



BALLY'S LANDING



RIVERWALK



SOUTH PARK



ENTERTAINMENT HUB



DROP-OFF / PORTE COCHERE



BALLY'S WAY LOOKING NORTH



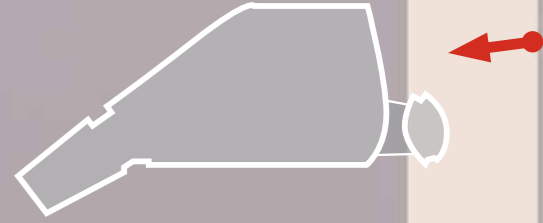
BALLY'S WAY | DROP-OFF PLAZA



BALLY'S WAY | DROP-OFF PLAZA



DROP-OFF NORTH FACADE



6.0 IMPLEMENTATION

Project Information

Project Location

Project Address		
Type of Project (land use)	Casino, Mixed Use and Entertainment District	
Size of Project	30 Acres	
Is this a river dependent or critical service use?	<input type="checkbox"/> Yes	<input type="checkbox"/> No
Character Zone	<input type="checkbox"/> Northwest Zone <input type="checkbox"/> North Branch Zone <input type="checkbox"/> Loop Zone	<input type="checkbox"/> South Branch Zone' <input type="checkbox"/> Southwest Zone <input type="checkbox"/> Bubbly Creek Zone
30 ft. river setback (as verified by plat of survey)	<input type="checkbox"/> Yes	<input type="checkbox"/> No
Public Access	<input type="checkbox"/> Yes	<input type="checkbox"/> No

Chicago River Design Guidelines

3.2 Multi-Use Path

3.2.1 Design Criteria	<input type="checkbox"/> Yes	<input type="checkbox"/> No
3.2.2 Public Access	<input type="checkbox"/> Yes <input type="checkbox"/> N/A, please describe	<input type="checkbox"/> No
3.2.3 Minimum Path Width, select all that apply	<input type="checkbox"/> Separated 12' bicycle, 8' pedestrian <input type="checkbox"/> Combined, 16'	<input type="checkbox"/> Combined, 10' <input type="checkbox"/> Combined, 10' with 2' paved, gravel, or mowed shoulders <input type="checkbox"/> Other, please describe below
3.2.4 Paving and Materials, select all that apply	<input type="checkbox"/> Granite <input type="checkbox"/> Architectural Granite <input type="checkbox"/> Poured-in-Place Concrete <input type="checkbox"/> Concrete Pavers <input type="checkbox"/> Unit Pavers	<input type="checkbox"/> Permeable Pavers <input type="checkbox"/> Asphalt <input type="checkbox"/> Decomposed Aggregate <input type="checkbox"/> Other, please describe below

Comments, please not which section from the list above (e.g. 3.2.3) you're describing

Paving is anticipated to be predominantly poured-in-place concrete with the possibility of specialty pavers at upgraded areas of interest. All materials will be in compliance with Chicago River North Branch Design Guidelines.

3.3 Furnishings

3.3.1 Site Furnishing Guidelines

Materials, select all the apply	<input type="checkbox"/> Stainless Steel <input type="checkbox"/> Galvanized Steel <input type="checkbox"/> Powder Coated Steel	<input type="checkbox"/> Hardwoods, describe below <input type="checkbox"/> Recycled Plastic Lumber, describe below <input type="checkbox"/> Other, please describe below
Benches & Tables, on per 250 linear feet (LF) of river frontage.	LF of river frontage	_____
	Total Benches	_____
	Total Tables	_____
Trash and Recycling Receptacles, one metal trash and one metal recycling receptacles per 250 linear feet (LF) of river frontage	LF of river frontage	_____
	Total Trash Receptacles	_____
	Total Recycling Receptacles	_____
Railings	<input type="checkbox"/> Yes	<input type="checkbox"/> No

Comments, please note which section from the list above you're describing

The project is in design phase and final materials to be determined.

3.4 Seating and Gathering Areas

3.4.1 Seating Area Guidelines

Location, one per 500 linear feet (LF) of river frontage.	LF of river frontage	_____
	Total Seating Areas	_____
Furnishings, Each seating area should provide a minimum of two benches and one trash receptacle.	Total Benches	_____
	Total Trash Receptacles	_____

3.5 Lighting

3.5.1 Lighting Guidelines, see follow pages for an example of an acceptable product data sheet.

Fixture Height, recommended between 14-30 feet tall	<input type="checkbox"/> Yes, between 14-30'	<input type="checkbox"/> Other, please describe below
Color Temperature, LED bulbs that provide white light with a color temperature of 3000K or below.	<input type="checkbox"/> 3000K or below	<input type="checkbox"/> Other, please describe below
Light Pollution	<input type="checkbox"/> Dark Sky Compliant	<input type="checkbox"/> N/A
Additional Features	<input type="checkbox"/> Yes, please describe below	<input type="checkbox"/> No
Security Lighting	<input type="checkbox"/> Yes	<input type="checkbox"/> No

Comments, please note which section from the list above you're describing

The project is in design phase and lighting plan is not yet determined. It is anticipated that Phase 1 will be Dark Sky Compliant within the 30 foot riverwalk setback zone at a minimum.

3.6 Wayfinding and Signage

3.5.1 Lighting Guidelines 6.1 Signage Guidelines

Brand and Identity	<input type="checkbox"/> Yes <input type="checkbox"/> N/A	<input type="checkbox"/> No
Directional Signage, indicate total signs	<input type="checkbox"/> Yes <input type="checkbox"/> N/A	<input type="checkbox"/> No
Mile Marker, locate every quarter mile	<input type="checkbox"/> Yes <input type="checkbox"/> N/A	<input type="checkbox"/> No
Regulatory Signage, indicate total signs	<input type="checkbox"/> Yes <input type="checkbox"/> N/A	<input type="checkbox"/> No
Identity Signage, indicate total signs	<input type="checkbox"/> Yes <input type="checkbox"/> N/A	<input type="checkbox"/> No

Comments, please note which section from the list above you're describing

The project is in design phase and wayfinding/signage plan is not yet complete. However, the project will incorporate brand/identity signage, directional signage, riverwalk mileage markers, regulatory signage, and identity signage as required and as appropriate for the project.

3.7 Landscaping

3.7.1 Preservation and Restoration Guidelines

Preservation, preserve existing habitat and plantings	<input type="checkbox"/> Yes <input type="checkbox"/> N/A	<input type="checkbox"/> No
Disturbance, minimize site disturbance	<input type="checkbox"/> Yes <input type="checkbox"/> N/A	<input type="checkbox"/> No
Protection, protect existing vegetation during construction by installing tree protection fence	<input type="checkbox"/> Yes <input type="checkbox"/> N/A	<input type="checkbox"/> No
Tree Preservation and Removal, preserve mature, healthy, native shade and evergreen trees	<input type="checkbox"/> Yes <input type="checkbox"/> N/A	<input type="checkbox"/> No

3.7.2 Plant Selection Guidelines

Plant Selection, per Appendix 7.4. Identify total number of plants selected from Appendix list.	Submergent	_____
	Emergent	_____
	Riparian	_____
	Upland	_____
Fencing, 4-6' ornamental metal fence for vehicular use areas, 4' ornamental metal fence for non-vehicular use areas	<input type="checkbox"/> Yes, height _____ <input type="checkbox"/> N/A	<input type="checkbox"/> No
Trees, 2 per 25 LF of river frontage for vehicular use areas, 1 per 25 LF for non-vehicular use areas	LF of river frontage	_____
	Total Vehicular Area Trees	_____
	Total Non-Vehicular Area Trees	_____
Hedges, continuous hedge on the river side of fence is required for vehicular use areas	<input type="checkbox"/> Yes <input type="checkbox"/> N/A	<input type="checkbox"/> No
Foundation Plantings, required for non-vehicular use areas	<input type="checkbox"/> Yes <input type="checkbox"/> N/A	<input type="checkbox"/> No

Comments, please note which section from the list above you're describing

The project is in design phase and the landscape plan is preliminary. Proposed plant materials to be native and adaptive species compliant with the North Branch Design Guidelines. The design team will provide updated landscape plan and plant materials later in the process. A preliminary list of plant materials are incorporated into the 9/7/2022 River Ecology + Governance presentation.

3.8 Riverbank Treatments

3.8.1 Riverbank Guidelines

Existing sloped riverbank is to be retained and improved

Yes No

N/A - There is no existing sloped riverbank within the project extents. River edge is steel sheet piles.

3.9 River Edge Treatments

3.9.1 River Edge Guidelines

Describe the proposed river edge stabilization and enhancement treatments.

The current steel (sheet pile) seawall has a concrete topping on the northern zone adjacent to the casino and exposed steel to the south. An existing concrete pier exists where the former Eire Street bridge abutment was located. The design team is reviewing improvements the river wall in include floating wetlands and ways to interact with the water edge. All improvements will be contingent on AHJ approval.

3.10 Sloped Bank Treatments

3.10.1 Sloped Bank Guidelines

Describe the proposed sloped bank treatments.

N/A

3.11 Vertical Bulkhead or Seawall Treatments

3.11.1 Vertical Bulkhead or Seawall Guidelines

Describe the proposed vertical bulkhead and seawall guidelines treatments.

Along the vertical seawall there is an opportunity to provide floating wetlands to naturalize the edge.

3.12 Guidelines for Improvements Outside of the Required Setback

3.12.1 Design, Orientation, and Massing of New Structures and Buildings

Placement, locate buildings and vehicular areas outside of the river setback

Yes, height _____ No
 N/A

River-facing façade, river-facing facade should be designed as a principal or major façade

Yes, height _____ No
 N/A

Massing and Articulation, locate lower buildings with active frontage adjacent to river setback area. Step back massing along river. Locate taller buildings behind low buildings

Yes No
 N/A

Neighborhood Transitions, step down height of buildings to transition to the scale of adjacent neighborhoods

Yes No
 N/A

First Floor, activate first floors of buildings with direct access to river and multi-use path

Yes No
 N/A

Wildlife, incorporate bird-friendly design standards into building designs

Yes No
 N/A

Sunlight, river corridor should have sunlight for approximately six (6) hours per day

Yes No
 N/A

3.12.2 Screening Guidelines

Outdoor Storage, if necessary, storage areas should be located beyond the minimum 30' setback area	<input type="checkbox"/> Yes , height _____ <input type="checkbox"/> N/A	<input type="checkbox"/> No
Materials, select all that apply	<input type="checkbox"/> Poured-in-Place Concrete <input type="checkbox"/> Split Face Concrete Masonry Units <input type="checkbox"/> Ground Face Concrete Masonry Units	<input type="checkbox"/> Heavy Wood <input type="checkbox"/> Other, please describe below
Walls and Fences, screening walls and fences should be planted with vines at the base	<input type="checkbox"/> Yes <input type="checkbox"/> N/A	<input type="checkbox"/> No
Access, fencing that separates the riverfront from the outside of the setback area should be avoided	<input type="checkbox"/> Yes <input type="checkbox"/> N/A	<input type="checkbox"/> No

3.13 Transition Between Adjacent Developments

Describe the proposed transitions and treatments between adjacent developments

Transitions will be provided to the riverwalk and river front park that include: Stairs and landing at the River Garden, stairs and sloped walkway to Chicago Avenue, stairs and sloped walkway from the Riverfront Park up to Jefferson Street. Complimentary land uses are located adjacent to the riverwalk and riverfront park.

Menu of Improvements

4.1 Overview

4.1.1 Application

PD Project Size

- Large, >1,980 LF of riverfront
 Medium, 660-1,980 LF of riverfront

Others

Total Expected Menu Items _____

Required # of Priority Menu Items _____

4.3 Nature



4.3.2 New Naturalized Shoreline (priority)

Describe the proposed new naturalized shoreline improvements

N/A



4.3.3 Stormwater Management Best Practices (priority)

Describe the proposed stormwater management best practices

The PD 1426 district storm water approach will transition away from the combined sewer system and comply with the City's storm water ordinance. The landscape treatments will include a combination of permeable pavement, bio-infiltration systems, and floating wetlands to help capture and clean the storm water run-off.



4.3.4 Aquatic Wildlife Habitats (priority)

Describe the proposed aquatic wildlife habitats

Aquatic wildlife habitats are planned to include floating wetlands adjacent to the riverwall.



4.3.5 Robust Urban Habitats

Describe the proposed robust urban habitats

Robust urban habitats are planned along the riverwalk and as part of the design of the riverfront park. Proposed plant materials to be native and adaptive species compliant with the North Branch Design Guidelines.



4.3.6 Increased Setback

Describe the proposed increased setback

Increases beyond the 30 foot setback for amenity spaces and habitat are proposed to occur along the riverwalk in multiple locations and include: setting back the hotel at the north end of the site, a proposed water access platform near the mid-point of the casino, utilization of the existing Erie Street bridge abutment at the south end of the Casino, and floating wetlands adjacent to the abutment.



4.3.7 :Large Riverfront Park

Describe the proposed large riverfront park

A large multi-level riverfront park is located at the river's bend on the southern edge of the Bally's Casino theater. The park includes multiple amenity zones and seating areas, accessible circulation, flexible lawn and paved open spaces, and vegetation consisting of native and adapted species.

4.4 Recreation



4.4.2 Access to Water and Docking Facilities

Describe the proposed access to water and docking facilities

A water taxi dock facility is being planned at the north end of the riverwalk near the Chicago Avenue bridge, adjacent to the River Garden.



4.4.3 Expanded Seating Areas

Describe the proposed expanded seating areas

To compliment the food and beverage venues, expanded seating areas are being planned adjacent to the casino and within the riverwalk zone, similar to the riverwalk adjacent to Wacker Drive.



4.4.4 Riverfront Overlooks

Describe the proposed riverfront overlooks

River overlook areas are being planned to provide greater access and exposure to the water's edge. The areas are proposed to include utilization of the existing Erie Street bridge abutment as well as floating platform along the riverwall. Improvements will be contingent on AHJ approval.



4.4.5 Recreational Areas

Describe the proposed recreational areas

The full extent of the riverwalk and riverfront park are considered recreational areas for the use and enjoyment of the broader community.



4.4.6 Support Amenities

Describe the proposed support amenities

Community support amenities include boat dock, a variety of seating places, dog park, lawn gathering areas and flexible open space, and public art.

4.5 Connectivity



4.5.2 Underbridge Connections

Describe the proposed underbridge connections

An underbridge connection at Chicago Avenue is being planned and coordinated between the Onni property to the north and the Bally's property to the south of the Chicago Avenue bridge in conjunction with CDOT who are reconstructing Chicago Avenue and replacing the bridge structure.



4.5.3 Enhanced Connections to Street & Transportation Network

Describe the proposed enhanced connections to the street and transportation network

The project will meet all City and North Branch accessibility standards and will provide accommodations for both pedestrian and cyclists. Enhanced connections will be provided adjacent to the riverwalk and riverfront park that include: stairs and landing at the River Garden, stairs and sloped walkway to Chicago Avenue, stairs and sloped walkway from the Riverfront Park up to Jefferson Street.



4.5.4 Elevator / Increased Accessibility

Describe the proposed elevator / increased accessibility

In addition to the stairs and sloped walkways at the north and south ends of the Casino project, public elevators will be available at the River Garden and Casino atrium to the north and event center to the south to provide public access from the street level to the riverwalk.



4.5.5 Cantilevered & Floating Walkways

Describe the proposed cantilevered and floating walkways

Cantilevered overlooks and walkways are being considered at strategic vista locations along the riverwalk.



4.5.6 Interpretive Signage

Describe the proposed interpretive signage

There are numerous opportunities to provide interpretive signage along the riverwalk that tells the history of the site or provides information to support the river experience. These are being considered in conjunction with other riverwalk signage elements.



4.5.7 Public Art & Specialty Lighting

Describe the proposed public art and specialty lighting

Public art will be incorporated into the site design and could entail strategically placed sculpture, artful seating, interactive displays, murals and special paving patterns.

2.2.2 IMPROVEMENTS

1 3' SETBACK IMPROVEMENTS

CANOPIES/AWNINGS SHOULD HAVE A MINIMUM **CLEARANCE OF 10 VERTICAL FEET** AND DON'T EXTEND **3 OR MORE FEET** INTO THE SETBACK ZONE.

2.2.4 VARIANCE

2 **10' MAXIMUM VARIANCE** WITH ENCROACHMENTS INTO THE 30' RIVER SETBACK A **MAXIMUM OF 1/3** OF SITES RIVER FRONTAGE (**LINEAR FEET**)

2.2.5 MITIGATION FOR VARIANCES

3 **30' SETBACK** MITIGATION WITH OPEN SPACE **2.5 TIMES** THE LAND/OPEN SPACE LOST TO THE ENCROACHMENT (**AREA**)

EXCERPTS FROM: CHICAGO RIVER DESIGN GUIDELINES

2.2.2 Improvements or Structures Permitted in the Setback Area

Improvements or structures permitted in the setback area include:

- Paved or unpaved walkways;
- Projections from buildings in the private development zone, including but not limited to awnings and canopies, bay windows and balconies, and overhanging eaves and gutters, provided the projection does not extend three (3) or more feet into the setback zone, and has a minimum clearance of ten (10) vertical feet from setback grade;
- Stormwater best management practices (BMPs);
- Shade structures such as arbors, trellises, or pavilions;
- Fences and walls per zoning code requirements;
- Site furnishings including, but not limited to, benches, light fixtures, drinking fountains, and bike racks;
- Signage and wayfinding; and,
- Public gathering spaces and other riverfront amenities. See **Section 3.4 Seating & Gathering Areas on page 26** and **Chapter 4 on page 40**.

2.2.3 Improvements or Structures Not Permitted in the Setback Area

Improvements or structures not permitted in the setback area include:

- Buildings or structures of any kind;
- Vehicular use areas including, but not limited to, parking lots, driveways, service drives, loading docks, vehicular staging, or vehicular storage areas;
- Overhead utilities; and,
- Private yards, patios, terraces, or decks.

2.2.4 Variances

In certain cases a setback less than the required 30 feet may be permitted to address constrained sites, small, irregularly shaped sites, and to allow flexibility for optimal site plans.

Maximum variance (depth):

Structures and private yards may encroach into the 30-foot river setback a maximum of ten (10) feet so that the minimum setback is never less than twenty (20) feet from the top of the bank.

Maximum variance (length):

Structures and private yards may encroach into the required river setback, provided that the encroachment, or the area with a reduced setback, occurs along a maximum of one-third (1/3) of the site's river frontage, measured in linear feet (LF), so that the required setback never occurs along less than two-thirds (2/3) of the site's river frontage.

2.2.5 Mitigation for Variances

Requirement for additional open space for mitigation of variances:

Where structures and/or private yards encroach into the setback and riverfront, and the setback is therefore less than thirty (30) feet from the top of bank, additional land should be provided adjacent to the river setback and urban greenway zone to compensate for the loss of open space. This additional land should not be defined or developed as a private yard and should be free of structures.

Amount of additional open space for mitigation of variances:

Additional land should be provided adjacent to, and contiguous with, the setback zone at a rate of 2.5 times the land or open space lost to the encroachment.

The proportion of additional open space for mitigation of variances:

Additional open space design should avoid excessively long, deep, or narrow parcels of land that could be relatively or completely unusable and have little to no public benefit. Additional open space proportions should be no more than two (2) feet of open space per one (1) foot of frontage along the river setback line. Depth is measured perpendicular to the setback line.

SITE PLAN | 2.2.4 VARIANCE

2.2.4 VARIANCE

10' MAXIMUM VARIANCE WITH ENCROACHMENTS INTO THE 30' RIVER SETBACK A **MAXIMUM OF 1/3** OF SITES RIVER FRONTAGE (**LINEAR FEET (LF)**)

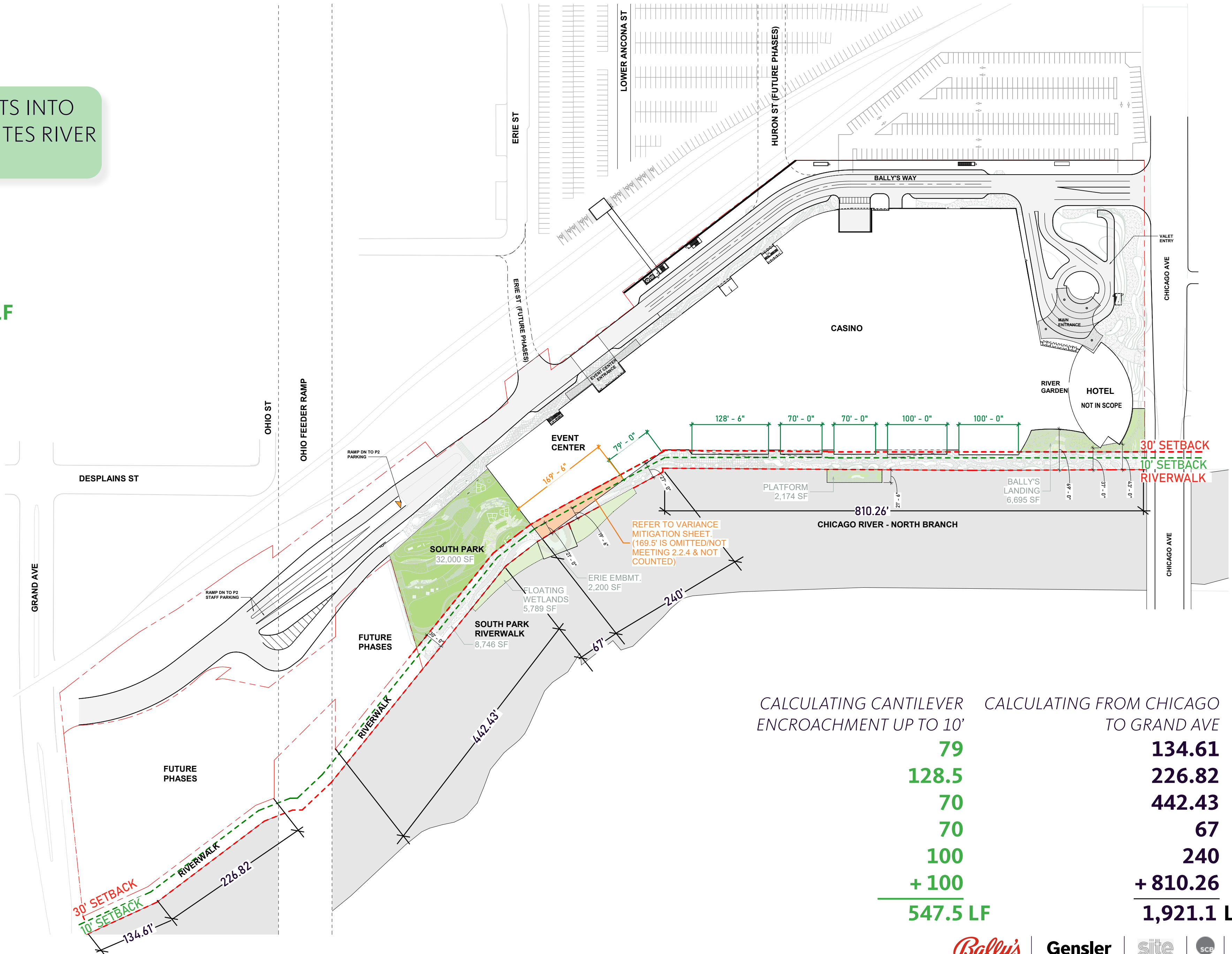
CALCULATING FROM CHICAGO AVE TO GRAND AVE
 TOTAL RIVERWALK FRONTAGE: **1,921.1 LF**
 $1921.1/3 = 640.4 \text{ LF}$

CANTILEVER ENCROACHMENT UP TO 10' : **547.5 LF**

547.5 LF < 640.4 LF

PERCENTAGE: **28.5%**

PERCENTAGE NEEDED: **33.3%** (MAX 1/3 OF SITE)



CALCULATING CANTILEVER ENCROACHMENT UP TO 10'	CALCULATING FROM CHICAGO TO GRAND AVE
79	134.61
128.5	226.82
70	442.43
70	67
100	240
+ 100	+ 810.26
547.5 LF	1,921.1 LF

SITE PLAN | MITIGATION FOR VARIANCE

2.2.5 MITIGATION FOR VARIANCE

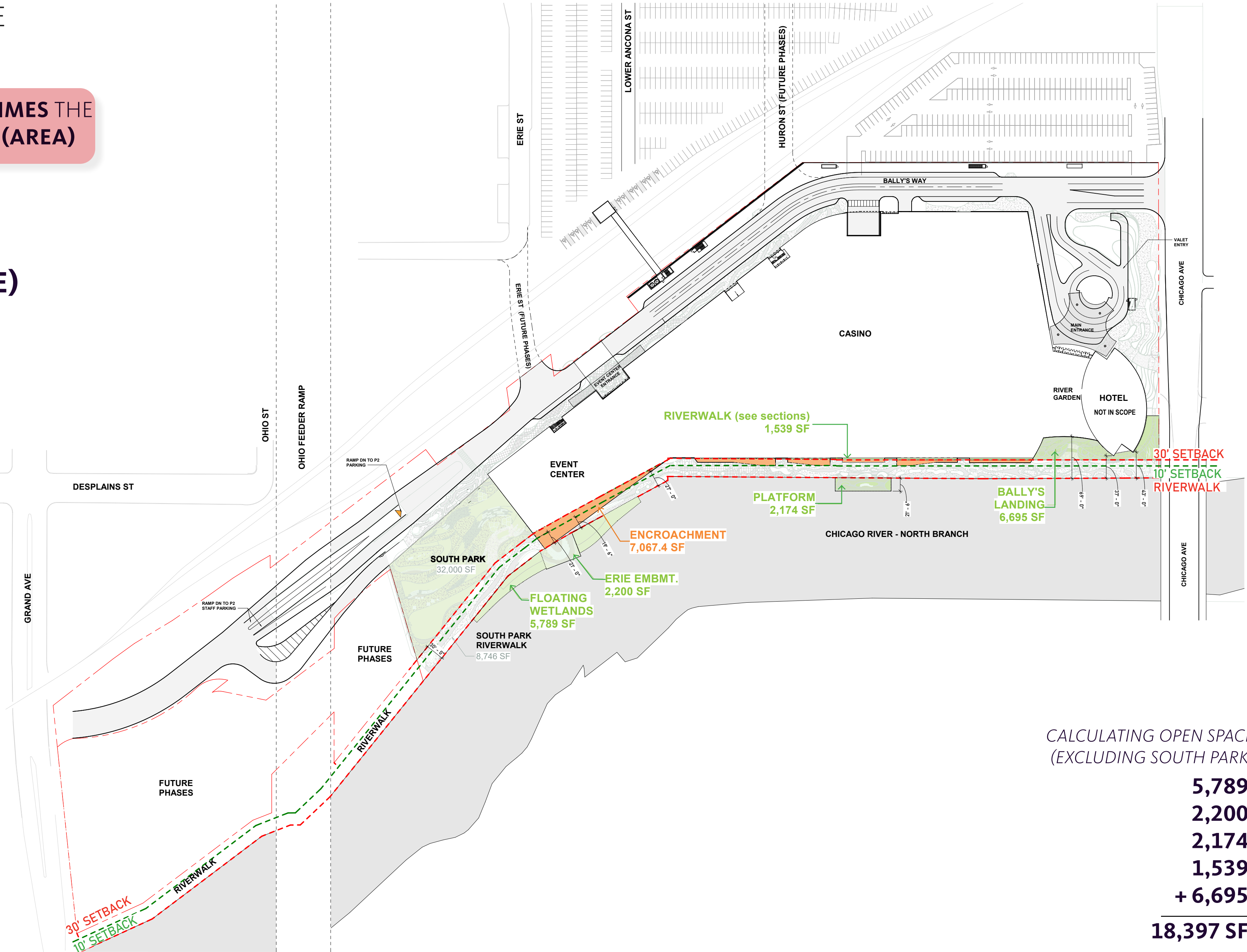
30' SETBACK MITIGATION WITH OPEN SPACE 2.5 TIMES THE LAND/OPEN SPACE LOST TO THE ENCROACHMENT (**AREA**)

ENCROACHMENT BEYOND 10': **7,067.4 SF**

7,067.4 SF X 2.5 = **17,668.5 SF**

17,668.5 SF < **18,397 SF (OPEN SPACE)**

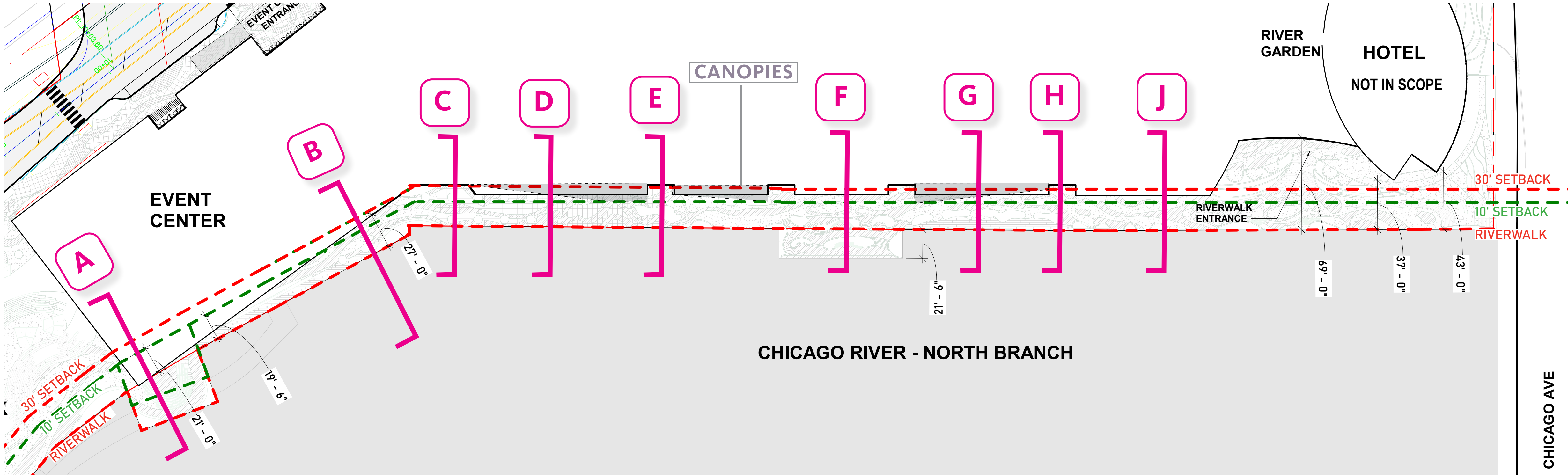
PERCENTAGE: **96%**

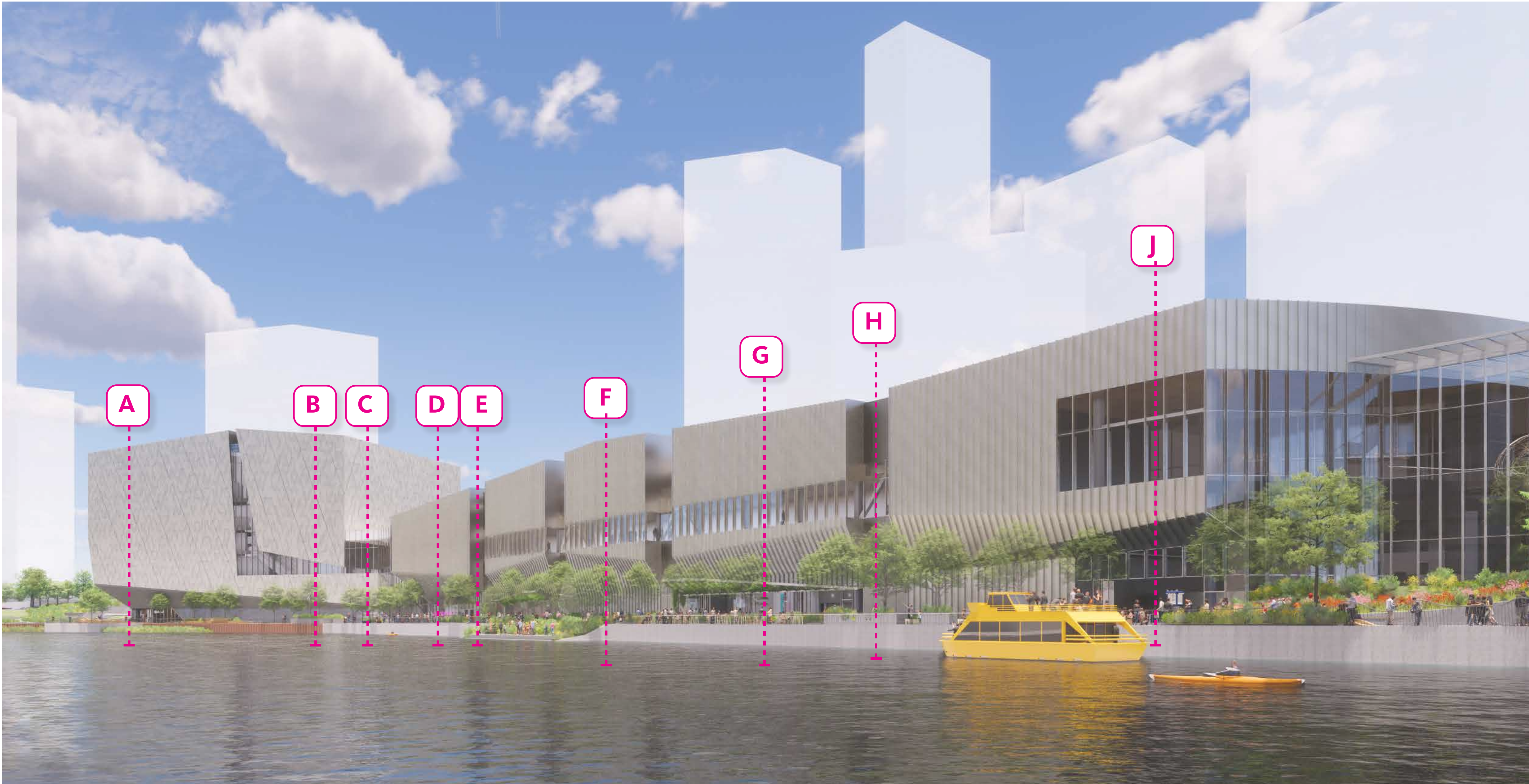


CALCULATING OPEN SPACE (EXCLUDING SOUTH PARK)

5,789
2,200
2,174
1,539
+ 6,695
18,397 SF

SETBACKS | OVERVIEW





SETBACKS | AT SOUTH PARK

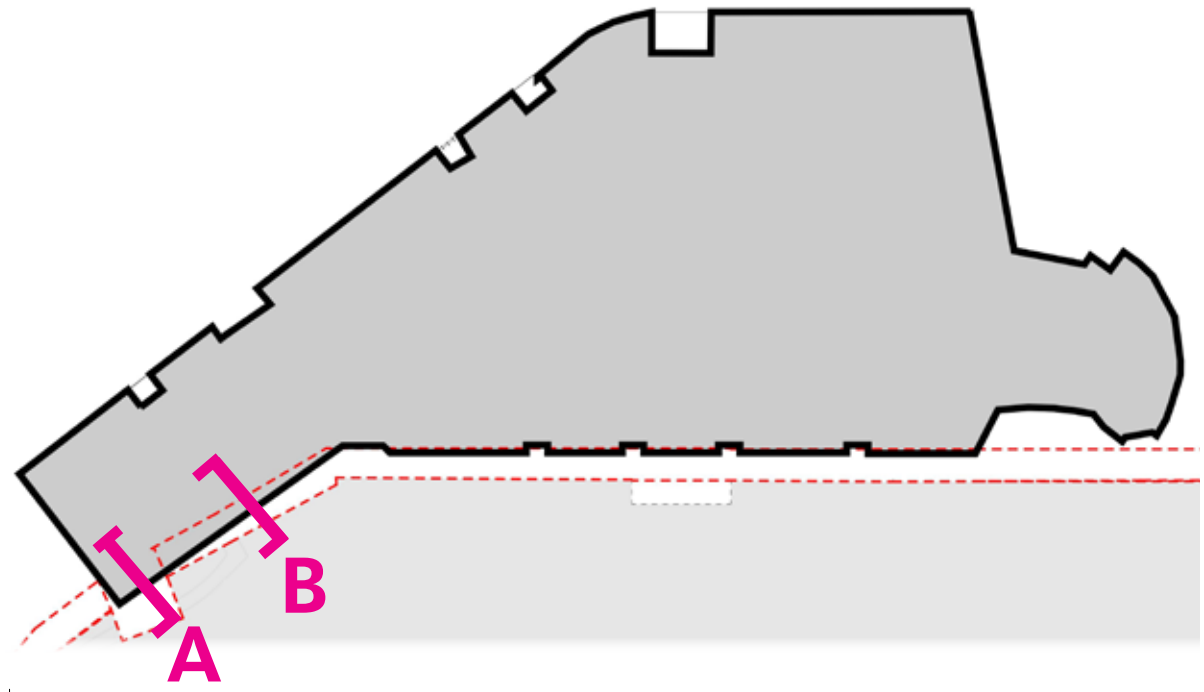
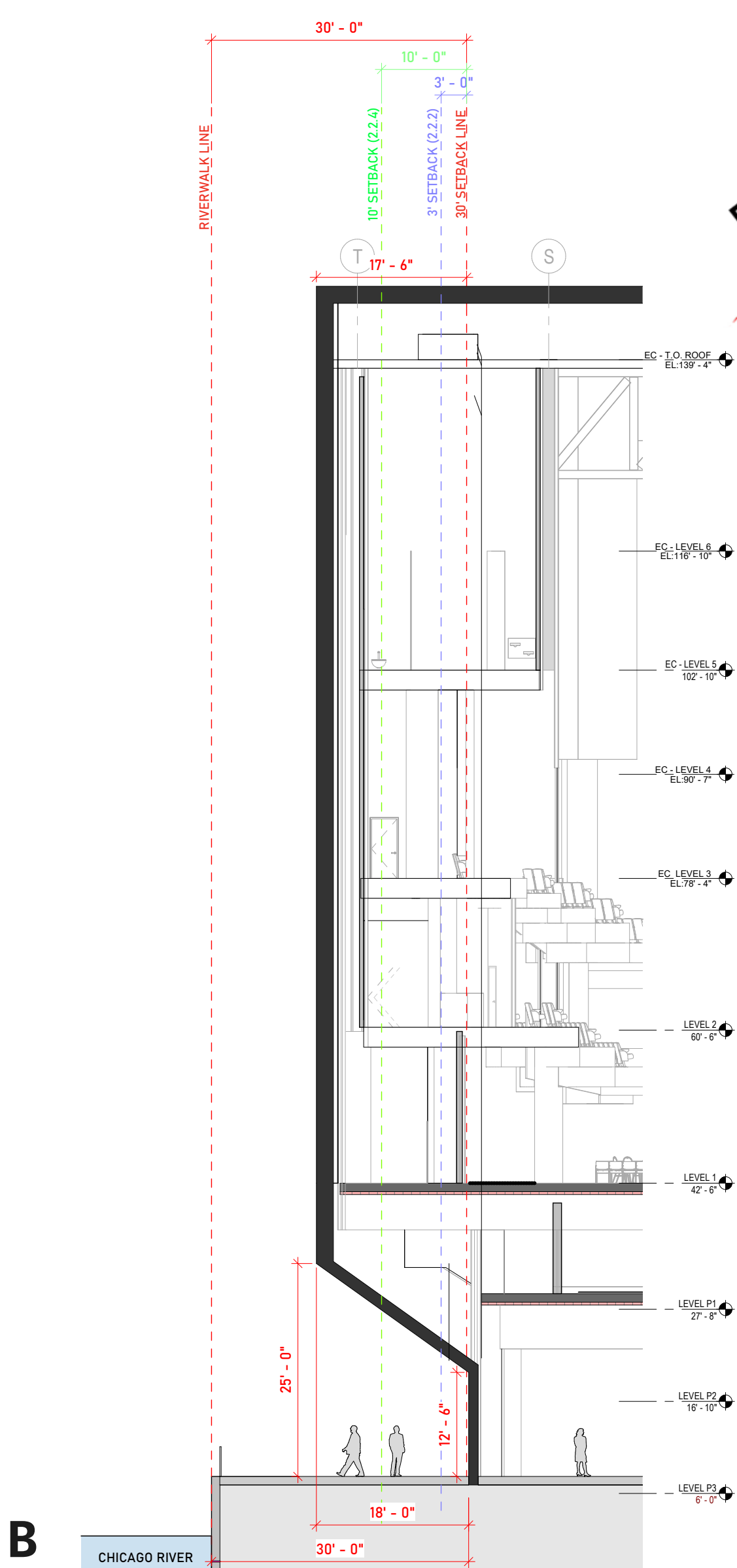
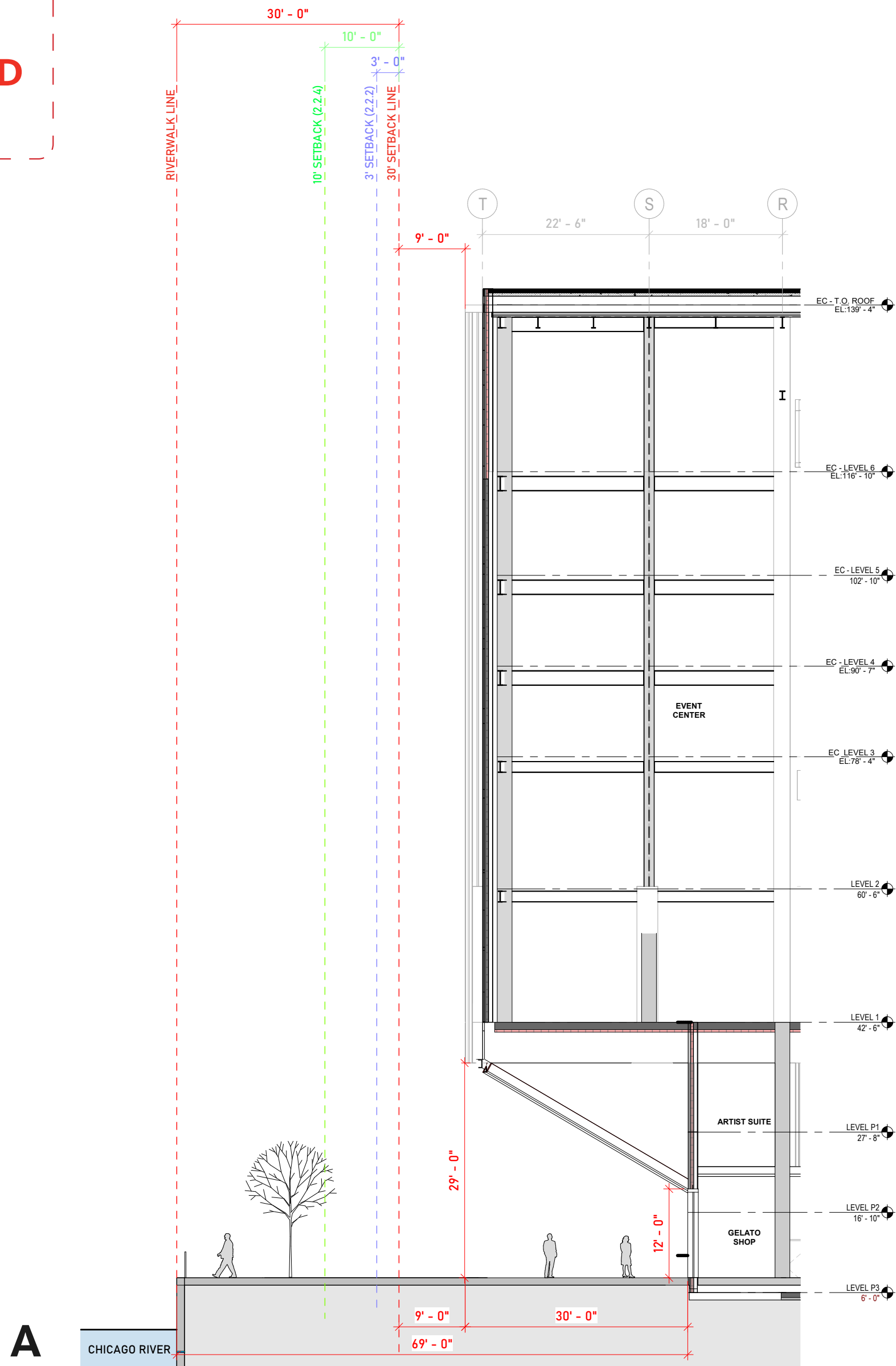


SETBACKS | AT EVENT CENTER

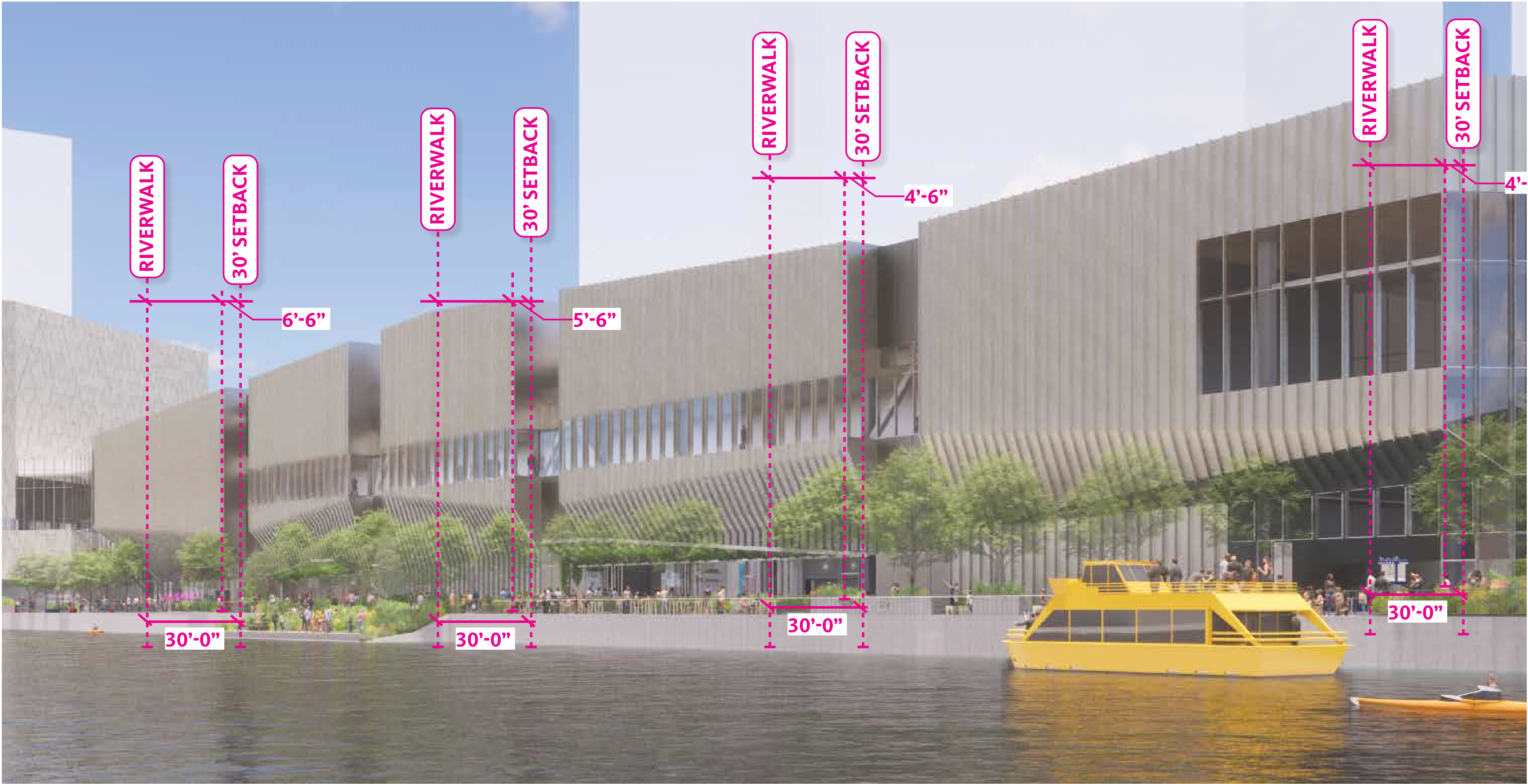


SETBACKS | AT EVENT CENTER

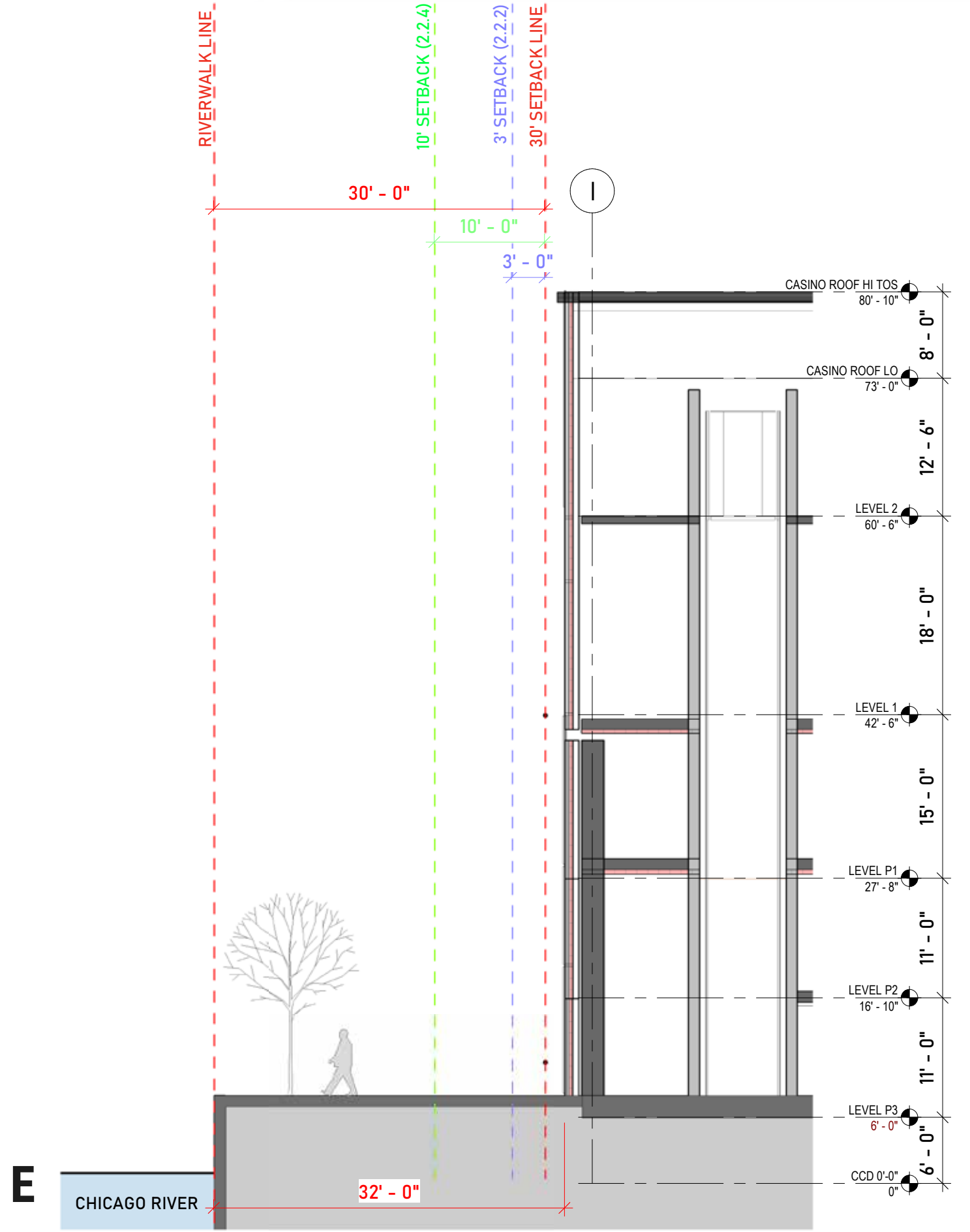
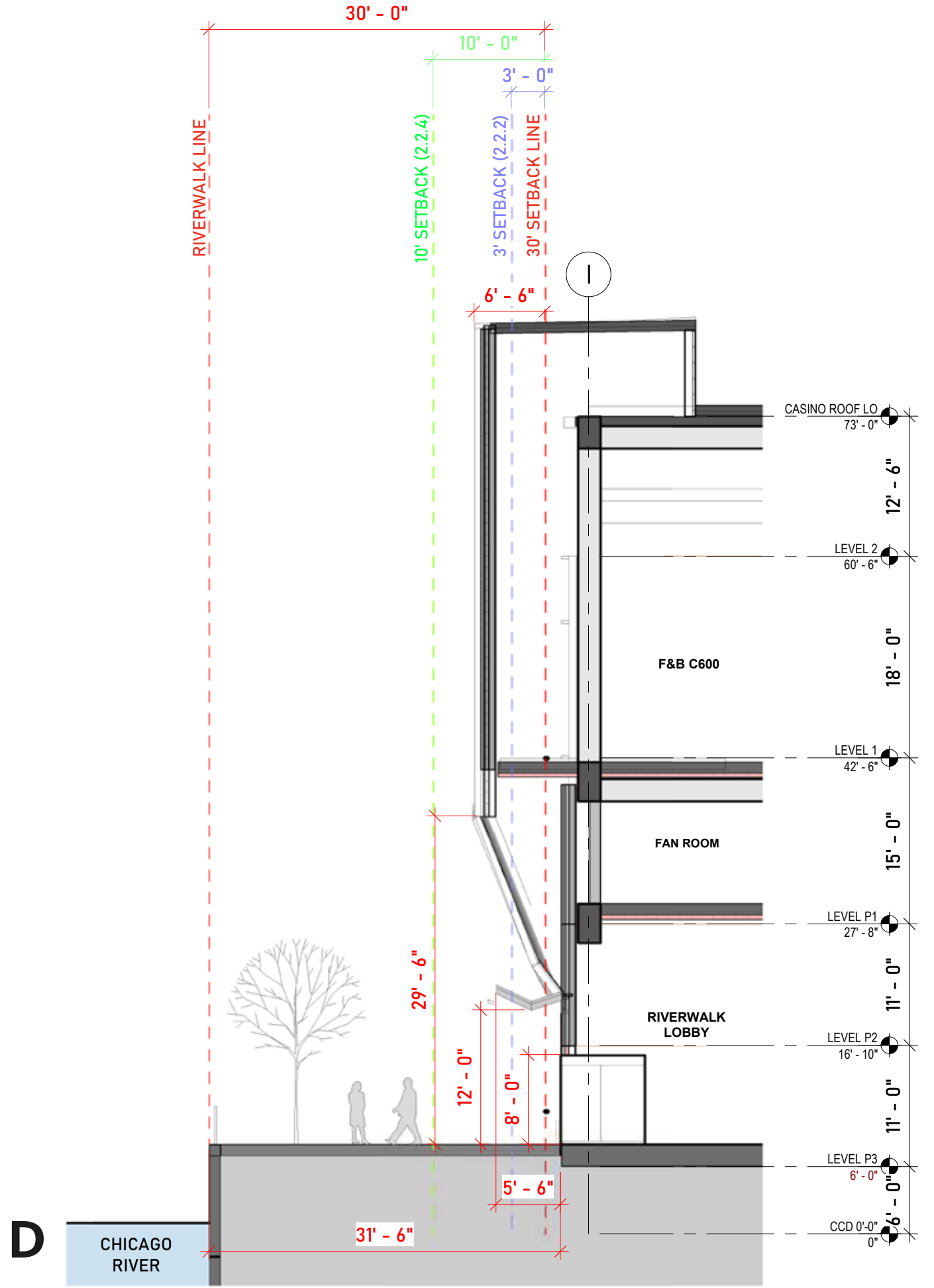
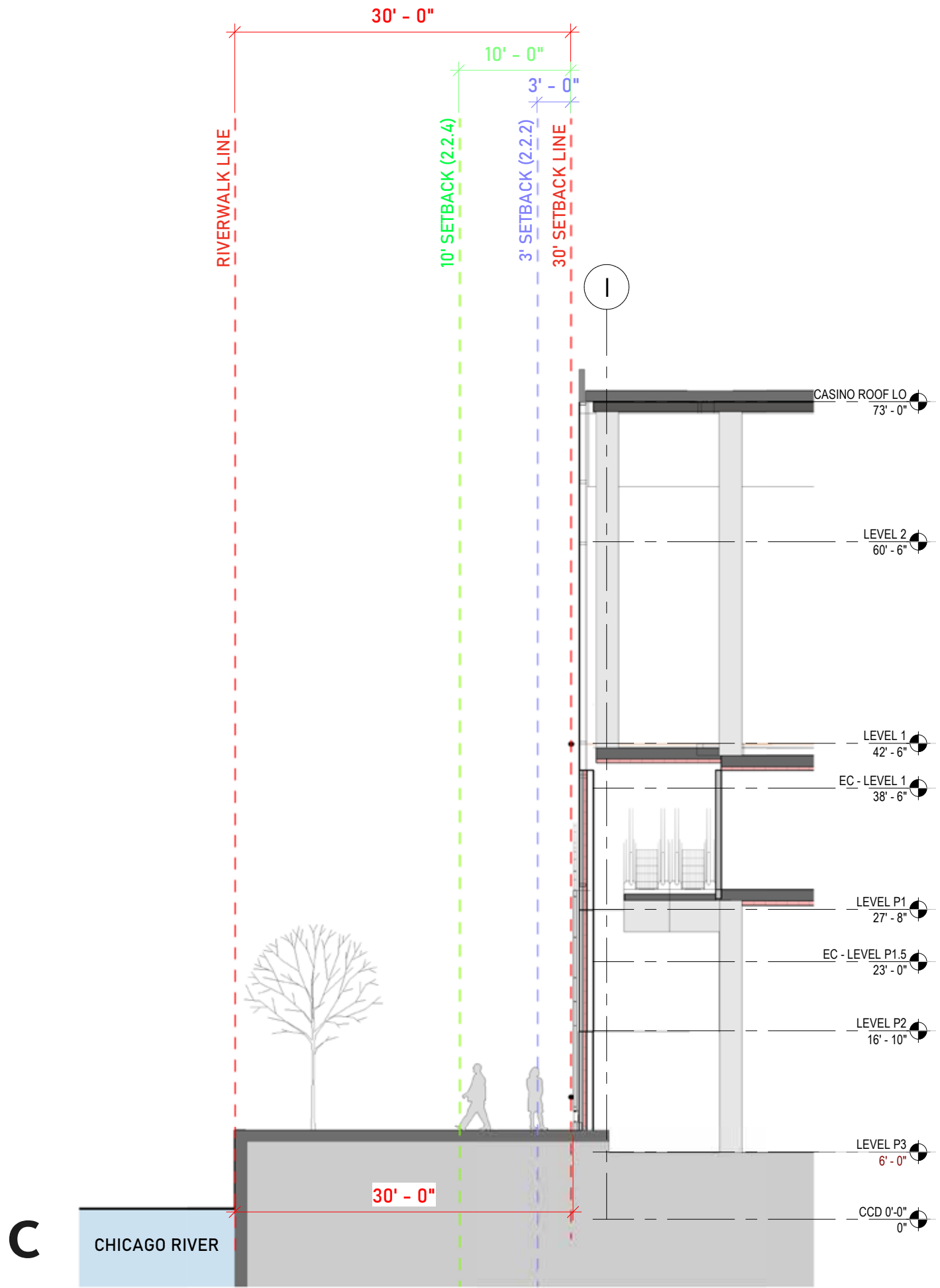
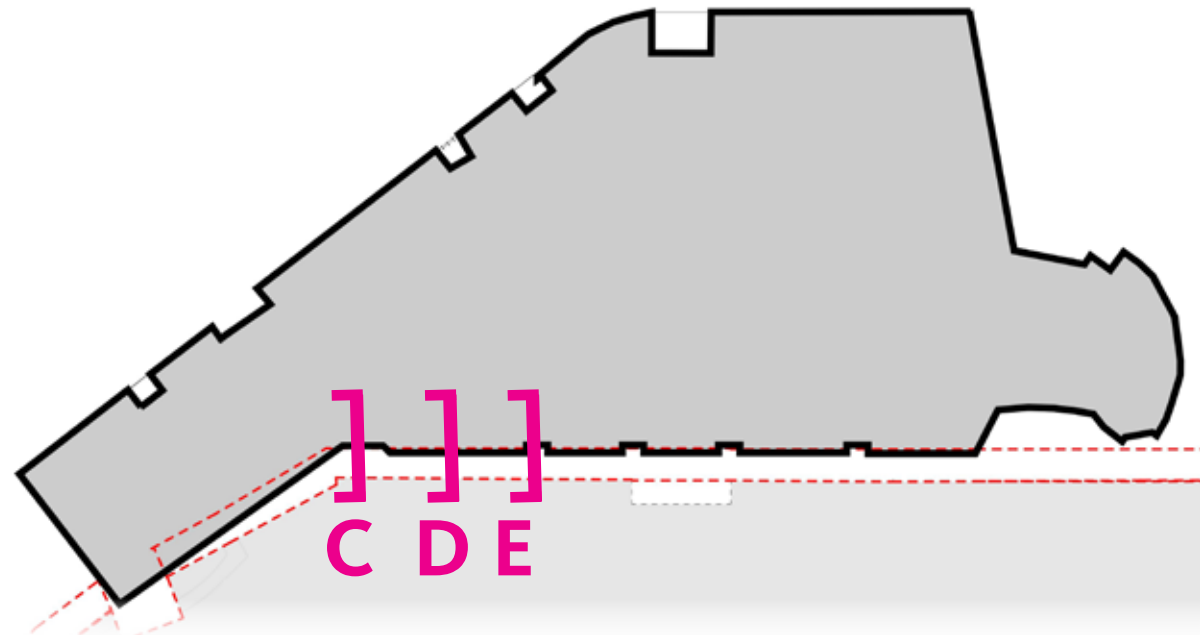
**PREVIOUSLY DISCUSSED
EVENT CENTER SETBACK
AND CONFIRMED WITH DPD
JANUARY 6, 2023**



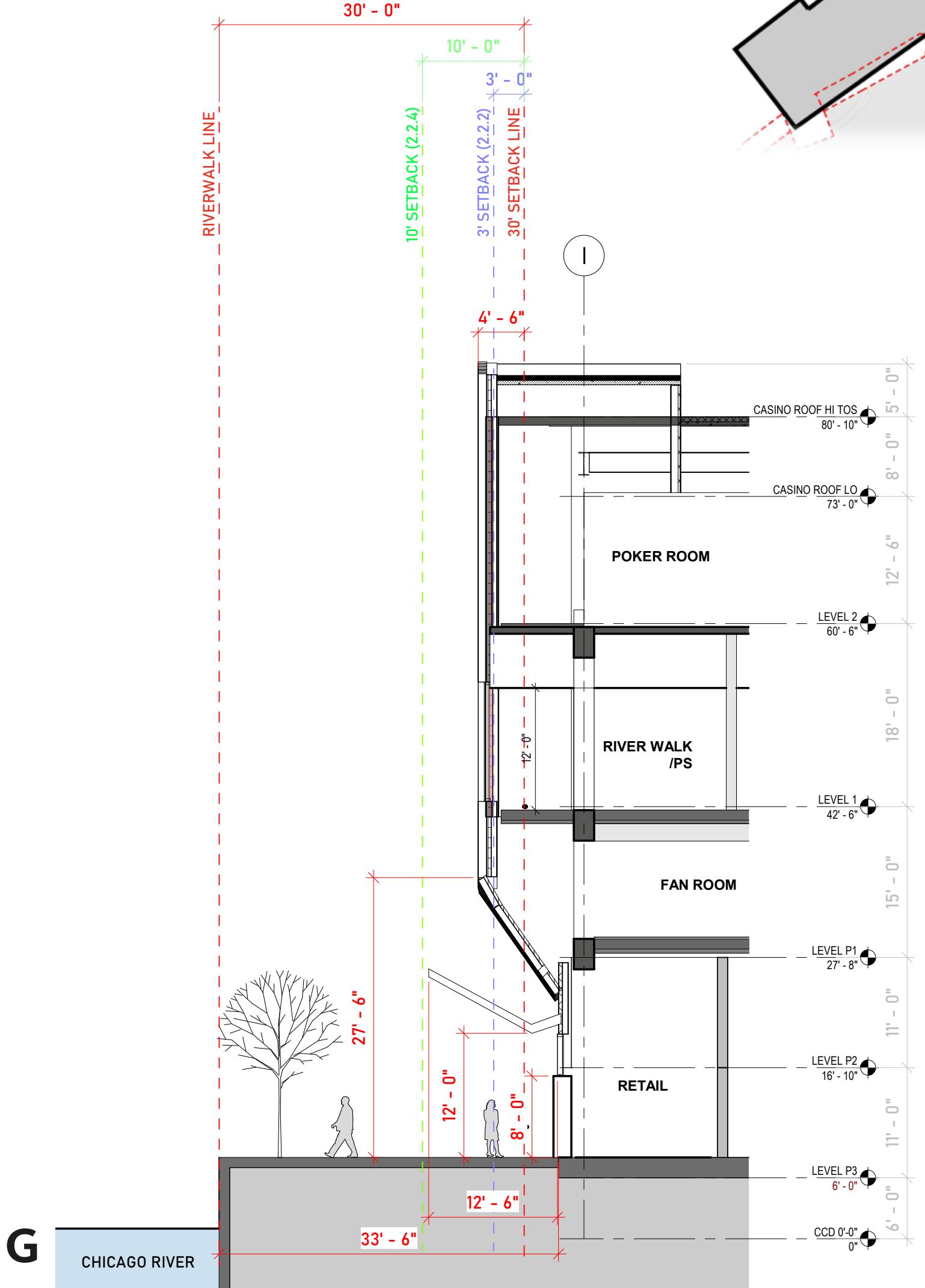
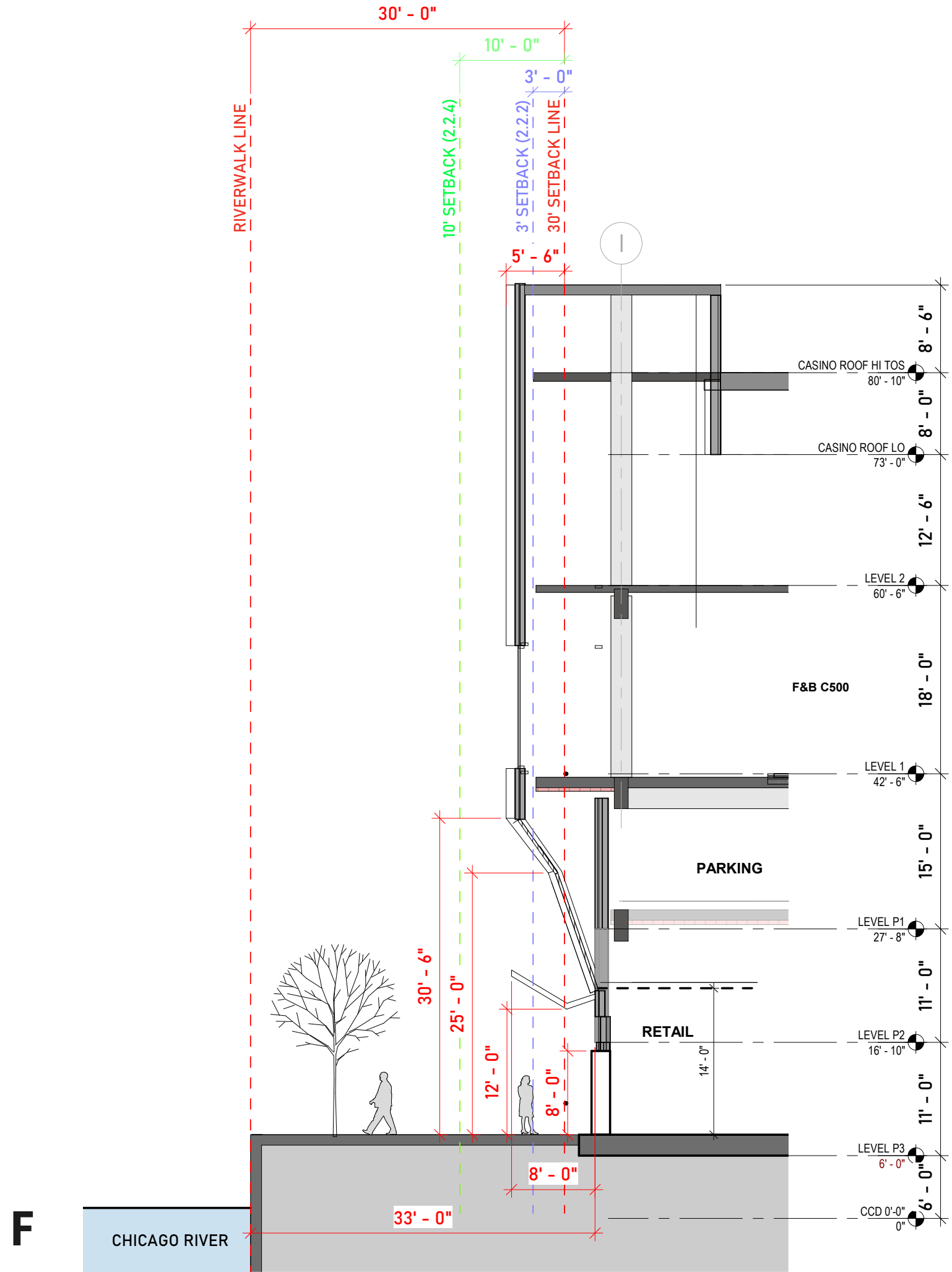
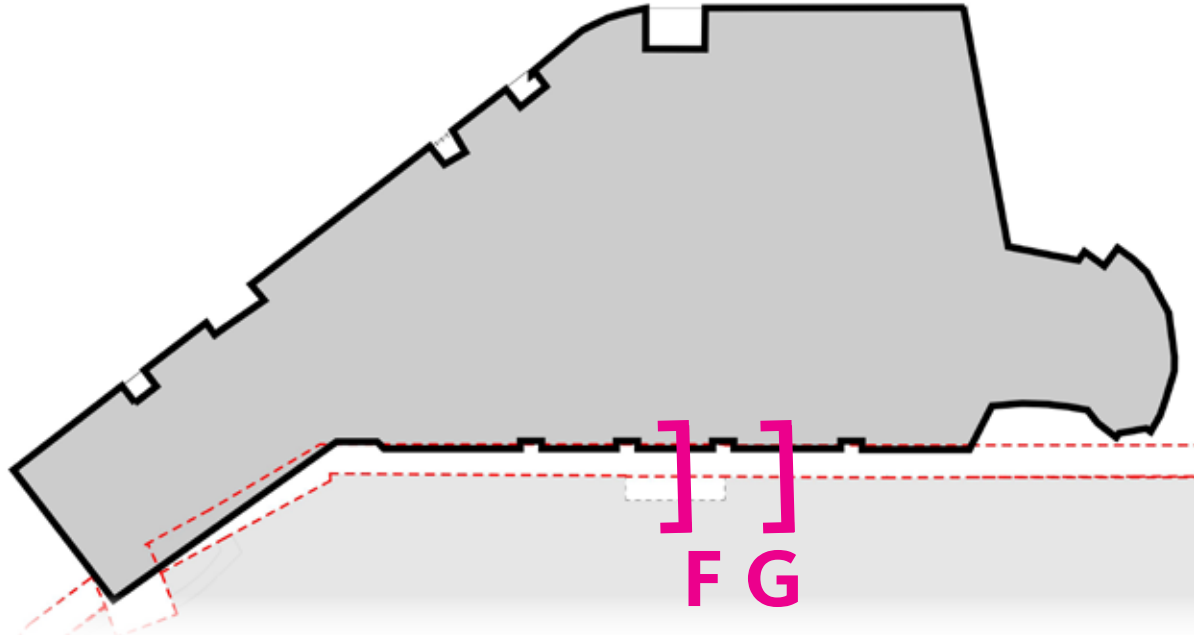
SETBACKS | AT RIVERWALK



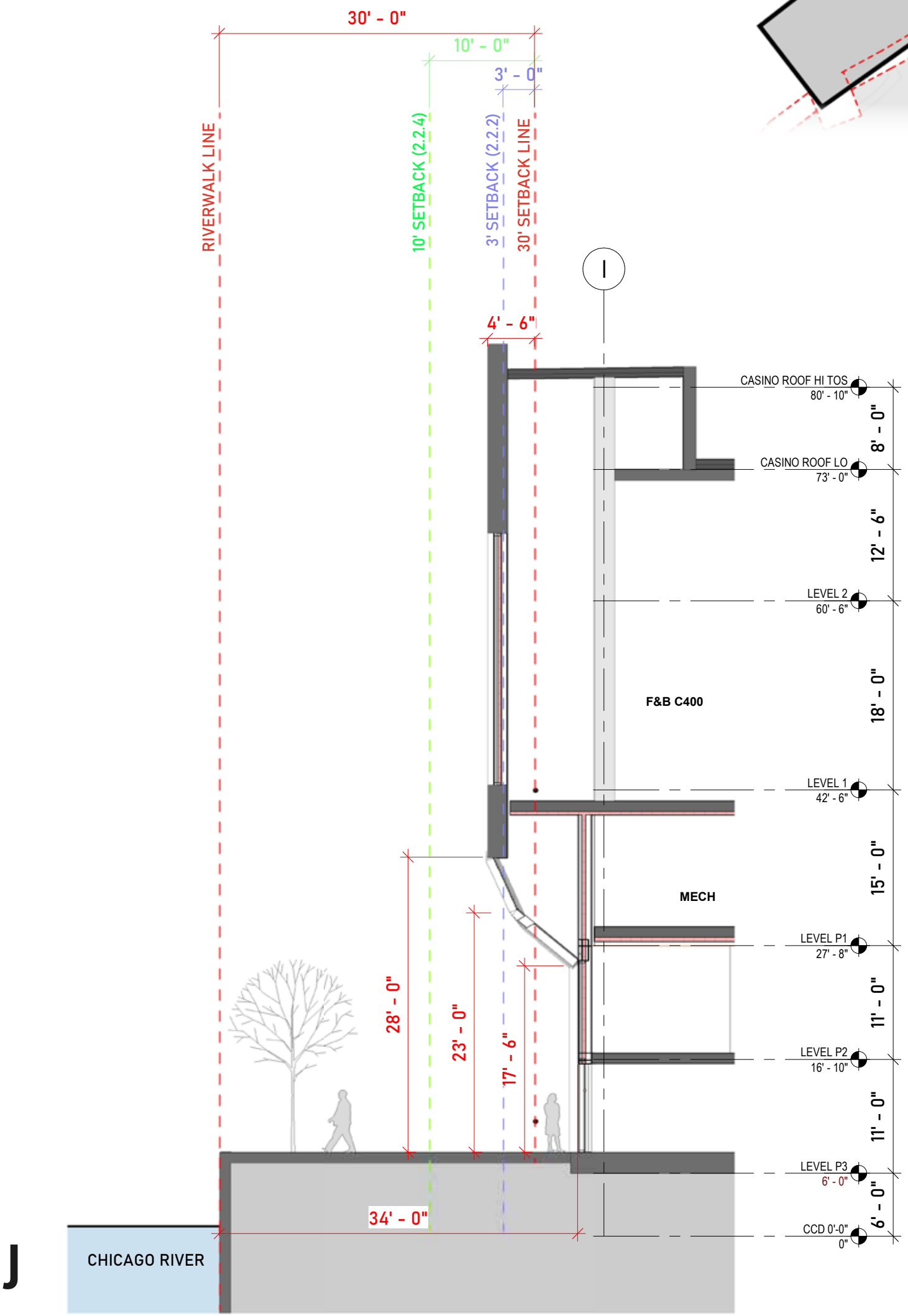
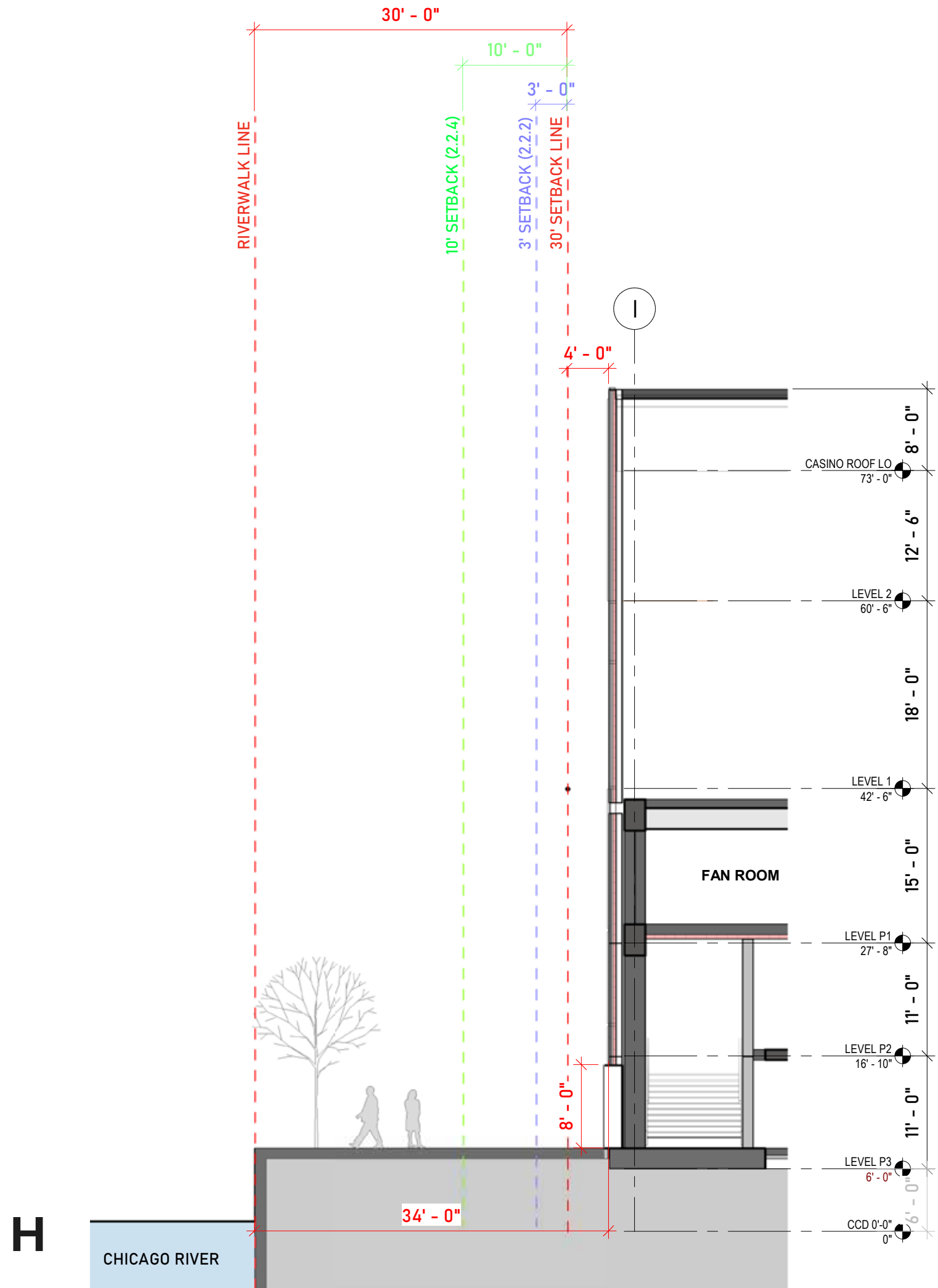
SETBACKS | AT MUSEUM



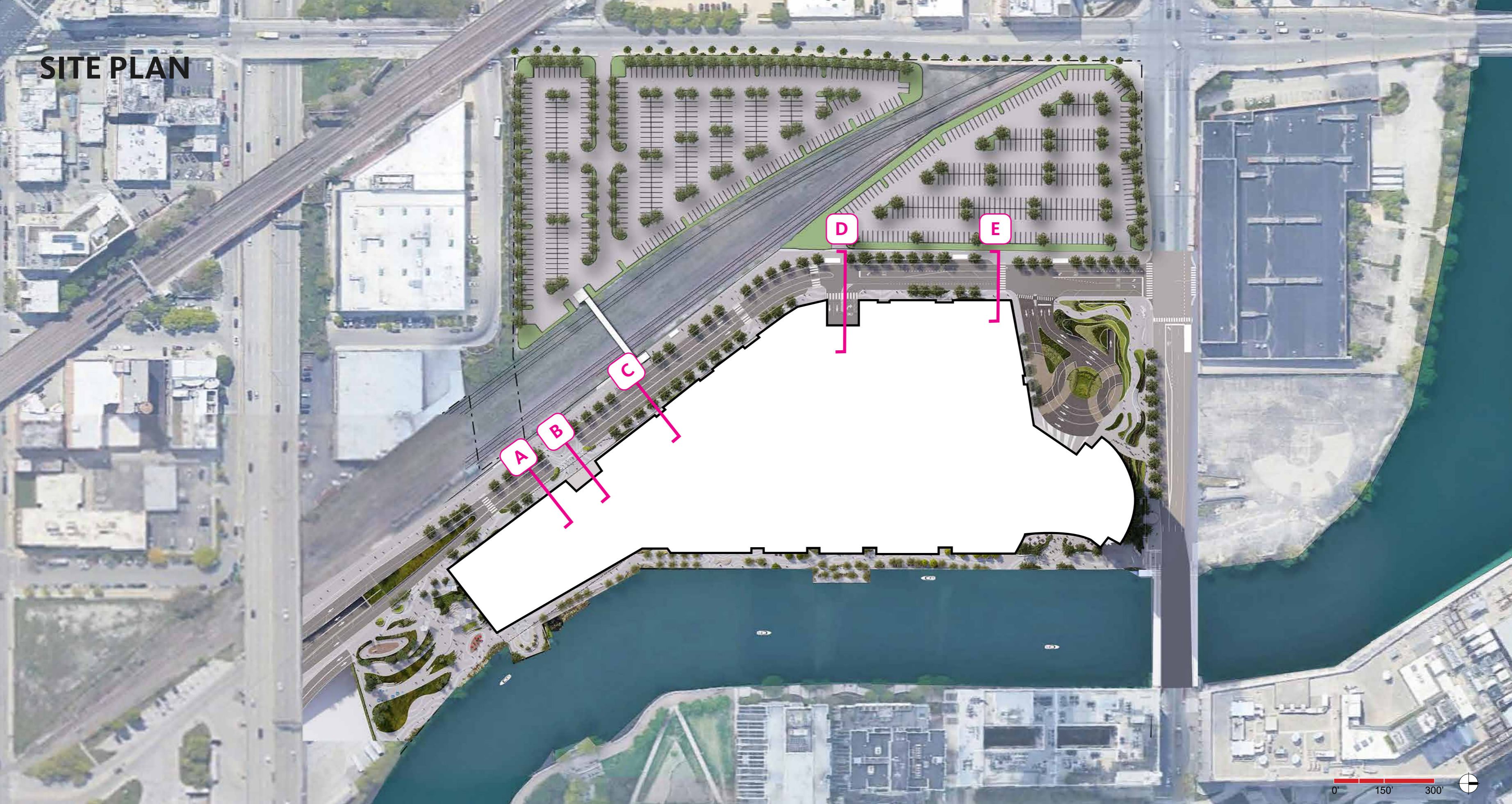
SETBACKS | AT RIVERWALK



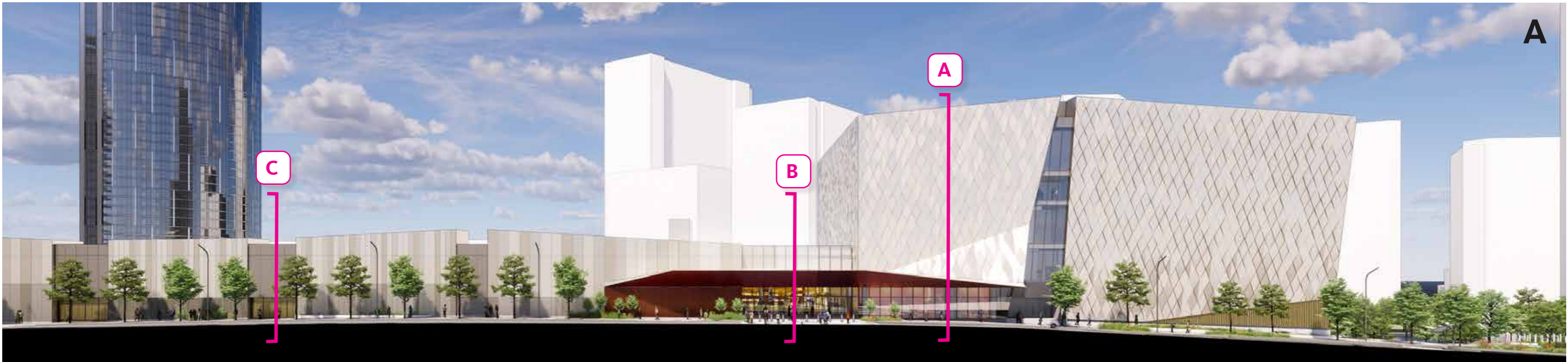
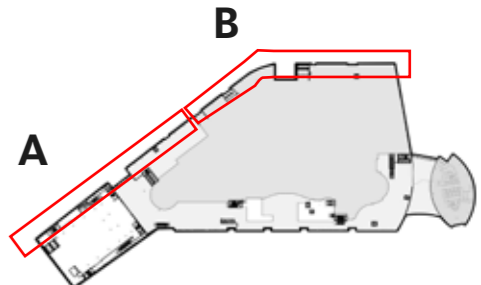
SETBACKS | AT RIVERWALK (NEAR BALLY'S LANDING)



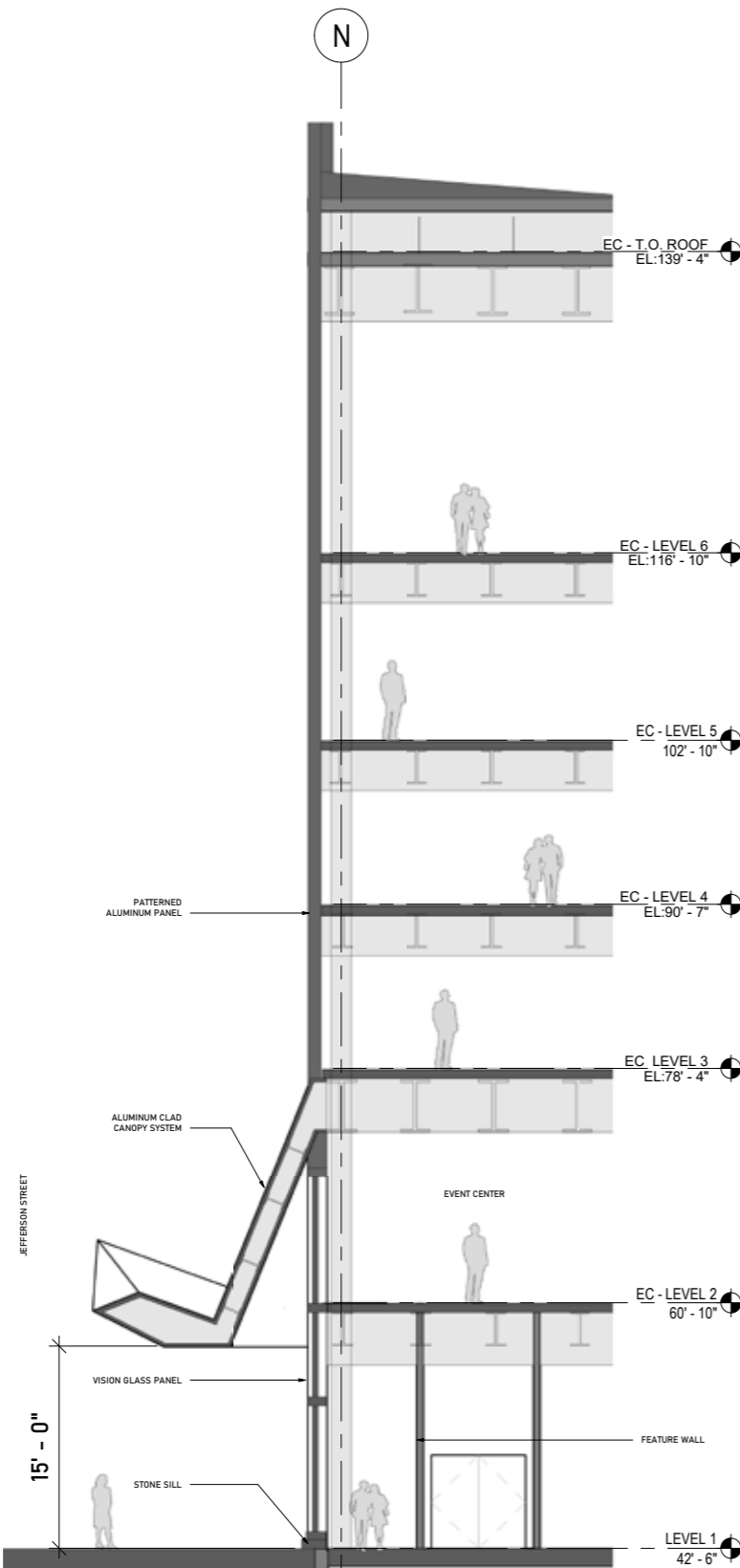
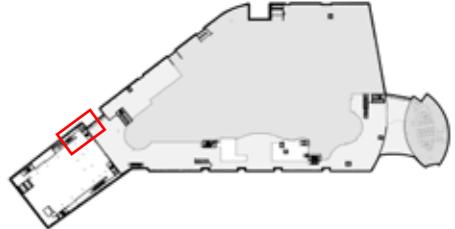
SITE PLAN



BALLY'S WAY | ELEVATIONS



A | EVENT CENTER | SECTION + VIGNETTE

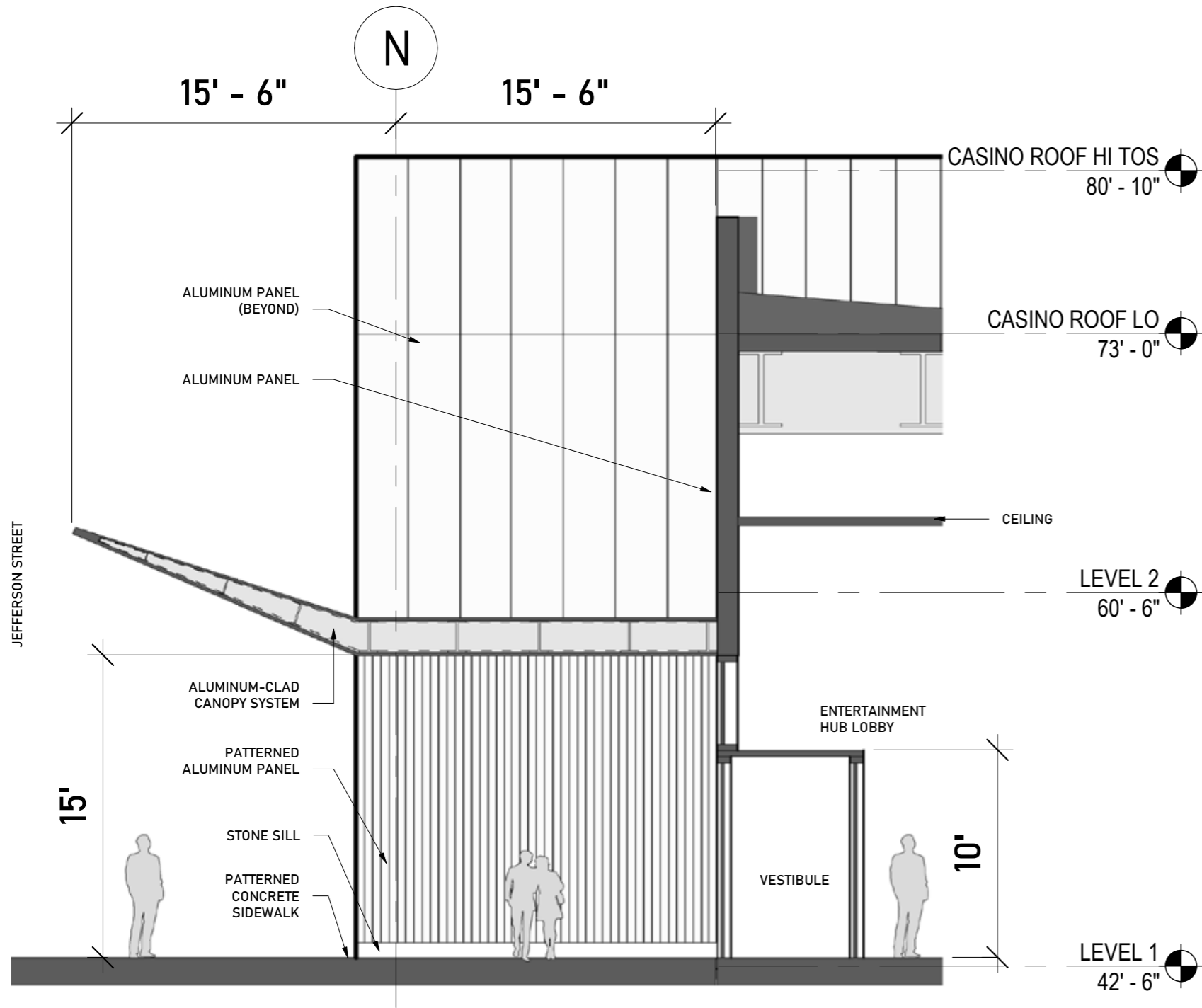


SECTION AT EVENT CENTER

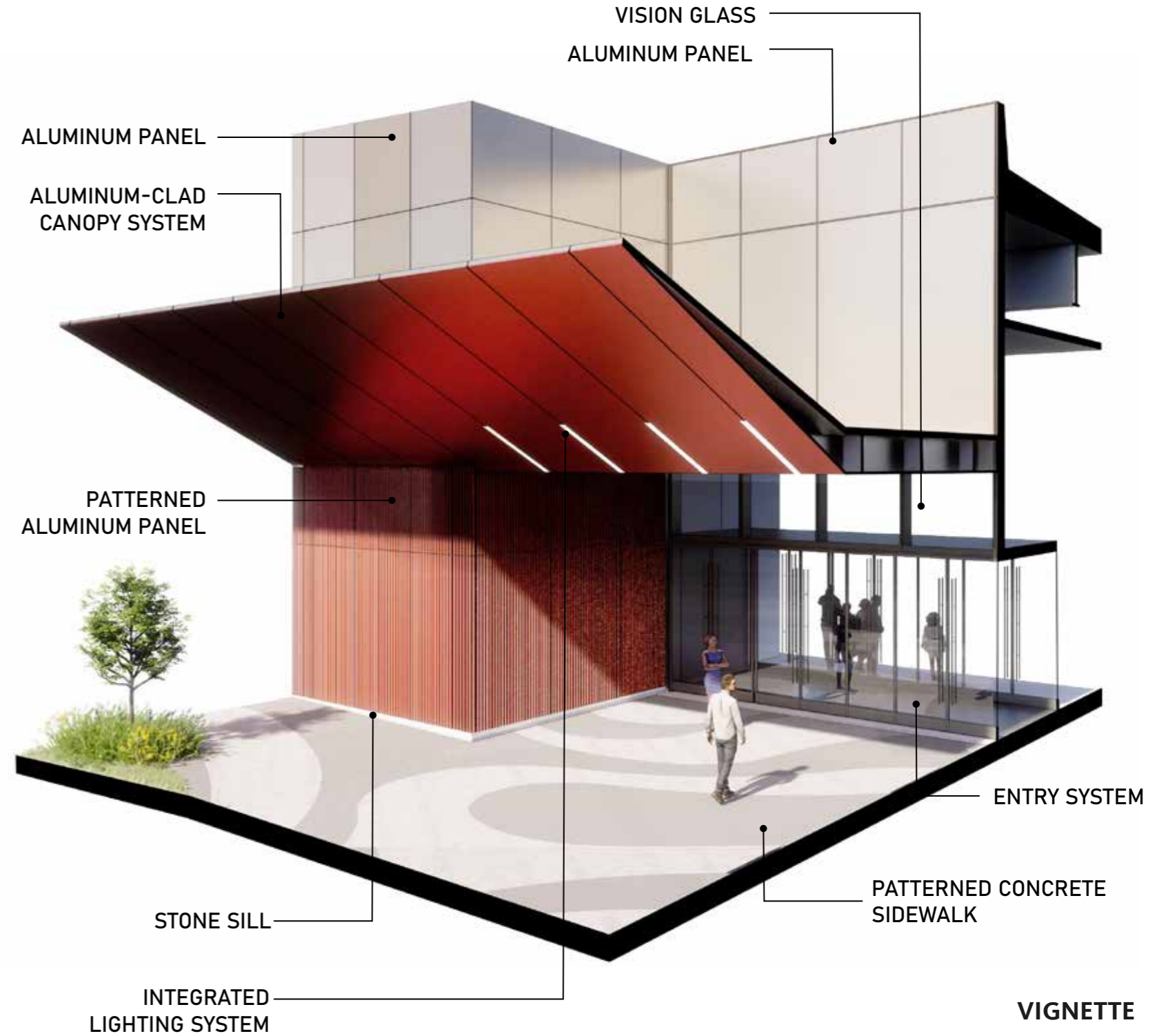
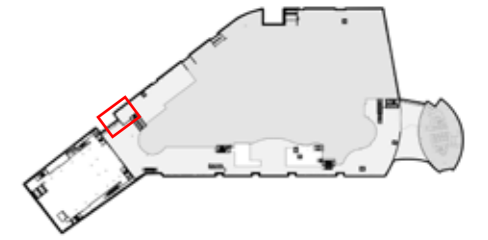


VIGNETTE

B | ENTERTAINMENT HUB ENTRY | SECTION + VIGNETTE

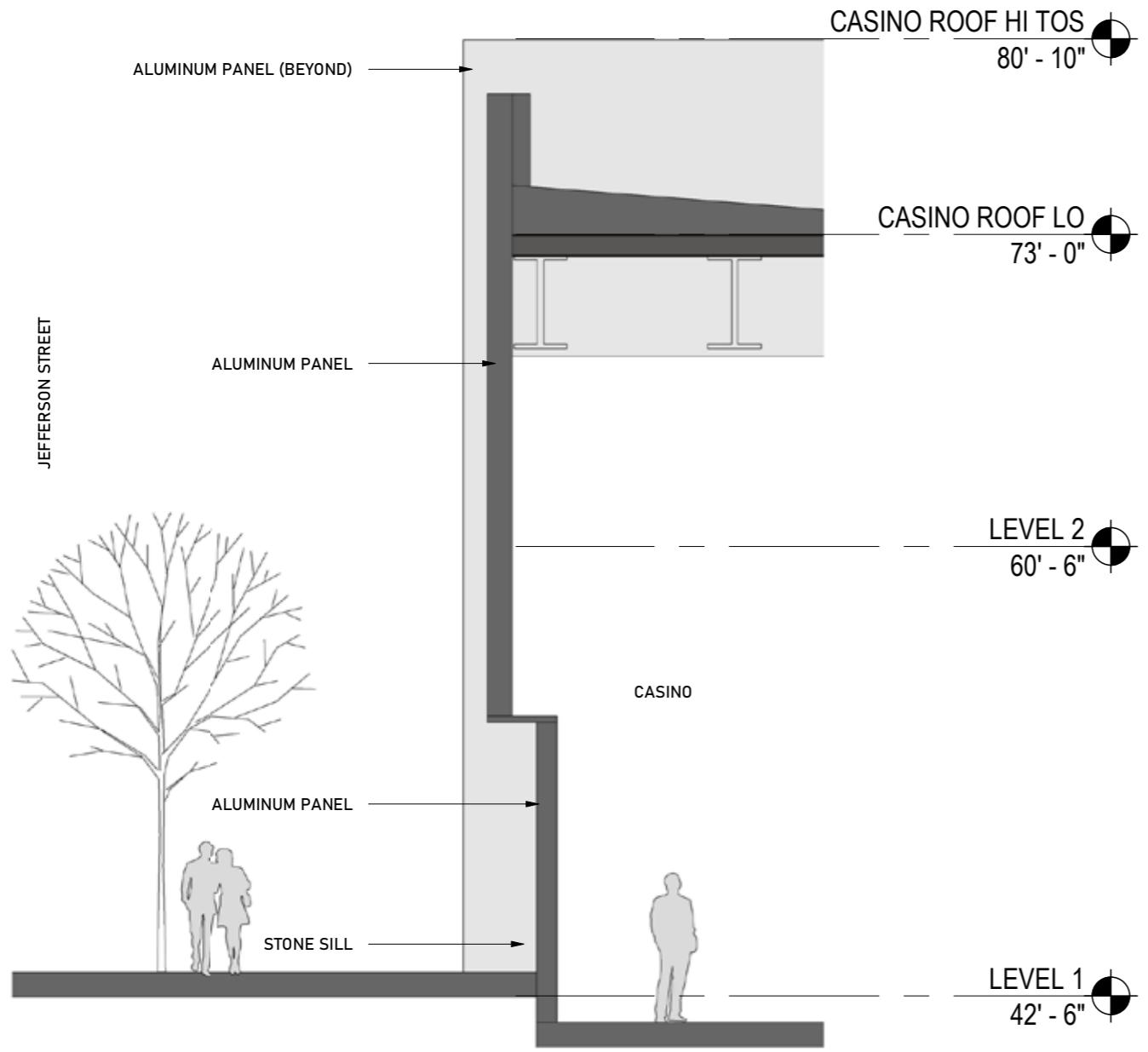
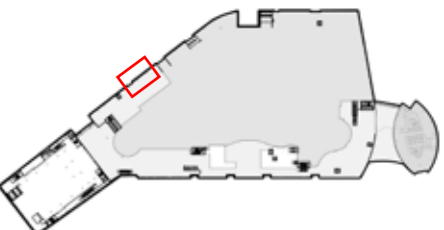


SECTION AT ENTERTAINMENT HUB ENTRY

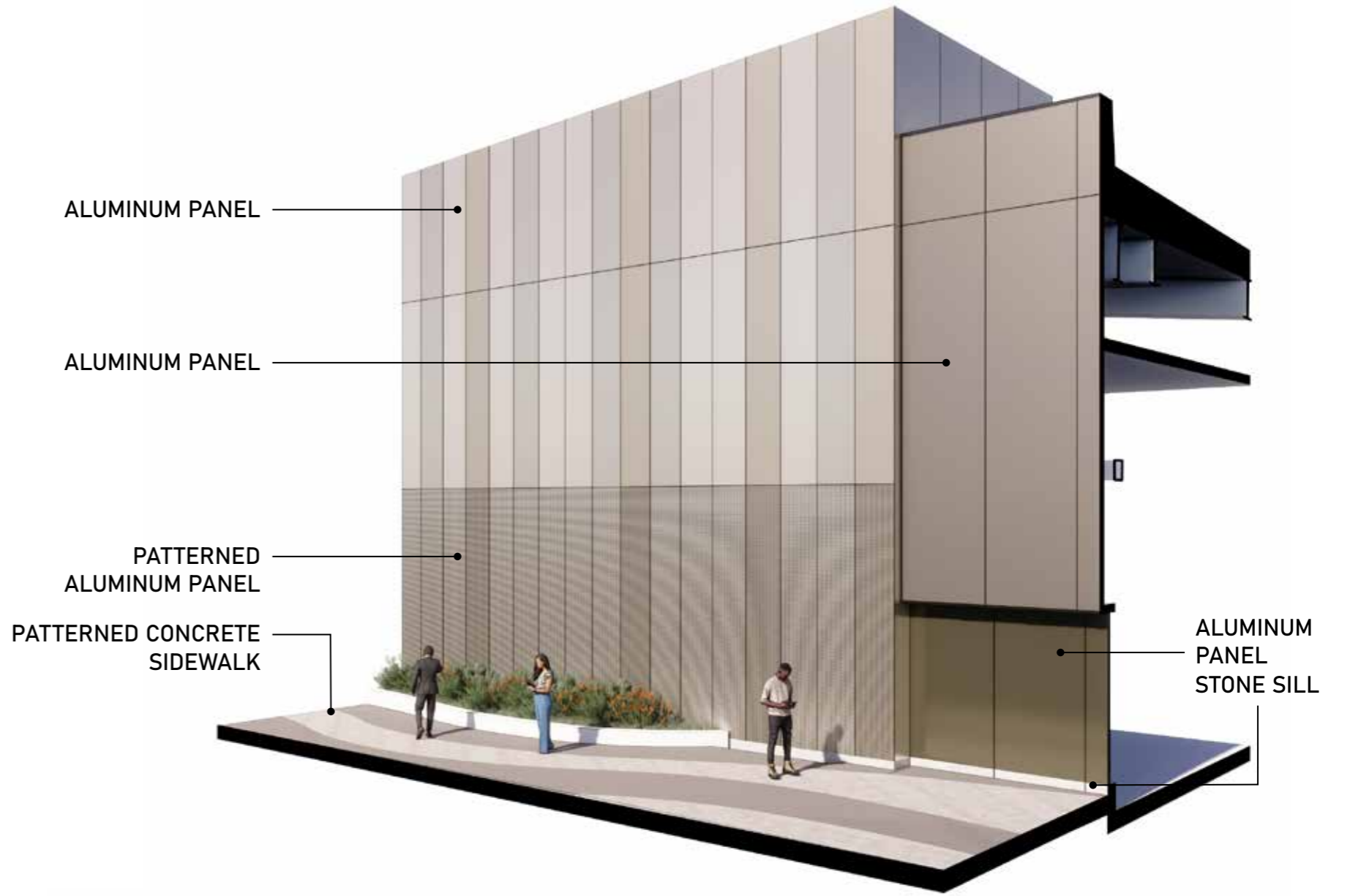


VIGNETTE

C | JEFFERSON STREET TYPICAL | SECTION + VIGNETTE

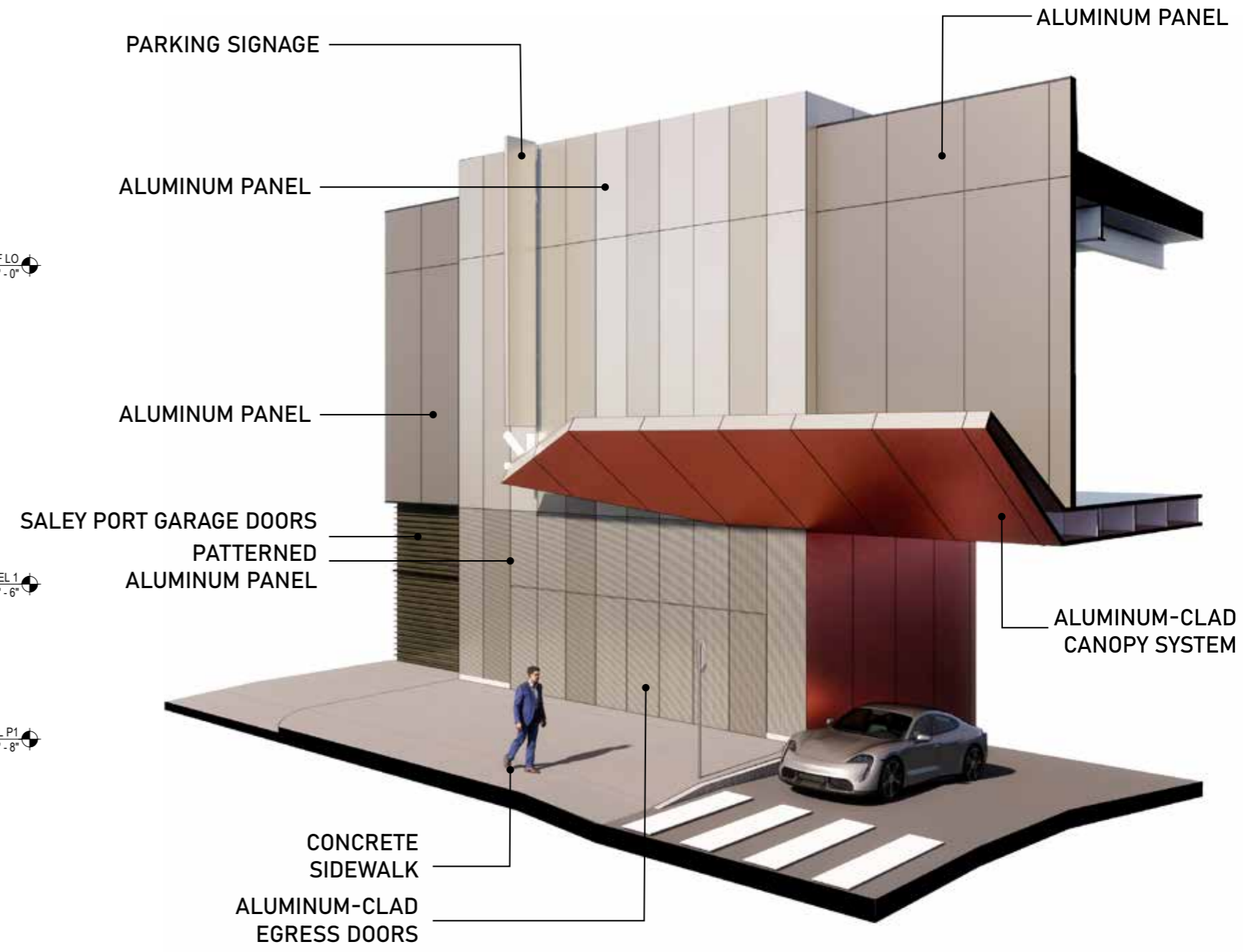
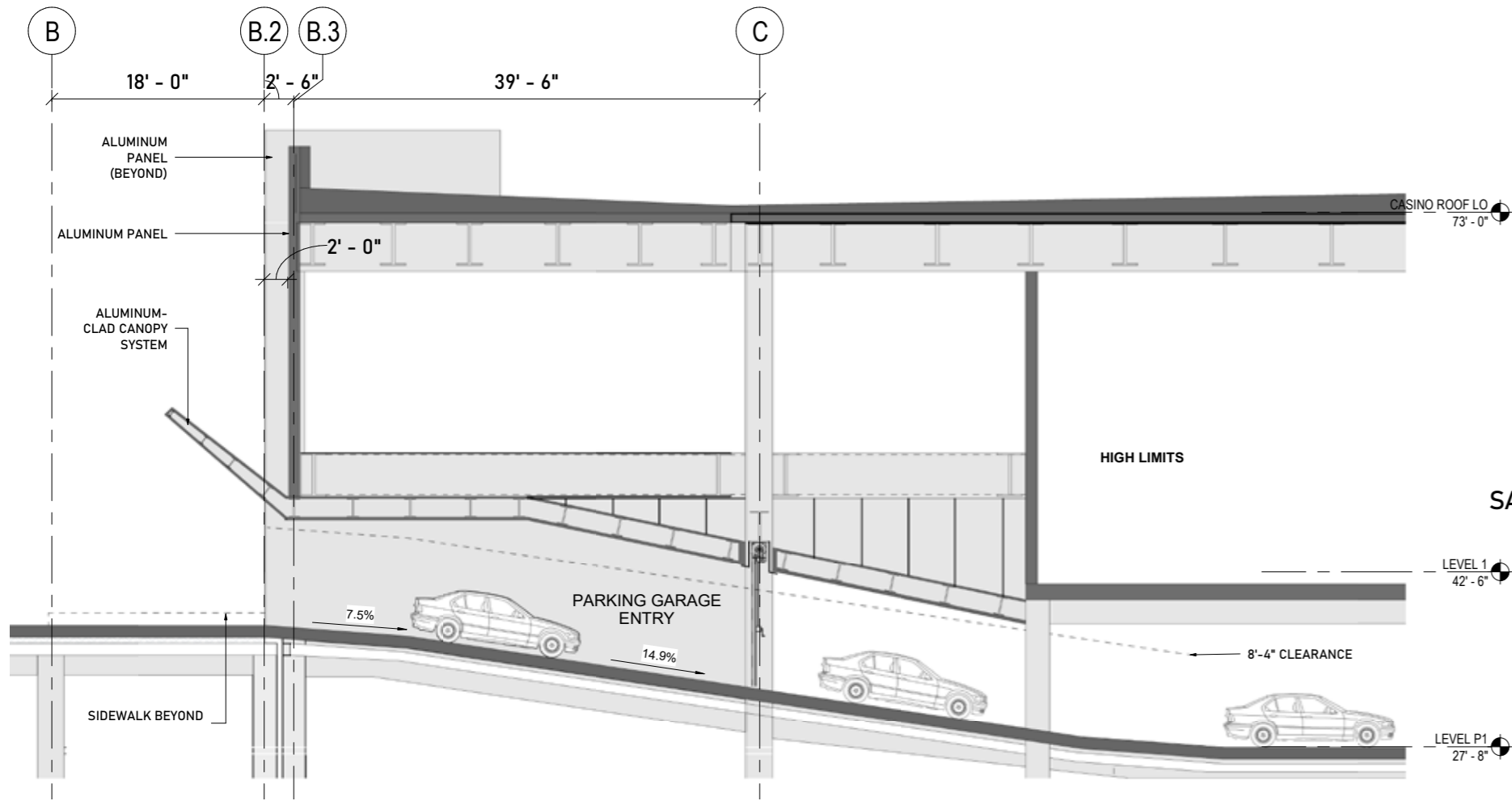
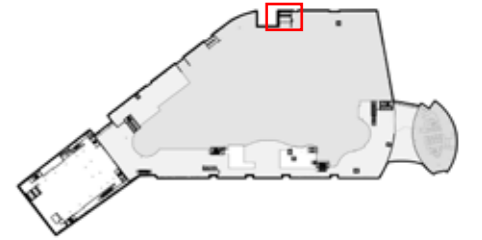


TYPICAL SECTION



VIGNETTE

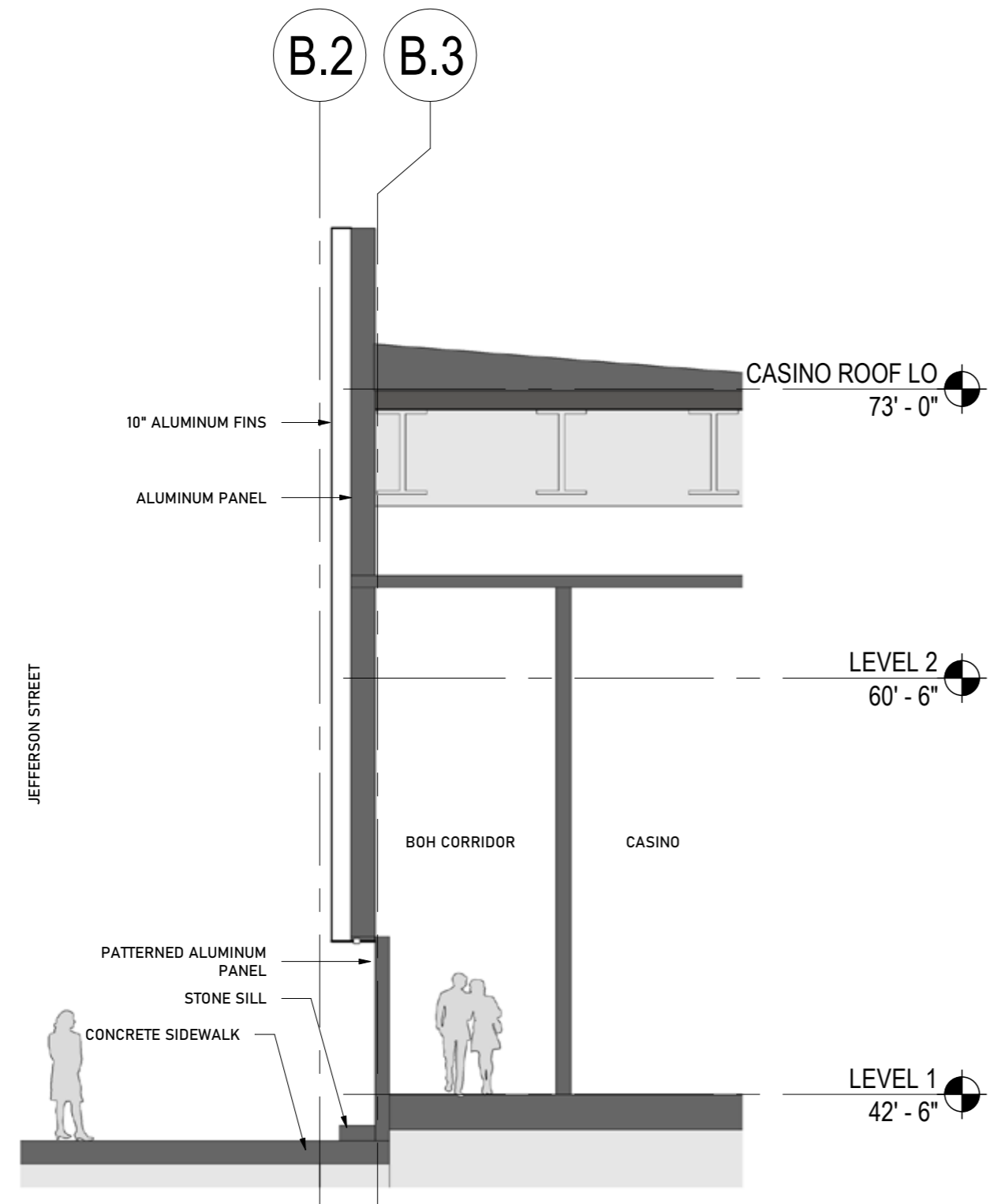
D | PARKING ENTRY | SECTION + VIGNETTE



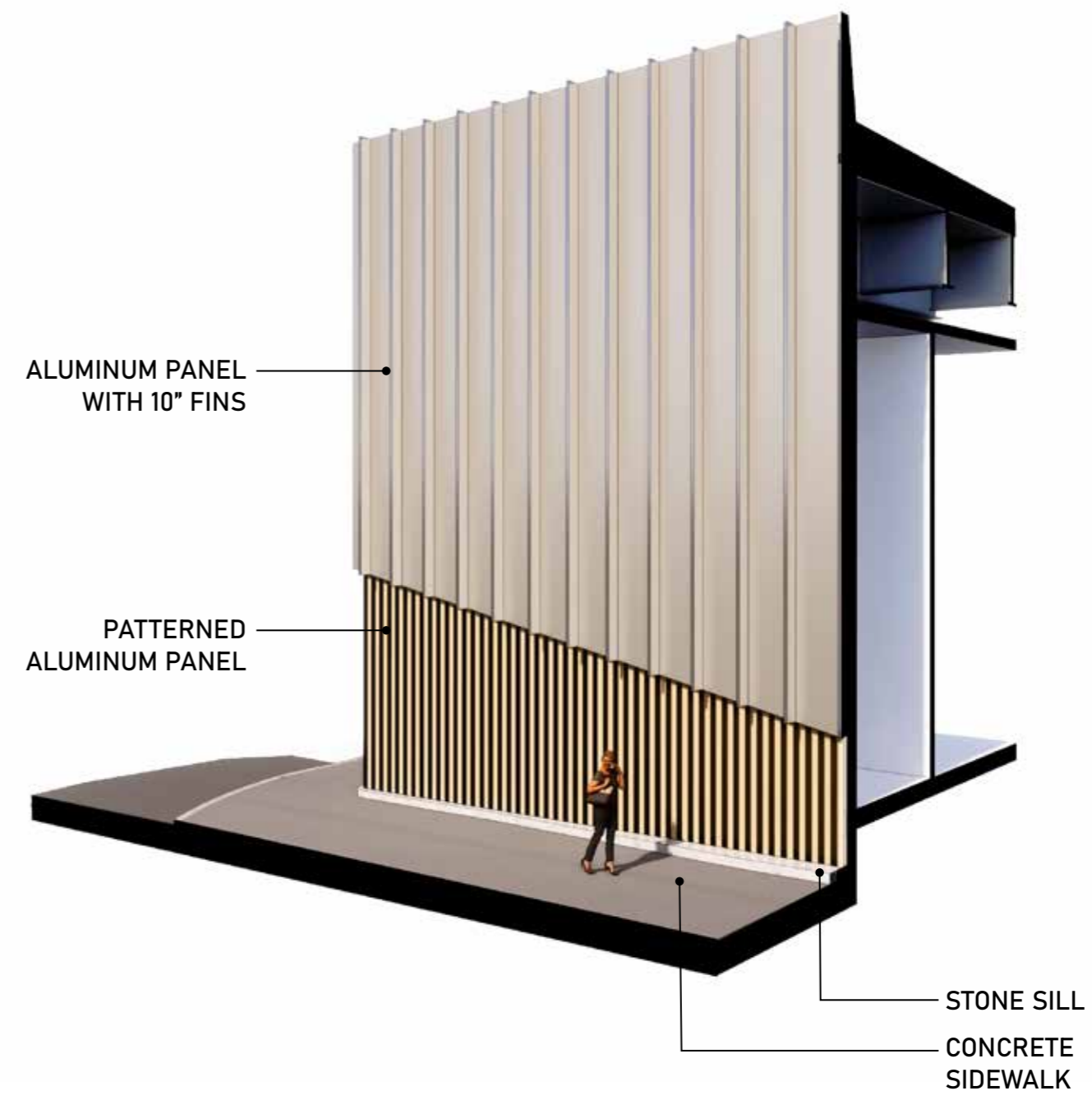
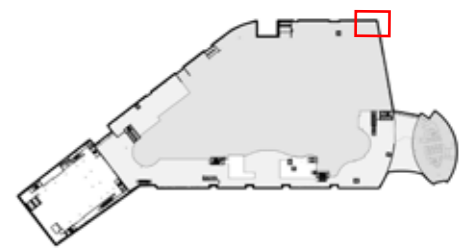
SECTION AT PARKING ENTRY

VIGNETTE

E | DROP-OFF AREA | SECTION + VIGNETTE



SECTION AT DROP-OFF AREA



VIGNETTE

AMENDMENT OF TITLE 17 OF MUNICIPAL CODE BY RECLASSIFICATION OF AREA SHOWN ON MAP NO. 1-F.

(As Amended)

(Application No. 21129)

WBRPD 1426,00

(Common Address: 643 -- 741 W. Chicago Ave., 641 -- 739 N. Halsted St., 632 -- 740 W. Erie St., 627 -- 661 W. Erie St., 501 -- 531 N. Desplaines St. And 524 -- 630 W. Grand Ave.)

[SO2022-2629]

(Committee Meeting Held December 13, 2022)

The Committee on Zoning, Landmarks and Building Standards submitted the following report:

CHICAGO, December 14, 2022.

To the President and Members of the City Council:

Presenting a report for your Committee on Zoning, Landmarks and Building Standards which held a meeting on December 13, 2022, the following items were passed by a majority of the members present:

Page 1 contains Document Number O2022-3777 for amendment of Municipal Code Titles 2, 4, 11, 14A, 14B, 17 and 18 and technical corrections to the *Journal of the Proceedings of the City Council of the City of Chicago* of April 21, 2021 (Ordinance Number O2021-1193) and September 21, 2022 (Ordinance Number SO2022-2008) regarding Chicago construction codes.

Page 1 also contains Document Number O2022-3729 for amendment of an ordinance (O2022-1869) for zoning reclassification of the area shown on Map Number 14-H at 1920 -- 1924 West 59th Street.

Page 2 contains Document Number O2022-2629 regarding a map amendment in the 27th Ward for addresses commonly known as 643 -- 741 West Chicago Avenue, 641 -- 739 North Halsted Street, 632 -- 740 West Erie Street, 627 -- 661 West Erie Street, 501 -- 531 North Desplaines Street and 524 -- 630 West Grand Avenue.

Pages 1 through 7 contain various map amendments in the 5th, 13th, 20th, 25th, 26th, 27th, 30th, 32nd, 33rd, 34th, 37th, 42nd, 43rd and 44th Wards.

Page 8 contains various large signs over 100 square feet in area and 24 feet above grade in the 11th, 28th and 43rd Wards.

I hereby move for passage of the proposed substitute ordinance transmitted herewith.

Respectfully submitted,

(Signed) THOMAS TUNNEY,
Chairman.

On motion of Alderman Tunney, the said proposed substitute ordinance transmitted with the foregoing committee report was *Passed* by yeas and nays as follows:

Yeas -- Aldermen La Spata, Dowell, Sawyer, Mitchell, Harris, Lee, Abarca, Quinn, Coleman, Moore, Curtis, O'Shea, Taylor, Brookins, Rodriguez, Tabares, Scott, Sigcho-Lopez, Burnett, Ervin, Taliaferro, Reboyras, Cardona, Waguespack, Rodriguez-Sanchez, Ramirez-Rosa, Villegas, Sposato, Nugent, Vasquez, Napolitano, Knudsen, Tunney, Gardiner, Cappleman, Martin, Osterman, Hadden, Silverstein -- 39.

Nays -- Aldermen Hopkins, Beale, Burke, Lopez, Reilly -- 5.

Alderman Cappleman moved to reconsider the foregoing vote. The motion was lost.

Alderman King abstained from voting on this item pursuant to Rule 14 of City Council's Rules of Order and Procedure stating that she is doing so out of an abundance of caution because her husband's law firm has represented Bally's in the past.

The following is said ordinance as passed:

Be It Ordained by the City Council of the City of Chicago:

SECTION 1. Title 17 of the Municipal Code of Chicago, Chicago Zoning Ordinance, is

amended by changing all the Air Rights Waterway-Business-Residential Planned Development Number 1426 symbols and indications as shown on Map Number 1-F in the area bounded by:

beginning at West Chicago Avenue; the North Branch of the Chicago River; a line 74.5 feet north of the north line of West Ohio Street extended; a line 451 feet east of the centerline of North Desplaines Street extended; a line 67.50 feet north of the north line of West Ohio Street extended; a line from a point 67.50 feet north of the north line of West Ohio Street extended and 411.96 feet east of the centerline of North Desplaines Street to a point 507.03 feet east of the centerline of North Desplaines Street and 13.0 feet south of the south line of West Ohio Street extended; a line 13.0 feet south of the south line of West Ohio Street extended; the North Branch of the Chicago River; West Grand Avenue; North Desplaines Street; the westerly right-of-way line of the Chicago and Northwestern Railway; the north line of West Ohio Street; a line from a point 60.57 feet west of the westerly right-of-way line of the Chicago and Northwestern Railway along the north line of West Ohio Street to a point and from a point 55.79 feet west of the westerly right-of-way line of the Chicago and Northwestern Railway along a line 67.50 feet north of the north line of West Ohio Street to a point (said line runs diagonally to the northwest for a distance of 84.92 feet); a line 67.50 feet north of the north line of West Ohio Street; the westerly right-of-way line of the Chicago and Northwestern Railway, the south right-of-way line of relocated West Erie Street; the westerly right-of-way line of the Chicago and Northwestern Railway; the northerly right-of-way line of relocated West Erie Street; North Union Avenue; and North Halsted Street (ToB),

to those of an Air Rights Waterway-Business-Residential Planned Development Number 1426, as amended, which is hereby established in the area described, subject to such use and bulk regulations as are set forth in the Plan of Development attached herewith and made a part thereof and to no others.

SECTION 2. This ordinance shall be in full force and effect from and after its passage, due publication and upon all approvals necessary by State of Illinois to conduct a casino gambling operation within Air Rights Waterway-Business-Residential Planned Development Number 1426, as amended.

Plan of Development Statements referred to in this ordinance read as follows:

Air Rights Waterway-Business-Residential Planned Development No. 1426, As Amended.

Plan Of Development Statements.

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1. The area delineated herein as Air Rights Waterway Business Residential Planned Development Number 1426 ("Planned Development") consists of approximately 1,184,654 net square feet of property (27.20 acres) which is depicted on the attached Planned Development Boundary and Property Line Map ("Property") and is owned and controlled by the Applicant, Bally's Chicago Operating Company, LLC (the "Applicant") under 99 year ground lease.
2. The requirements, obligations and conditions contained within this Planned Development shall be binding upon the Applicant, its successors and assigns and, if different than the Applicant, the legal title holders and any ground lessors. All rights granted hereunder to the Applicant shall inure to the benefit of the Applicant's successors and assigns and, if different than the Applicant, the legal title holder and any ground lessors. Furthermore, pursuant to the requirements of Section 17-8-0400 of the Chicago Zoning Ordinance, the Property, at the time of application for amendments, modifications or changes (administrative, legislative or otherwise) to this Planned Development are made, shall be under single ownership or designated control. Single designated control is defined in Section 17-8-0400 of the Zoning Ordinance, provided, however, (a) that for so long as the Applicant or any affiliate thereof owns or controls any part of the Property, any application to the City for any such changes or modifications (administrative, legislative or otherwise) must in all cases be additionally authorized by the Applicant and (b) such change or modification (administrative, legislative or otherwise) shall comply with the terms of any zoning rights agreement, deed restriction, or other written agreement by the Applicant or between owners or designated controlling parties of the Property or portions thereof. Where portions of the improvements located on the Property have been submitted to the Illinois Condominium Property Act, the term "owner" shall be deemed to refer solely to the condominium association of the owners of such portions of the improvements and not to the individual unit owners therein. Nothing herein shall prohibit or in any way restrict the alienation, sale or any other transfer of all or any portion of the Property or any rights, interests or obligations therein including any ground or air-rights leases. Upon any alienation, sale or any other transfer of all or any portion of the Property or the rights therein including any ground or air-rights leases (but not including an assignment or transfer of rights pursuant to a mortgage-or otherwise as collateral for any indebtedness) and solely with respect to the portion of the Property so transferred, the term "Applicant" shall be deemed amended to apply to the transferee thereof (and its beneficiaries if such transferee is a land trust) and the seller or transferor thereof (and its beneficiaries if such seller or transferor is a land trust) shall thereafter be released from any and all obligations or liability hereunder; provided, however that the Applicant's right to authorize changes or modifications to this Planned Development for so long as it owns or controls all or any portion of the Property as set forth in clause (a) of this Statement Number 2 above shall not be deemed amended or transferred to apply to a transferee (or its beneficiaries as aforesaid) unless expressly assigned in a written instrument executed by the original Applicant hereunder.

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3. All applicable official reviews, approvals or permits are required to be obtained by the Applicant or its successors, assignees or grantees. Any dedication or vacation of streets or alleys or grants of easements or any adjustment of the right-of-way shall require a separate submittal to the Department of Transportation on behalf of the Applicant or its successors, assign or grantees.

Any requests for grants of privilege, or any items encroaching on the public way, shall be in compliance with the Planned Development. Ingress or egress shall be pursuant to the Planned Development and may be subject to the review and approval of the Departments of Planning and Development and Transportation. Closure of all or any public street or alley during demolition or construction shall be subject to the review and approval of the Department of Transportation.

All work proposed in the public way must be designed and constructed in accordance with the Department of Transportation Construction Standards for Work in the Public Way and in compliance with the Municipal Code of the City of Chicago. Prior to the issuance of any Part II approval, the submitted plans must be approved by the Department of Transportation. Pursuant to a negotiated and executed Perimeter Restoration Agreement ("Agreement") by and between the Department of Transportation's Division of Infrastructure Management and the Applicant, the Applicant shall provide improvements and restoration of all public way adjacent to the property, which may include, but not be limited to, the following as shall be reviewed and determined by the Department of Transportation's Division of Infrastructure Management:

- Full width of streets
- Full width of alleys
- Curb and gutter
- Pavement markings
- Sidewalks
- ADA crosswalk ramps
- Parkway & landscaping

The Perimeter Restoration Agreement must be must be executed prior to any Department of Transportation and Planned Development Part II review permitting. The Agreement shall reflect that all work must comply with current Rules and Regulations and must be designed and constructed in accordance with the Department of Transportation's Construction Standards for work in the Public Way and in compliance with the Municipal Code of Chicago Chapter 10-20. Design of said improvements should follow the Department of Transportation's Rules and Regulations for Construction in the Public Way as well as The Street and Site Plan Design Guidelines. Any variation in scope or design of public way improvements and restoration must be approved by the Department of Transportation.

- The Applicant commits to provide and fully-fund the following infrastructure improvements during the course of the development Subarea A and in accordance with Statement 8F below. All infrastructure improvements will be subject to review and approval by the Department of Transportation and must be designed and constructed in

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accordance with the current Department of Transportation Construction Standards for Work in the Public Way and in compliance with the Municipal Code of the City of Chicago. This includes all required pavement markings or lane reconfigurations as necessary, as approved by the Department of Transportation.

New Streets & publicly accessible Private Drives

- Construct Jefferson Street from Grand Avenue to Chicago Avenue
- Construct Huron Street from Halsted Street to Jefferson Street contingent on the following:

The applicant commits to the design, construction and dedication of Huron Street, from Halsted Street to Jefferson Street, within 20 months following approval by the underlying property owner to reduce its clearance requirement to 19', or to a point that allows a design mutually acceptable to the Applicant and the Department of Transportation. Such approval from the underlying property owner shall be obtained in coordination with the Chicago Department of Transportation. If the approval from the underlying property owner is not obtained prior to the completion of the development of Subarea A, the design and construction of the east/west extension of Huron Street shall be completed prior to obtaining the Certificate of Occupancy of the development of parcels B-4 or B-5. The infrastructure improvement will be subject to review and approval by the Department of Transportation in a timely manner and must be designed and constructed in accordance with the Department of Transportation Construction Standards for Work in the Public Way and in compliance with the Municipal Code of the City of Chicago.

Traffic Signal Improvements

- Install new traffic signal at Grand Avenue and Jefferson Street
- Install new traffic signal at Chicago Avenue and Jefferson Street with interconnect to Chicago Avenue and Halsted Street Signal
- Install new traffic signal at Grand Avenue and Desplaines Avenue
- Retime traffic signal to improve traffic flow at Division Street and Halsted Street
- Retime traffic signal to improve traffic flow at North Branch Street and Halsted Street
- Retime traffic signal to improve traffic flow at Chicago Avenue and Larrabee Street
- Full modernization of traffic signal to improve traffic flow at Ogden Avenue and Chicago Avenue
- Full modernization of traffic signal to improve traffic flow at Milwaukee Avenue and Chicago Avenue
- Full modernization of traffic signal to improve traffic flow at Milwaukee Avenue and Ogden Avenue
- Retime traffic signal to improve traffic flow at Orleans Street and Ontario Street
- Retime traffic signal to improve traffic flow at Orleans Street and Ohio Street
- Retime traffic signal to improve traffic flow at Orleans Street and Grand Avenue
- Retime traffic signal at Grand Avenue, Halsted Street, and Milwaukee Avenue

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The applicant acknowledges that any traffic signal controllers requiring retiming must be upgraded to ATC 1000 controllers with all activated electrical components if not already installed and must have pedestrian count down timers, and signal arrows installed if not currently present and operable.

Other Mobility Infrastructure

- Fund and provide space for the installation of one Divvy bike share station with no less than 23 racks.

Dedication of all public right of way for roadways throughout the site must be planned for each subarea and completed in accordance with the Development Subarea Diagram. All application fees and the construction of the public infrastructure to be dedicated are the responsibility of the Applicant. All changes or modifications to the future infrastructure improvements and / or dedications may be approved administratively, pursuant to Section 17-13-0611-A, without amending the PD. Furthermore transit way planning and implementation will continue throughout the life of the project and coordination between the Applicant and CDOT will be necessary.

The Applicant acknowledges that a roadway connecting Jefferson Street to Desplaines Street shall be dedicated between subareas C3 and D5 at a future date. The specific orientation and design of which will be determined at a later date, no later than at the time of site plan approve of those sites.

The Applicant acknowledges that, for any privately owned roadway necessary for public access and circulation, it will provide an easement for site egress of pedestrians, bicycles, and vehicular traffic within and across the site for public use, the terms of which will be determined in coordination with the City and Department of Transportation. This easement or agreement will outline the maintenance responsibilities of either party in those instances.

The Applicant acknowledges Erie Street, within the existing dedicated right of way, must be built to connect to Jefferson Street prior to the development of subareas B7 and C1.

The Applicant commits to provide and fully-fund the Subarea A traffic demand measures and infrastructure improvements, in accordance with and as detailed in this Statement 3 above (all such measures and improvements are collectively referred to herein as the "Improvements"). The funding, design and provision of all such Improvements are the responsibility of the Applicant and its successors and assigns, subject to review and installation scheduling by the Department of Transportation. The Applicant must design and construct the Improvements in accordance with the Department of Transportation Construction Standards for work in the Public Way and in compliance with the Municipal Code of Chicago.

The Applicant shall enter into an agreement with the Department of Transportation to implement the provisions of this Statement 3. Such agreement shall be recorded against the Property prior to the issuance of the issuance of Part II approval for any Parcel in Subarea A. The approval of subsequent subareas may require new or updated traffic counts, traffic impact studies, and infrastructure improvements mitigating any further traffic impacts, as determined by the Department of Transportation.

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A second, 23 rack Divvy station must be installed within the subsequent phases, at the direction of the Department of Transportation.

The applicant agrees to coordinate with both the Department of Transportation and Department of Planning and Development to implement future infrastructure improvements related to this PD. Roadway and signal planning and design will continue throughout the life of the project and coordination between the applicant and CDOT will be necessary. Site design and access, and right-of-way design are subject to change and CDOT approval. Modifications to site plans may be made pursuant to the minor change provisions of section 17-13-0611.

The applicant agrees to annual reporting on transportation operations and travel demand management strategies for the entertainment district, following the completion of Subarea A in the form of a memo to the Chicago Department of Transportation, coordinating with the department on implementation of any new strategies or initiatives.

4. This Plan of Development consists of 21 Statements: a Bulk Regulations Table; Existing Zoning Map, Existing Land Use Map, General Land Use Plan, Planned Development Boundary and Property Map, Right-of Way Adjustment Map, Huron / Jefferson Cross Section, Plan, Development Plan with Associated Infrastructure Requirements, Subarea and Parcelization Plan, Development Phasing Diagram, Phase 1: Interim Site Plan for Parking and Riverwalk, Phase 1: Interim Site Plan for Parking, Site Access and Load Plan: Level 1 (0.0 to 30.0 CCD), Site Access and Load Plan: Level 2 (+30.0 to 45.0 CCD), Site Level Plan: Level 1 (0.0 to + 30.0 CCD), Site Level Plan: Level 2 (+38.0 to 45.0 CCD), Site Level Plan: Level 3 (45.0 CCD), Site Section Plan (1 of 2), Site Section Plan (2 of 2), Public Open Space Plan, Generalized Landscape Plan, Bike and Pedestrian Connectivity Plan, Phase 1: Bike and Pedestrian Connectivity Plan, Transitway Accommodation Plan, Design Guidelines, Phase 1: Floor Plan L1, Phase 1: Floor Plan P1, Phase 1: Floor Plan P2, Phase 1: Floor Plan P3, Phase 1: Floor Plan – Typical Hotel Plans, One Building Section (1 of 2), Phase One Building Section (2 of 2), Phase 1 Proposed Landscape Plan, Phase 1 Riverwalk Site Sections, Phase 1: Riverwalk Site Section I-I, Phase 1: Building Elevations North / South, Phase 1: Building Elevation East / West North all prepared by Solomon Cordwell Buenz Architects dated December 12, 2022, submitted herein. In any instance where a provision of this Planned Development conflicts with the Chicago Building Code, the Building Code shall control. This Planned Development conforms to the intent and purpose of the Zoning Ordinance, and all requirements thereto, and satisfies the established criteria for approval as a Planned Development. In case of a conflict between the terms of this Planned Development Ordinance and the Zoning Ordinance, this Planned Development Ordinance shall control.
5. The following uses are allowed in the Planned Development delineated herein:

Subarea A: indoor participant sport and recreation uses, including casino, hotel, eating and drinking establishment (all); entertainment and spectator sports (all); dwelling units located above the ground floor, cultural exhibits and museum; food and beverage retail sales; office; general retail sales; co-located wireless communications facilities; Public and Civic uses, Commercial uses as permitted in the DX-Downtown Mixed Use District, accessory parking; water taxi; and related and ancillary uses. Printing facility, warehousing, freight and storage uses, provided such uses are only for the existing Chicago Tribune facility

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Subarea B, C, and D: Residential uses including dwelling units located both on and above the ground floor; Lodging (all); co-located wireless communications facilities; Public and Civic uses, and Commercial uses, warehousing, freight and storage uses, printing facility, as permitted in the DX Downtown Mixed Use District, with accessory and non-accessory parking and related accessory uses. Printing facility, warehousing, freight and storage uses, provided such uses are only for the existing Chicago Tribune facility, and are approved pursuant to Site Plan approval, in accordance with PD Statement 12.

Interim uses within the PD shall include surface accessory and non-accessory parking.

The following uses shall be prohibited within the Planned Development: recycling facilities, drive-through facilities, payday/title secured loan store, pawn shop, gas station and free standing wireless tower. In addition, the following uses shall be permitted in all subareas subject to the review and approval of the Department of Planning and Development:

- a. other than as prohibited above, broadcast and telecommunication structures, equipment and installations including parabolic transmitting and receiving antennae;
 - b. public utility and public service uses necessary to serve the development including, but not limited to, district electrical generation and utility substations;
 - c. district cooling and heating; and
 - d. Day Care
6. On-Premise signs and temporary signs, such as business, construction and marketing signs, shall be permitted within the Planned Development, subject to the review and approval of the Department of Planning and Development. Off-Premise signs are prohibited within the boundary of the Planned Development.
7. For purposes of height measurement, the definitions in the Chicago Zoning Ordinance shall apply. The height of any building shall also be subject to height limitations, if any, established by the Federal Aviation Administration. Except as provided herein, the maximum height of any building within this Planned Development shall not exceed 650 feet.
8. The maximum permitted Floor Area Ratio ("FAR") for the Property and each subarea shall be in accordance with the attached Bulk Regulations Table. For the purposes of FAR calculations and measurements, the definitions in the Zoning Ordinance shall apply. The permitted FAR identified in the Bulk Regulations Table has been determined using a net site area of 1,184,654 square feet and a base FAR of 5.0.

The Applicant acknowledges that the project has received a bonus FAR of 0.572 pursuant to Sec. 17-4-1000 of the Zoning Ordinance. With this bonus FAR, the total FAR for the Planned Development is 5.572. In exchange for the bonus FAR, the Applicant is required to make a corresponding payment, pursuant to Sections 17.4-1003-B & C, prior to the issuance of the first building permit for any building in the Planned Development; provided, however, if the Planned Development is constructed in phases as planned, the bonus payment may be paid on a pro rata basis when the first building permit and each subsequent new building permit or phase of construction is issued for a development Parcel or Subarea that exceed an

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FAR of 5.0, utilizing first the excess FAR transferred from Subarea A to Subareas B, C, and D on a prorata basis according to the net site area of each subarea, in accordance with Note 2 of the Bulk Table.

The improvements to be constructed on the Property will be subject to the following Neighborhood Opportunity Fund floor area bonus criteria; otherwise more specifically described in Sections 16-14-010, 17-4-1000 and other referenced portions of the Municipal Code of Chicago. The bonus payment will be split between three separate funds, as follows: 80% to the Neighborhoods Opportunity Fund, 10% to the Citywide Adopt-a-Landmark Fund and 10% to the Local Impact Fund, pursuant to Section 17-41003-D. Such funds will be utilized pursuant to Section 17-4-1004-B (Neighborhoods Opportunity), Section 17-4-1006-C (Citywide Adopt-a-Landmark) and Section 17-41005-C (Local Impact). The bonus payment will be recalculated at the time of payment (including partial payments for phased developments) and may be adjusted based on changes in median land values in accordance with Section 17-4-1003-C.3. Provided, however, the Commissioner of the Department of Planning and Development shall, at the request of the Applicant, be permitted to reduce the amount of the bonus payment for any parcel that does not seek an FAR above 5.0. Funds that become available through the Neighborhood Opportunity Local Impact Fund may be used by CDOT for the design and construction of traffic signal improvements at Ogden Avenue and the northbound Kennedy Expressway on-ramp.

- A. Portions of subareas A and B will be constructed along an elevated roadways between + 18 and + 45 feet Chicago City Datum, but may be lower or higher depending on factors such as roadway levels. For purposes of measuring height and FAR, grade shall be determined pursuant to Section 17-17-0265.
- B. Bulk and Density Regulations
1. Warehousing and storage uses permitted as principal uses in Statement Number 5 shall be limited to eight hundred thousand (800,000) square feet and shall be established in accordance with the DX-5 Downtown Mixed-Use District regulations existing on the effective date of this Planned Development.
 2. With regard to areas devoted as a principal use to office, hotel, residential and retail sales and service uses, the following maximums shall apply;

Office, maximum floor area =	3,000,000 square feet
Hotel, maximum number of rooms/keys =	800
Residential, maximum number of dwelling units = with efficiency units to be determined by MLA.	4,799 units
Retail sales and service, maximum floor area =	160,000 square feet

Provided, however, that in calculating these maximums, ballrooms, meeting rooms, exhibition space and eating facilities associated with a hotel use and located at or above grade shall be chargeable against the maximum permitted floor area for principal retail sales and service of office uses.

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3. The Applicant may increase the maximum number of dwelling units allowed within this planned development by converting a portion of the maximum allowed office space and/or a portion of the maximum allowed hotel rooms/keys. The Applicant may increase the maximum amount of office space by converting a portion of the maximum dwelling units and/or a portion of the maximum number of hotel rooms/keys. The Applicant may increase the maximum number of hotel rooms/keys to one thousand and two hundred (1,200) rooms/keys by converting a portion of the maximum allowed office space and/or a portion of the maximum allowed dwelling units. The Applicant may increase the maximum amount of retail space by converting a portion of the maximum allowed office space and/or portion of the maximum allowed dwelling units. At no point can the overall square footage devoted to residential development exceed 75% of the total allowable buildable square footage within this planned development.

Conversion Chart:

1.	Eight hundred and fifty (850) square feet of office or retail space shall be equal to one (1) dwelling unit.
2.	Two (2) Hotel rooms/keys shall be equal to one (1) dwelling unit.
3.	Two (2) Hotel rooms/keys shall be equal to (850) square feet of office space.
4.	One (1) square foot of office space shall equal one (1) square foot of retail space.

4. The number of efficiency units within this Planned Development shall not exceed thirty percent (30%) of the number of permitted dwelling units.
5. To the extent this Planned Development does not cover all items required for development, all development shall be in general conformity with the DX-5 Downtown Mixed-Use District regulations of the Chicago Zoning Ordinance.
- C. Periphery Setbacks and Minimum Distance Between Buildings
1. Setbacks shall be in conformance with the site plans and Design Guidelines.
 2. Periphery setback and distance provisions may be adjusted where required to allow flexibility of architectural or site design arrangement, subject to the approval of the Department of Planning and Development.
- D. Parking
1. Minimum Requirements for uses:
 - Office: 0.3 spaces per one thousand (1,000) square feet.
 - Hotel: none for the first 15,000 square feet of meeting, conference or banquet area and then: 1 space per 1,000 square feet.
 - Residential: 0.35 parking spaces shall be required per dwelling unit including efficiency units
 - Retail: 0.5 parking space per one thousand (1,000) square feet.
 2. Location. All parking spaces required to serve buildings or uses shall be located on the same parcel as the building or use served, or (a) if a residential use, within six

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hundred (600) feet walking distance measured from the property line; or (b) if office use, within one thousand (1,000) feet walking distance measured from the property line. Said required accessory parking may be located within a different subarea than the use for which it serves.

3. Vehicular entrances and exits to accessory automobile parking areas shall be located in general conformance with the Site Access and Loading Plans attached hereto. Provided, however, that temporary or relocated driveways shall be permitted within the Planned Development subject to the review and approval of the Department of Transportation and the Department of Planning and Development in accordance with Statement 13 below.

4. Transit Served Locations. Any parcel within this Planned Development that qualifies as a transit served location as defined under Section 17-10-0102-B of the Zoning Ordinance may reduce the parking requirements for such subarea by the maximum amounts permitted under Section 17-10-0102-B and approval by DPD. As part of the review of such request to reduce the parking requirement for such parcel, the applicant must provide an exhibit identifying the parcel and its proximity to Public Transit. No approval can be granted without the submission of such document.

E. Loading. Except as provided in the Bulk Table, minimum off-street loading shall be provided in accordance with the regulations applicable in the DX-5 Downtown Mixed-Use District of the Chicago Zoning Ordinance existing on the effective date hereof. The location of loading berths shall be subject to the review of the Department of Transportation and the approval of the Department of Planning and Development.

F. Construction of Public Roadway and Traffic Improvements. No certificate of occupancy shall be issued for any improvement located within a subarea parcel in this Planned Development until such time as the Applicant for the certificate produces evidence that construction of the public roadway and traffic improvements (an improvement available for use by the public) related to the improvement within such parcel has been completed, is under construction or is under contract for construction, or that adequate access can be provided, all as certified by the Department of Transportation and approved by the Department of Planning and Development.

9. The Applicant acknowledges and agrees that the rezoning of the Property from Air Rights Waterway Business Residential Planned Development 1426, to Air Rights Waterway Business Residential Planned Development 1426, as amended, is an "entitlement" that triggers the requirements of Section 2-44-085 of the Municipal Code of Chicago (the "ARO"). The PD is located in a "downtown district" within the meaning of the ARO and permits the construction of up to 4,799 dwelling units in Subareas B, C and D. The Applicant intends to construct a 4,799-unit rental housing project in Subareas B, C and D.

Developers of rental projects in downtown districts with 30 or more units must provide between 10% and 20% of the units in the residential development as affordable units, depending on the average depth of affordability provided, as described in subsection (F)(2) of the ARO. Regardless of the applicable percentage of affordable units in the rental project,

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developers must construct at least 25% of the affordable units on-site and another 25% on-site or off-site (collectively, the "Required Units"), and may satisfy the balance of their affordable housing obligation through: (a) the establishment of additional on-site or off-site affordable units; (b) payment of a fee in lieu of the establishment of on-site or off-site affordable units; or (c) any combination thereof. All on-site affordable units must be accessible dwelling units, as required under subsection (W)(10) of the ARO, and developers must give preference in leasing accessible units to people with disabilities, as specified in the ARO rules. All off-site affordable units must have at least two bedrooms and must be located in a downtown district, inclusionary housing area, or community preservation area, as those terms are defined in the ARO. Whether on-site or off-site, developers must give preference in leasing affordable units of two bedrooms or more to multi-person households, as specified in the ARO rules. If a residential project is located in a transit-served location, off-site units must be located in a substantially comparable transit-served location.

The Applicant has elected the 20% option as set forth in the chart in subsection (F)(2) of the ARO. As a result, the Applicant's affordable housing obligation is 960 affordable units (20% of 4,799), half of which (480 affordable units) are Required Units. The Applicant has agreed to satisfy its affordable housing obligation by providing the Required Units in the rental building(s) to be constructed in Subareas B, C and D. The Applicant agrees that the affordable rental units must be affordable to households with a range of incomes averaging 60% of the Chicago Primary Metropolitan Statistical Area Median Income ("AMI"), as updated annually, provided that (x) the maximum income level for any affordable unit may not exceed 80% of the AMI, (y) at least one-third (or 160 units) must be affordable to households at or below 50% of the AMI, of which one-sixth (or 27 of the 160 units) must be affordable to households at or below 40% of the AMI, and (z) all income levels must be multiples of 10% of the AMI. The Applicant has not yet determined how it will satisfy its remaining affordable housing obligation of 480 affordable units, but acknowledges and agrees that it must comply with (a), (b), or (c) in the preceding paragraph.

If the Applicant requests any material change to its method of compliance with the ARO for the Required Units, such as locating affordable units off-site instead of on-site or changing the target affordability level after the passage of this PD, The Department of Housing ("DOH") may adjust the Affordable Housing Profile ("AHP") form attached hereto as requested, in accordance with the ARO, without amending the PD, provided however, the Applicant must update and resubmit the revised AHP to DOH for review and approval and, at DOH's request, provide an informational presentation to the Plan Commission on such change. Prior to the issuance of any building permits for any residential building in the PD, including, without limitation, excavation or foundation permits, the Applicant must execute and record an Inclusionary Housing Agreement ("IHA") in accordance with subsection (N) of the ARO. The terms of the IHA and any amendments thereto are incorporated herein by this reference. The Applicant acknowledges and agrees that the IHA will be recorded against the applicable portion of the PD, and will constitute a lien against such property. The Commissioner of DOH may enforce remedies for any breach of this Statement 9, including any breach of any IHA, and enter into settlement agreements with respect to any such breach, subject to the approval of the Corporation Counsel, without amending the PD.

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This statement does not include all ARO requirements and options. It is intended to provide an overview of the application of the ARO to this PD. In the event of any conflict between this statement and the terms and conditions of the ARO, the ARO shall govern.

10. The Applicant acknowledges the importance of the Chicago River as a resource for both commerce and recreation and acknowledges the City's goals of improving the appearance, quality and accessibility of the river, as contained in the waterway planned development guidelines in the Zoning Ordinance (Section 17-8-0912) and the Chicago River Corridor Design Guidelines and Standards or as amended. The Applicant also acknowledges the river development Design Guidelines of the North Branch Framework Plan that builds upon the North Branch Industrial Corridor's unique natural and built environment. To further these goals, the Applicant agrees, as set forth in the Public Open Space Plans, to: (a) provide, a river setback with an average width of between 30 ft and 60 ft with a continuous 16-ft wide multi-purpose riverwalk as shown in the Public Open Space Plan and General Landscape Plan, and where at no point is the river setback less than 30-foot; (b) provide a variety of active uses and river overlooks as indicated on said Plans; and (c) permit the connection of such riverwalk under Chicago Avenue and under Grand Avenue to the riverwalk of adjacent properties when adjacent properties are similarly improved.

The Applicant shall permit un-gated and unobstructed public access to the river setback, and provide informational and wayfinding signage at all entries that the riverwalk is open to the public, free of charge, during normal park hours from 6:00am to 11:00pm every day of the year. All improvements within the river setback for each development parcel must be substantially complete prior to receipt of Certificate of Occupancy for the first principal building located on the east side of Jefferson Street within each parcel as shown on the Development Phasing Diagram. Planting may be delayed, if consistent with good landscape practice, but not longer than one year following receipt of the occupancy certificate.

The Applicant shall also design and construct the riverfront park as depicted on the Public Open Space Plan (hereinafter the "Park"). The riverfront park will include activated programming along the east side of Jefferson Street and landscaping to minimize blank terraced walls along Jefferson Street. The riverfront park on the east side of Jefferson Street will be completed prior to receipt of Certificate of Occupancy for the first principal building within Parcel A-1.

The Linear Park over the Union Pacific Railroad ROW and on the west side of Jefferson Street is split into multiple parcels, Parcels B-3, B-6 and B-8. For constructability, that portion of the Linear Park located in Parcel B-3 will be completed prior to receipt of Certificate of Occupancy for the first principal building within Parcel B-2 or B-3; and the portion of the Linear Park located on Parcel B-6 will be completed prior to receipt of Certificate of Occupancy on first principal building within Parcel B-5; and Linear Park located on Parcel B-8 will be completed prior to receipt of Certificate of Occupancy on first principal building within Parcel B-7.

Upon completion of each Park, the Applicant shall be responsible for maintaining and managing the Park for the purposes set forth herein, including ensuring that the Park's

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landscaping is well maintained, that the vegetation and plantings are kept in healthy condition and that the Park facilities and play areas are clean, well lit, litter free and clear of snow from hardscaped areas and debris. The Applicant shall provide sufficient liability insurance coverage necessary for the operation of the Park for public use. The Applicant shall permit un-gated and unobstructed public access to the Parks and shall provide informational and wayfinding signage at all entries that the Parks are open to the public, free of charge, during normal park hours from 6:00am to 11:00pm every day of the year.

Prior to issuance of building permits for the first principal building within each Parcel triggers the construction of a park or a portion thereof, the Applicant will enter into a development and maintenance agreement with the City of Chicago for the construction, maintenance, and management of any parks within such Parcel or adjacent Parcel. The development and maintenance agreement obligations shall be binding upon the Applicant, its successors and assigns, including but not limited to a master business or homeowners maintenance association whose purpose includes maintaining the Parks and/or the riverwalk. Upon completion of the Park, the parcel of land developed and devoted to open space shall be memorialized in a public access easement agreement with and for the benefit of the City of Chicago or a not for profit accredited land conservancy trust, as approved by the City, whose purpose is to hold open space public access easements. Any reasonable costs associated with establishing, monitoring, or stewardship of the public access easement shall be the responsibility of the Applicant, and would need to be contributed at the time when the easement is granted.

11. Upon review and determination, Part II Review, pursuant to Section 17-13-0610, a Part II Review Fee shall be assessed by the Department of Planning and Development. The fee, as determined by staff at the time, is final and binding on the Applicant and must be paid to the Department of Revenue prior to the issuance of any Part II approval.
12. The Site and Landscape Plans for future development parcels and phases for each parcel shall be in substantial conformance with the Landscape Ordinance and any other corresponding regulations and guidelines, including Section 17-13-0800. Final landscape plan review and approval will be by the Department of Planning and Development. Any interim reviews associated with site plan review or Part II reviews, are conditional until final Part II approval.

Prior to the issuance by the Department of Planning and Development of a determination pursuant to Section 7-13-0610 of the Chicago Zoning Ordinance ("Part II approval") for development or redevelopment of any development parcels within the Planned Development, other than alterations to existing buildings which, unless otherwise permitted by the Department of Planning and Development, do not increase their height or alter their footprints, a site plan for the proposed development, including parking areas and related information ("Site Plan Submittal"), shall be submitted to the Commissioner of the Department of Planning and Development for approval. Review and approval of the Site Plan Submittal by the Commissioner is intended to assure that specific development proposals conform with the Planned Development, are consistent with the intent of the Design

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Guidelines and to ensure coordination of any public improvements required by this Planned Development.

At the time of presentation to the Plan Commission, the proposed development of Sub-Area A was still in a state of evolving design. The Applicant is committed to on-going discussions regarding the final design and material finishes of the proposed development and has agreed that no Part II approval for proposed work in Sub-Area A shall be granted until a Site Plan Review request is submitted to the Department of Planning and Development and the Site Plan Review has been approved by the Department of Planning and Development.

No Part II approval for Subareas B, C and D, for which a Site Plan is required hereunder shall be granted until the Site Plan Submittal has been approved by the Commissioner and reviewed by the Chicago Plan Commission at a Public hearing. The hearing conducted by the Plan Commission shall be for review purposes only and no approval or recommendation shall be required in order for the Commissioner to issue an approval of the Site Plan Submittal. Further, all Part II submittals shall be in compliance with the Chicago Landscape Ordinance. Provided the Site Plan Submittal required hereunder is in general conformance with this Planned Development and the Design Guidelines, and provided Applicant has timely provided all Site Plan Submittals, the Commissioner shall issue such site plan approval and the Plan Commission shall conduct its review hearing. Following approval of a Site Plan Submittal by the Commissioner, the approved plan shall be kept on permanent file with the Department of Planning and Development and shall be deemed to be an integral part of this Planned Development. The approved Site Plan Submittal may be changed by the provisions of Section 17-13-0611 of the Chicago Zoning Ordinance. In addition, because of the long-term phasing of the Planned Development, design aspects of the Planned Development such as phasing, parcel sizes, building footprints, vehicular access drives and tower locations may be modified during the Site Plan Submittal approval process, but shall still be considered to be in general conformance with this Planned Development provided such modifications do not violate the bulk and density regulations of this Planned Development and are consistent with the Design Guidelines. A Site Plan Submittal shall, at a minimum, provide the following information with respect to the proposed improvements:

- a. the boundaries of the property;
- b. the footprint of the improvements;
- c. location and dimensions of all parking spaces and loading berths;
- d. preliminary landscaping plan prepared by a landscape architect with final landscaping plan to be approved during Part II review;
- e. all pedestrian circulation routes;
- f. the location of any adjacent public improvements;
- g. a signage plan for any building where retail or theater uses would be present above the ground level;
- h. preliminary building sections and elevations of the improvements with a preliminary building materials list; and
- i. statistical information applicable to the property limited to the following:
 - (1) floor area and floor area ratio;

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- (2) uses to be established;
- (3) building heights;
- (4) all setbacks, required and provided;
- (5) floor area devoted to all uses (e.g. office, retail etc.);
- (6) number of dwelling units;
- (7) number of parking spaces;
- (8) number of loading spaces/berths
- (9) If mutually agreed upon by DPD and the applicant a School Impact Study may be required with the first Site Plan Submittal.

A Site Plan Submittal shall include such other information as may be necessary to illustrate conformance with the applicable provisions of this Planned Development and the Design Guidelines. In addition, as part of the site plan review process for each phase of development, an updated traffic and parking study shall be submitted when it is determined by the Commissioners of the Department of Planning and Development and Department of Transportation that such an updated report is required. Changes within Subarea A may also be approved thru the Site Plan Submittal procedure in addition to minor changes approval process set forth in Section 17-13-0611.

13. In order to encourage architectural diversity and excellence in design, the Applicant will also ensure that each Site Plan Submittal substantially complies with the North Branch Framework and Design Guidelines (NBF) as part of the Part II Review process. Revisions and modifications to any previously approved site plan, landscape plan or building elevations must be substantially consistent with the NBF. Furthermore, Applicant its request, may continue to evolve the design of the building elevations within the PD. Changes to such elevations, if any, shall, if mutually agreed, be approved by the Department administratively as a Minor Change.
14. The Applicant acknowledges the potential alignments for a multi-modal trail, transit-way and other potential transportation improvements ("Transportation Improvements") through and adjacent to the Property. The Applicant has identified a conceptual location of the Transportation Improvements on the Transit Way Accommodation Plan attached hereto and agrees to provide an easement on, across and through such area of the Property as necessary to accommodate the Transportation Improvements as provided herein. The Applicant and the City of Chicago agree to cooperate on the final location of the Transportation Improvements through the Planned Development site. The Transportation Improvements are meant to improve connectivity and access to the Planned Development site and to other development sites near the Planned Development. The Transportation Improvements shall not adversely affect operations, security of the buildings or diminish the development rights within the Planned Development. In the event that the Transportation Improvements are incorporated into any of the right of ways being dedicated to the public as part of this Planned Development, the Applicant shall be permitted to recapture the square footage of such portion of right of way previously or to be dedicated and increase the net site area of the Planned Development. Any modifications that result from these Transportation Improvements shall be made pursuant to the minor change provisions of Section 17-13-0611.

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15. The Applicant shall comply with Rules and Regulations for the Maintenance of Stockpiles promulgated by the Commissioners of the Departments of Streets and Sanitation, Fleet and Facility Management and Buildings, under Section 13-32-085, or any other provision of the Municipal Code of Chicago.
16. The terms and conditions of development under this Planned Development ordinance may be modified administratively, pursuant to Section 17-13-0611-A, by the Zoning Administrator upon the application for such a modification by the Applicant, its successors and assigns and, if different than the Applicant, the legal title holders and any ground lessors. In addition to any other modifications that are otherwise permitted pursuant to Section 17-13-0611-A of the Chicago Zoning Ordinance, an increase in the maximum building height within any subarea by ten percent (10%) or less shall constitute a permitted modification hereunder
17. The Applicant acknowledges that it is in the public interest to design, construct and maintain the project in a manner which promotes, enables and maximizes universal access throughout the Property. Plans for all buildings and improvements on the Property shall be reviewed and approved by the Mayor's Office for People with Disabilities to ensure compliance with all applicable laws and regulations related to access for persons with disabilities and to promote the highest standard of accessibility.
18. The Applicant acknowledges that it is in the public interest to design, construct, renovate and maintain all buildings in a manner that provides healthier indoor environments, reduces operating costs and conserves energy and natural resources. The Applicant shall achieve LEED Gold and 125 point on the Chicago Sustainable Development Policy matrix in connection with the development of Subarea A. For Subareas B, C and D, the Applicant shall obtain the number of points necessary to meet the requirements of the Chicago Sustainable Development Policy, in effect at the time the Part II review process is initiated for each improvement that is subject to the aforementioned Policy and must provide documentation verifying compliance. The method of compliance must include sustainable strategies that are consistent with the North Branch Design Guidelines.

The Applicant agrees to install a stormwater management system that captures, cleans and discharges the majority of the on-site stormwater (as allowed by City regulations, approved by the Department of Water Management and technically feasible) to the Chicago River. At the time of a hearing before the Chicago Plan Commission, the Applicant agrees to be in compliance with the City of Chicago Sustainable Development Policy set forth by the Department of Planning and Development in effect at the time the Part II review process is initiated for each improvement (phase, subarea or sub parcel) that is subject to the aforementioned Policy and must provide documentation verifying compliance.

19. The Applicant acknowledges that it is the policy of the City to maximize opportunities for Minority and Women-owned Business Enterprises ("M/WBEs") and city residents to compete for contracts and jobs on construction projects approved through the planned development process. To assist the city in promoting and tracking such M/WBE and city resident participation, an applicant for planned development approval shall provide information at three points in the city approval process. First, the applicant must submit to

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DPD, as part of its application for planned development approval, an M/WBE Participation Proposal. The M/WBE Participation Proposal must identify the applicant's goals for participation of certified M/WBE firms in the design, engineering and construction of the project, and of city residents in the construction work. The city encourages goals of (i) 36% MBE and 10% WBE participation (measured against the total construction budget for the project or any phase thereof), and (ii) 50% city resident hiring (measured against the total construction work hours for the project or any phase thereof). The M/WBE Participation Proposal must include a description of the Applicant's proposed outreach plan designed to inform M/WBEs and city residents of job and contracting opportunities. Second, at the time of the Applicant's submission for Part II permit review for the project or any phase thereof, the Applicant must submit to DPD (a) updates (if any) to the Applicant's preliminary outreach plan, (b) a description of the Applicant's outreach efforts and evidence of such outreach, including, without limitation, copies of certified letters to M/WBE contractor associations and the ward office of the alderman in which the project is located and receipts thereof; (c) responses to the Applicant's outreach efforts, and (d) updates (if any) to the applicant's M/WBE and city resident participation goals. Third, prior to issuance of a Certificate of Occupancy for the project or any phase thereof, the Applicant must provide DPD with the actual level of M/WBE and city resident participation in the project or any phase thereof, and evidence of such participation. In addition to the foregoing, DPD may request such additional information as the department determines may be necessary or useful in evaluating the extent to which M/WBEs and city residents are informed of and utilized in planned development projects. All such information will be provided in a form acceptable to the Zoning Administrator. DPD will report the data it collects regarding projected and actual employment of M/WBEs and city residents in planned development projects twice yearly to the Chicago Plan Commission and annually to the Chicago City Council and the Mayor.

20. The Applicant acknowledges that the Property is located in the North Branch Industrial Corridor Conversion Area, and is undergoing a "rezoning" within the meaning of Chapter 16-8 of the Municipal Code (the "Industrial Corridor System Fund Ordinance"). As a result of this rezoning, the Planned Development is subject to the conversion fee provisions of the Industrial Corridor System Fund Ordinance. The purpose of the conversion fee is to mitigate the loss of industrial land and facilities in conversion areas by generating funds for investment in receiving industrial corridors in order to preserve and enhance the City's industrial base, support new and expanding industrial uses, and ensure a stable future for manufacturing and industrial employment in Chicago. For purposes of this Planned Development, the portion of the North Branch Industrial Corridor designated as NBCO-B, Central Subdistrict pursuant to 17-7-0402, shall be deemed an eligible receiving industrial corridor, in addition to all other receiving corridors referenced by definition pursuant to section 16-8-20 of the Chicago Zoning Ordinance. The Applicant is required to pay the conversion fee in full prior to the issuance of the first building permit for any building in the Planned Development; provided, however, if the Planned Development is constructed in phases, the conversion fee may be paid on a pro rata basis upon issuance of the building permit for each subsequent new building or phase of construction. The amount of the conversion fee due upon to the issuance of a building permit shall be calculated based on the fee rate in effect at the time of payment. The Applicant shall record a notice against the Property to ensure that the requirements of the Industrial Corridor System Fund Ordinance are enforced in accordance with Sec. 16-8-100.

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21. This Planned Development shall be governed by Section 17-13-0612 of the Zoning Ordinance. Provided, however, this Planned Development shall not lapse and shall remain in full force and effect provided that the provisions of Section 17-13-0612-A are satisfied for any single development parcel within this Planned Development. Should this Planned Development ordinance lapse, the Zoning Administrator of the Department of Planning and Development shall initiate a Zoning Map Amendment to rezone the property back to the Air Rights Waterway Business Residential Planned Development 1426, dated October 31, 2018.

[Existing Zoning Map; Existing Land-Use Plan; General Land-Use Plan; Boundary and Property Line Map; Right-of-Way Adjustment Map; Huron and Jefferson Streets Cross Sections; Development Plan with Associated Infrastructure Requirements; Subarea and Parcelization Plan; Development Phasing Diagram; Phase 1 -- Interim Site Plan for Parking and Riverwalk; Phase 1 -- Interim Site Plan For Parking; Site Access and Loading Plan Level 1 (+0.0 to +30.0 CCD); Site Access and Loading Plan Level 2 (+30.0 to +45.0 CCD); Site Level Plan: Level 1 (+0.0 to +30.0 CCD); Site Level Plan: Level 2 (+38.0 to 45.0 CCD); Site Level Plan: Level 3 (+45.0 CCD); Site Section Plans; Public Space Open Plan; Generalized Landscape Plan; Bike and Pedestrian Connectivity Plan -- Ultimate Build Out; Phase 1 -- Bike and Pedestrian Connectivity Plan; Transit Accommodation Plan; Design Guidelines Phase 1 Floor Plans -- L1, P1, P2 and P3; Phase 1 Floor Plans -- Typical Hotel Plans; Phase 1 Building Sections Phase 1 Proposed Landscape Plan; Riverwalk Site Sections and Riverwalk Site Section I-I; and Phase 1 Building Elevations -- North/South and East/West referred to in these Plan of Development Statements printed on pages 58331 through 58366 of this *Journal*.]

Bulk Regulations and Data Table, ARO Affordable Housing Profile Form (AHP) and ARO Web Form referred to in these Plan of Development Statements read as follows:

*Air Rights Waterway-Business-Residential Planned Development No. 1426, As Amended.**Bulk Regulations And Data Table.***FINAL FOR PUBLICATION**

Gross Site Area:	1,671,831	sf	38.38 acres
Total Area of Existing Right-of-Way	361,238	sf	8.29 acres
Total Area of Right-of-Way to be dedicated:	125,939	sf	2.89 acres
Total Net Site Area:	1,184,654	sf	27.20 acres

Base FAR	5.0
Bonus FAR	0.572
Total Maximum FAR	5.572
Maximum FAR Buildable Area	6,600,892.088 sf

Net Site Area by Sub-Area:

Sub Area A:	512,290	sf
Sub Area B:	374,857	sf
Sub Area C:	75,947	sf
Sub Area D:	221,560	sf

FAR Building Areas by Sub-Area:

Sub Area A:	1,000,000	sf
Sub Area B:	3,070,892.088	sf
Sub Area C:	500,000	sf
Sub Area D:	2,030,000	sf

FAR by Sub-Area:

Sub Area A:	1.952
Sub Area B:	8.190
Sub Area C:	6.584
Sub Area D:	9.163

Maximum Building Height by Sub-Area:

Sub Area A:	650 feet
Sub Area B:	650 feet
Sub Area C:	650 feet
Sub Area D:	650 feet

Maximum Number of Residential Units by Sub-Area:

Sub Area A:	0 units
Sub Area B:	2,300 units
Sub Area C:	499 units
Sub Area D:	2,000 units

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Maximum Number of Hotel Keys by Sub-Area:

Sub Area A:	500
Sub Area B:	300
Sub Area C:	0
Sub Area D:	0

Minimum Number of Parking Spaces:

Sub Area A:	2,500
Sub Area B:	per parking requirements below
Sub Area C:	per parking requirements below
Sub Area D:	per parking requirements below

Minimum Number of Loading Berths:

Sub Area A:	4 (10'x25') 2 (10'x25')
Sub Area B:	per loading requirements below
Sub Area C:	per loading requirements below
Sub Area D:	per loading requirements below

Minimum Accessory Parking Spaces

Office:	0.3 spaces per one thousand (1,000) sf
Hotel:	1 space per 1,000 sf if more than 15,000 sf of meeting, conference, or banquet area.
Residential:	0.35 parking spaces shall be required per dwelling unit including efficiency units.
Retail:	0.5 parking spaces per one thousand (1,000) sf.

Minimum Loading Requirements:

Per DX-5 Zoning District requirements.

Minimum Number of Bike Parking Spaces:

Residential:	1 per 2 auto parking spaces
non-residential:	1 per 10 auto parking spaces

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- (1) Ordinance Maximums are subject to adjustment and transfer between Sub-Areas per Statements 8 and 16.
- (2) In accordance with Section 17-7-0406-C of the Chicago Zoning Ordinance, the base FAR for this Planned Development shall be 5.0, provided, however, the Applicant shall have the right to develop up to a maximum of 5.572 FAR in accordance with this Planned Development. For any development parcels that exceeds 5.0 FAR, the developer shall be required to pay a floor area bonus in accordance with Statement 8 of this Planned Development. Individual development parcels may exceed an 5.572 FAR, provided the FAR for the entire property does not exceed 5.572.
- (3) Parcel sizes may be modified as provided in Statement 12.
- (4) The maximum FAR and MLA as allocated to the Sub-Areas may be transferred and shifted among the Sub-Areas pursuant to Section 17-13-0611 provided the overall FAR for the entire property does not exceed 5.572.
- (5) Minimum parking requirements of one sub-area may be satisfied by providing parking in another sub-areas which is designated for such sub-area per Statements 8 and 16.

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Date: December 2, 2022

DEVELOPMENT INFORMATION

Development Name: Bally's Chicago Casino
Development Address: 777 W. Chicago Avenue, Chicago IL
Zoning Application Number, if applicable: 21129

Ward: 27th

If you are working with a Planner at the City, what is his/her name?

Noah Szafraniec

Type of City Involvement
check all that apply

- City Land
- Financial Assistance
- Zoning increase
- Planned Development (PD)
- Transit Served Location (TSL) project

REQUIRED ATTACHMENTS: the AHP will not be reviewed until all required docs are received

- ARO Web Form completed and attached - or submitted online on
- ARO "Affordable Unit Details and Square Footage" worksheet completed and attached (*Excel*)
- If ARO units proposed, Dimensioned Floor Plans with affordable units highlighted are attached (*pdf*)
- If ARO units proposed are off-site, required attachments are included (see next page)
- If ARO units are CHA/Authorized Agency units, signed acceptance letter is attached (*pdf*)

DEVELOPER INFORMATION

Developer Name Bally's Chicago Operating Company, LLC
 Developer Contact Ameet Patel
 Developer Address c/o Bally's Corporation, 100 Westminster Street, Providence RI 02903
 Email APatel@bally's.com Developer Phone 401-475-8474
 Attorney Name Meg George Attorney Phone 312-870-8021

TIMING

Estimated date marketing will begin TBD, residential is a future phase of the development.
 Estimated date of building permit* TBD, residential is a future phase of the development.
 Estimated date ARO units will be complete TBD, residential is a future phase of the development.

*the in-lieu fee, recorded covenant and \$5,000 per unit administration fee (for off-site units) are required prior to the issuance of any building permits, including the foundation permit.

PROPOSED UNITS MEET REQUIREMENTS (to be executed by Developer & ARO Project Manager)

Ameet Patel 12/07/2022
 Developer or their agent Date
[Signature] 12/06/2022
 ARO Project Manager Date





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ARO Web Form

Applicant Contact Information
 Name: Chris A Leach
 Email: chris.leach@akerman.com

Development Information

Address **Submitted Date: 12/02/2022**

Number From: 777 Number To: N/A Direction: W
 Street Name: Chicago Avenue Postal Code: 60654

Development Name
 Bally's Chicago Casino

Are you rezoning to downtown?: No

Information

Ward: 27 ARO Geography: Downtown

Details
 ARO Trigger: Downtown Planned Development
 Total Units: 4,799
 Development Type: Rent
 Date Submitted: 12/02/2022

Requirements

ARO Units: 960 affordable units (20% of 4,799 total units), of which half are required units (480 Units)

How do you intend to meet your ARO obligation?

On-Site: 480 Off-Site: 0
 On-Site to CHA or Authorized agency: 0 Off-Site to CHA or Authorized agency: 0
 Total Units: 960

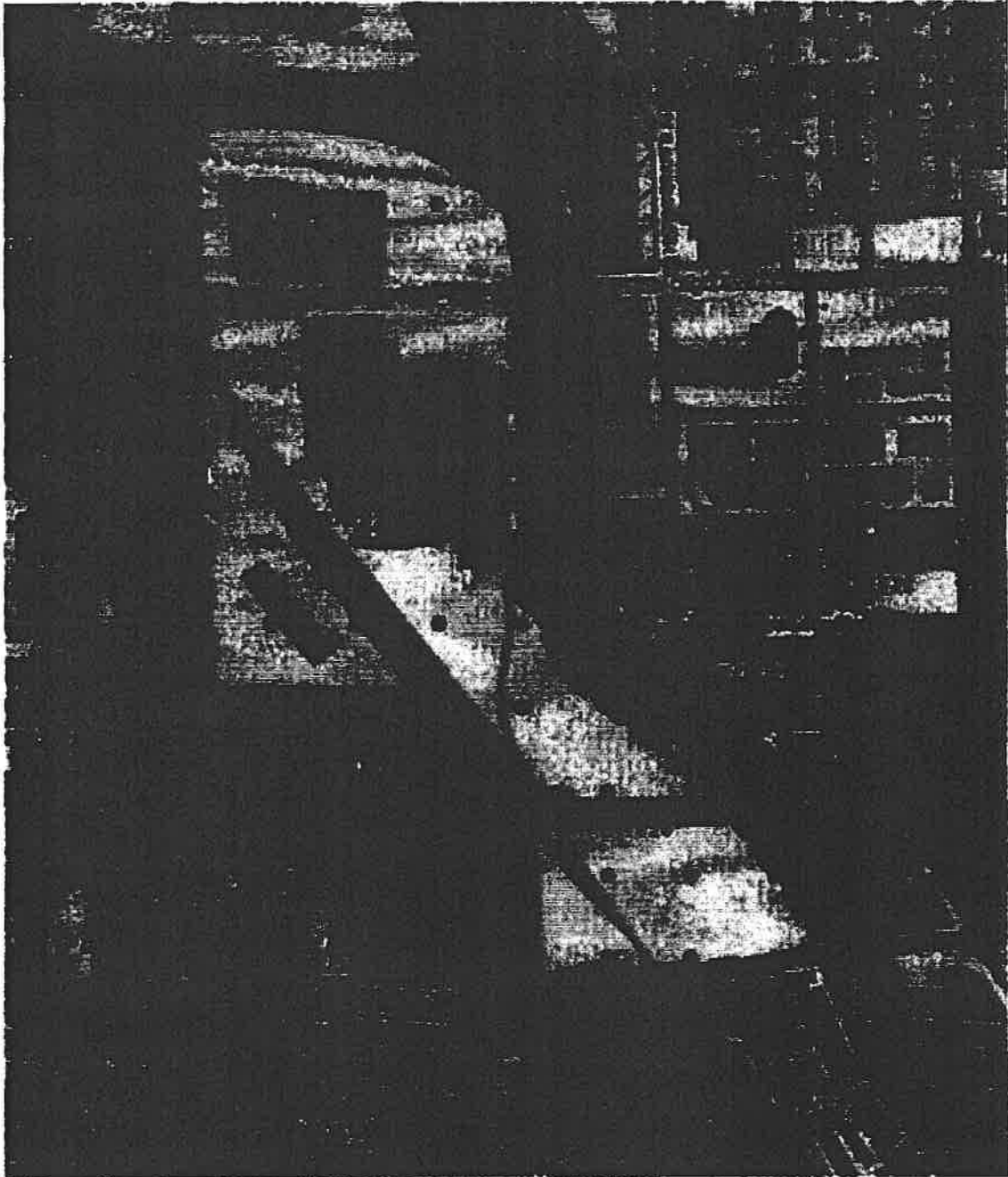
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This is a preliminary approved AHP. The Applicant has not yet determined how it will satisfy its remaining affordable housing obligation of 480 affordable units, but acknowledges and agrees that it must satisfy the balance of their ARO obligation in accordance with Statement 9 of PD 1426, as amended.

Prior to the issuance of any building permits for any residential building in Subareas B, C, and D, the Applicant must make the required payment of a fee in-lieu of the establishment of on-site or off-site affordable units and execute and record an affordable housing agreement.

The in-lieu fee will be recalculated at the time of payment (including partial payments for phased developments) and may be adjusted based on changes in the consumer price index in accordance with Section 2-44-085.

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EXISTING ZONING MAP

0' 125' 250'

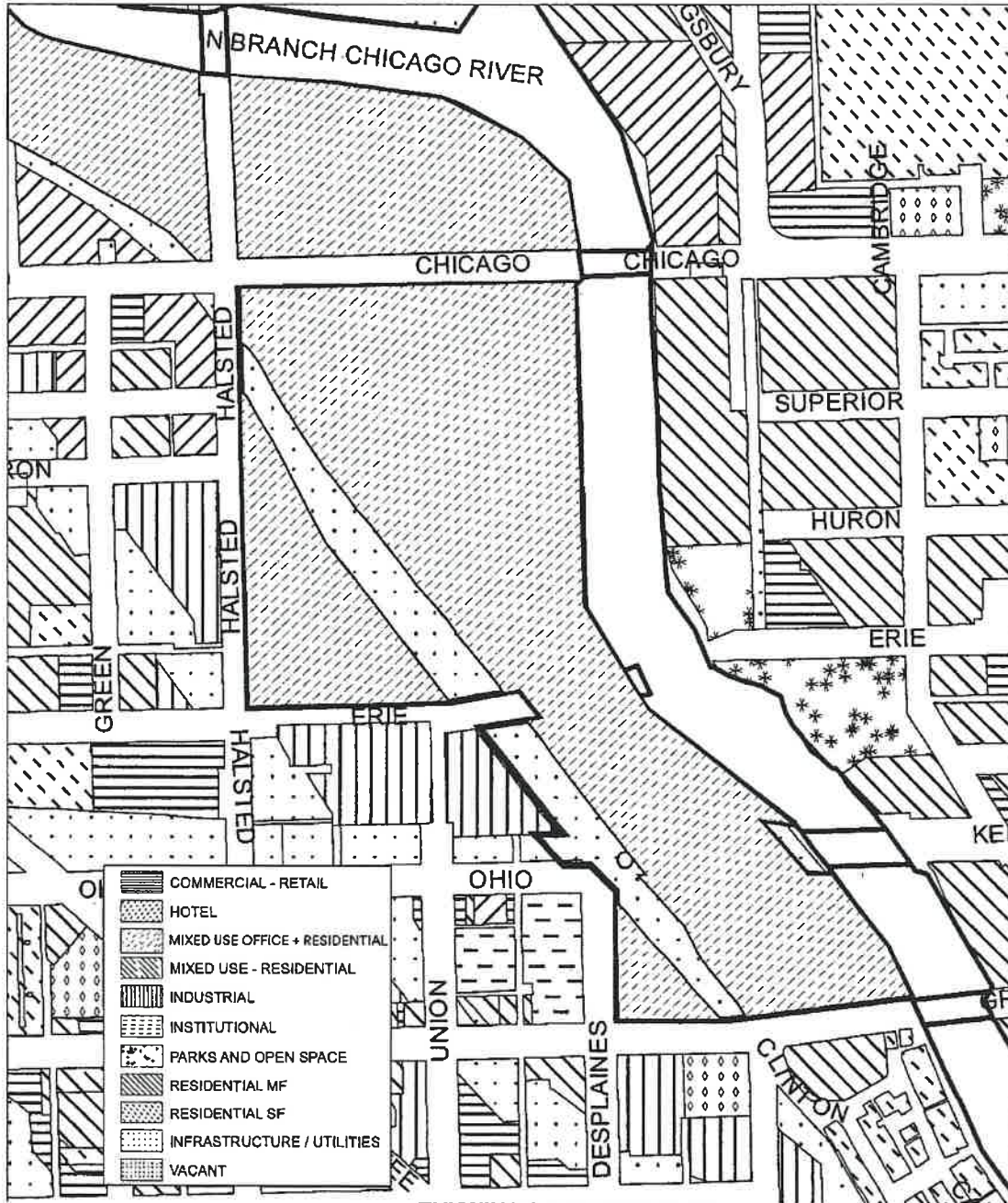


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Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740
W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; &
524-630 W. GRAND AVE.

Introduced: September 21, 2022
Plan Commission: December 12, 2022

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EXISTING LAND USE PLAN

0' 125' 250'

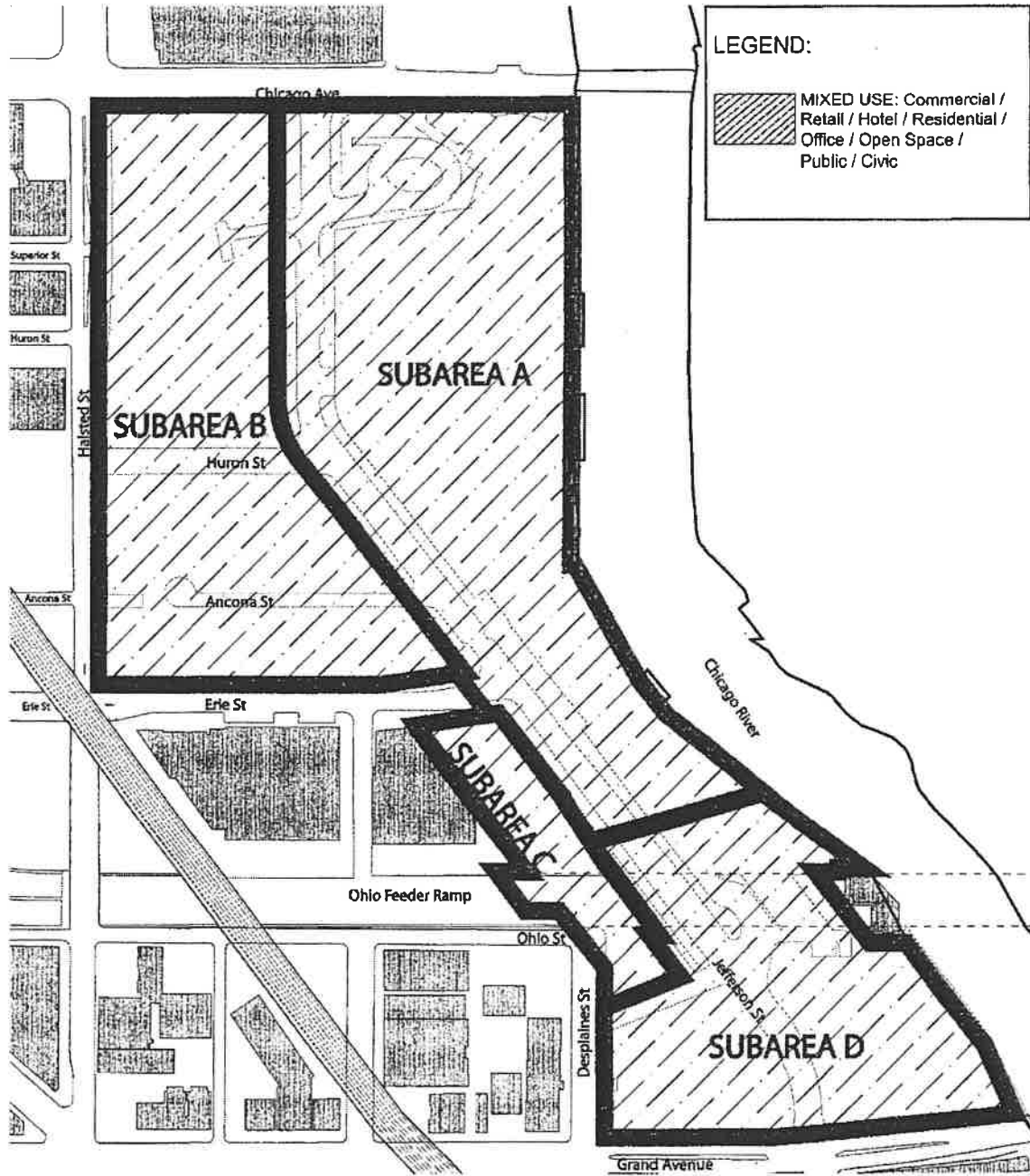


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Introduced: September 21, 2022
Plan Commission: December 12, 2022

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GENERAL LAND USE PLAN

0' 125' 250'

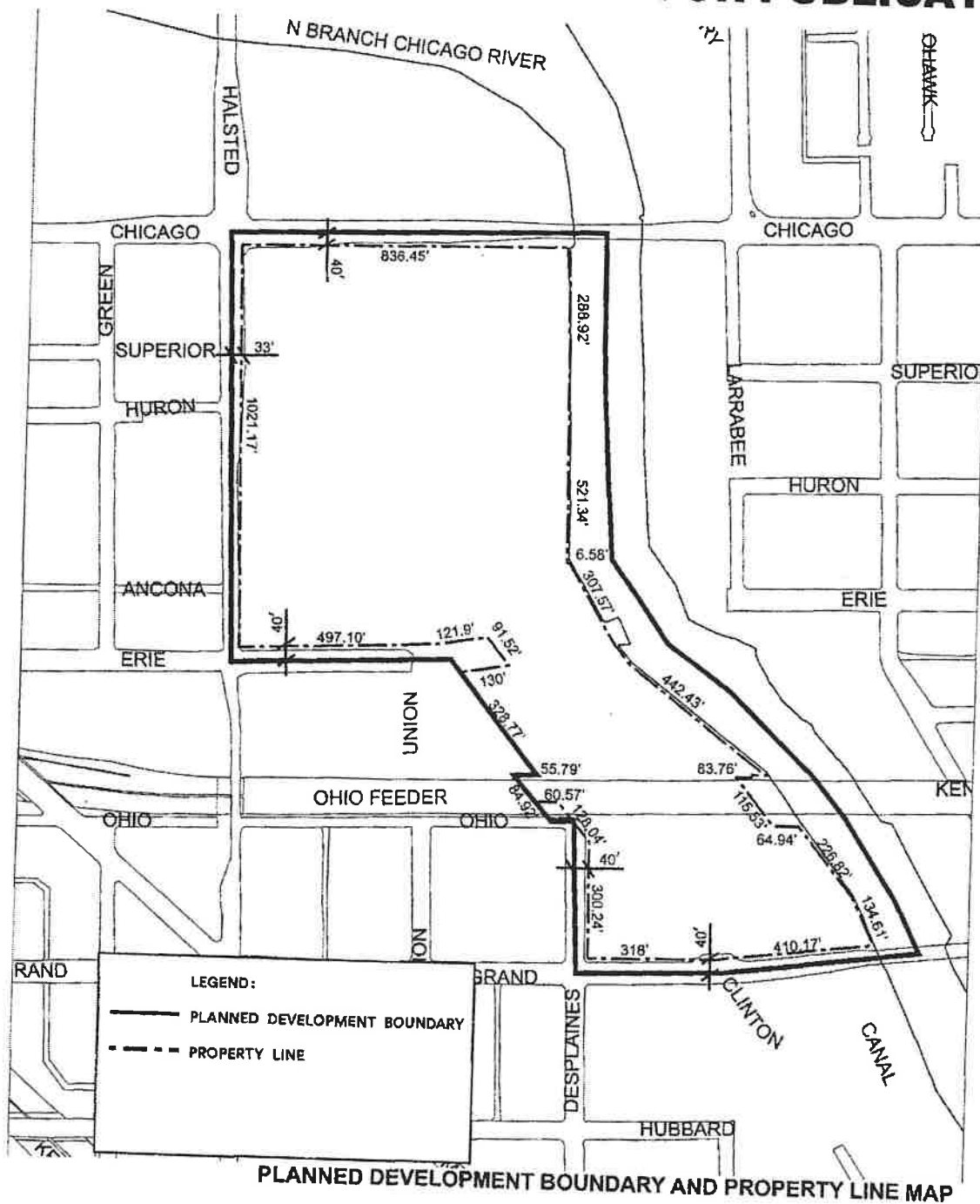


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PLANNED DEVELOPMENT BOUNDARY AND PROPERTY LINE MAP

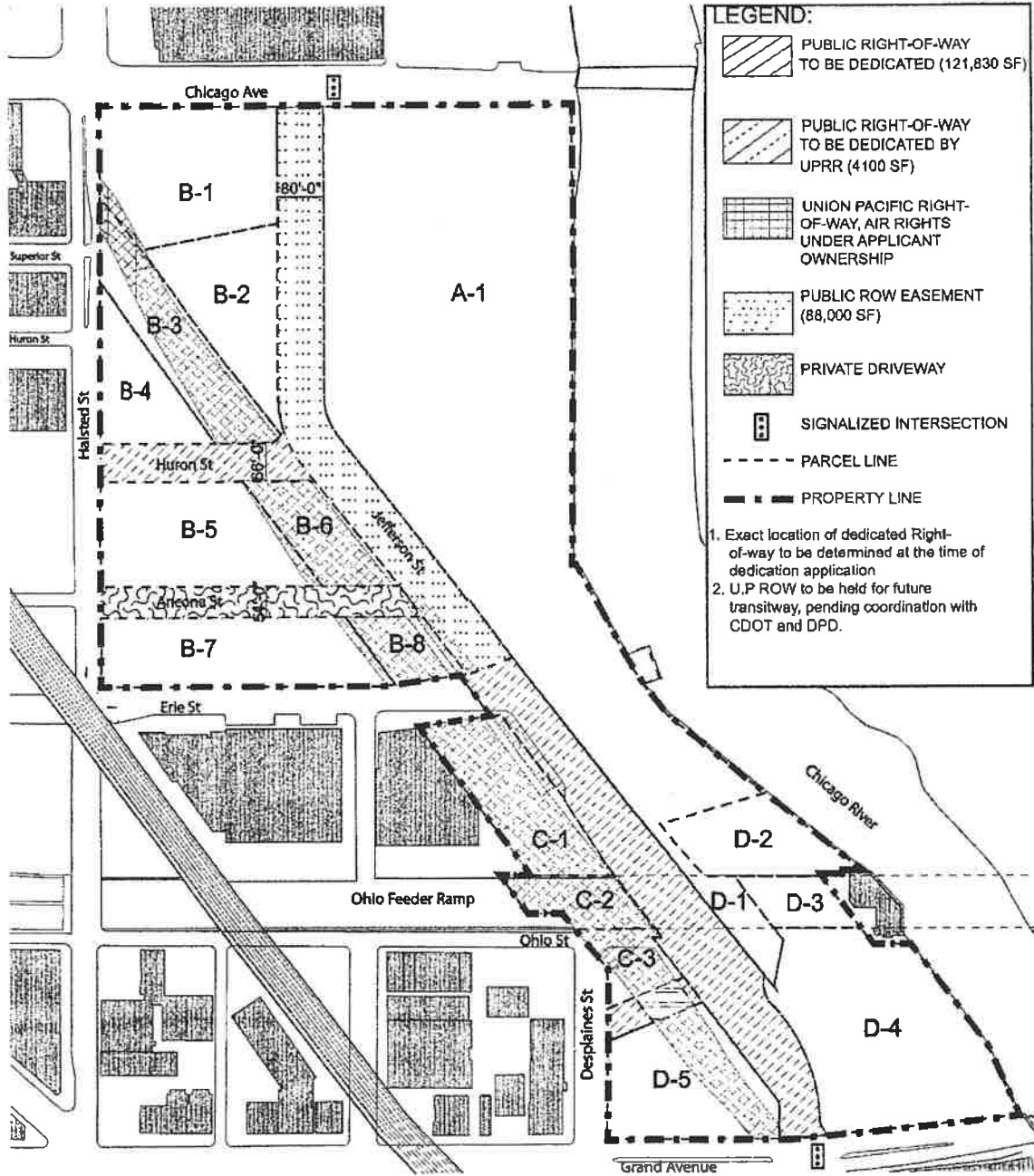
0' 125' 250'



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 Plan Commission: December 12, 2022

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Note: City of Chicago ROW intended to be 66'-0"

RIGHT-OF-WAY ADJUSTMENT MAP

0' 125' 250'

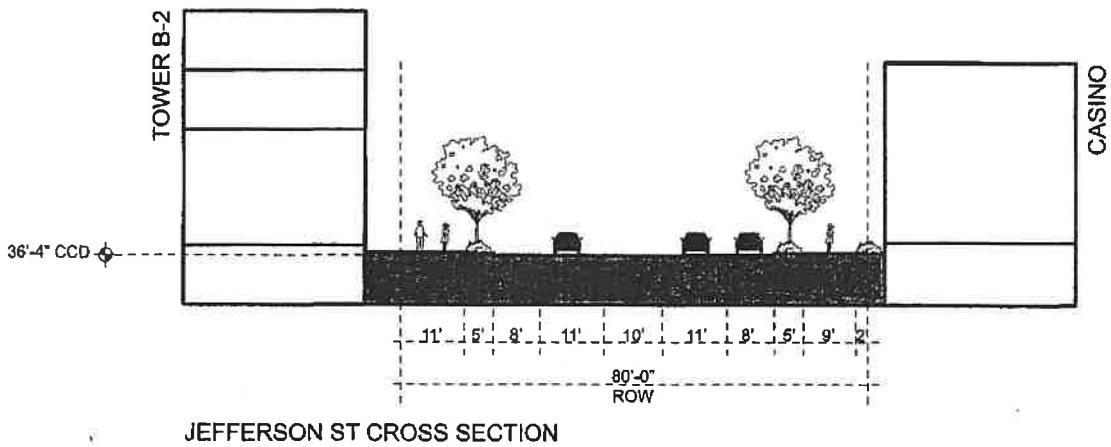
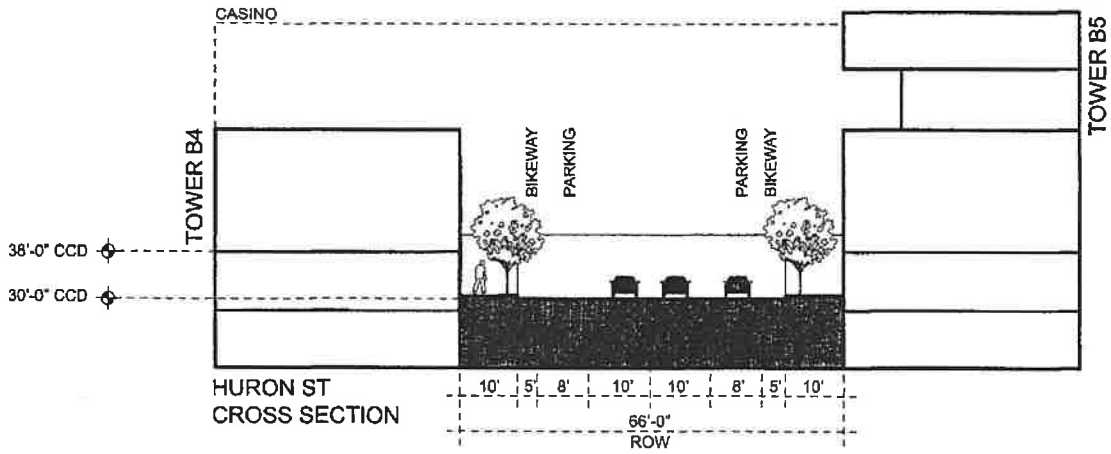


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0' 125' 250'

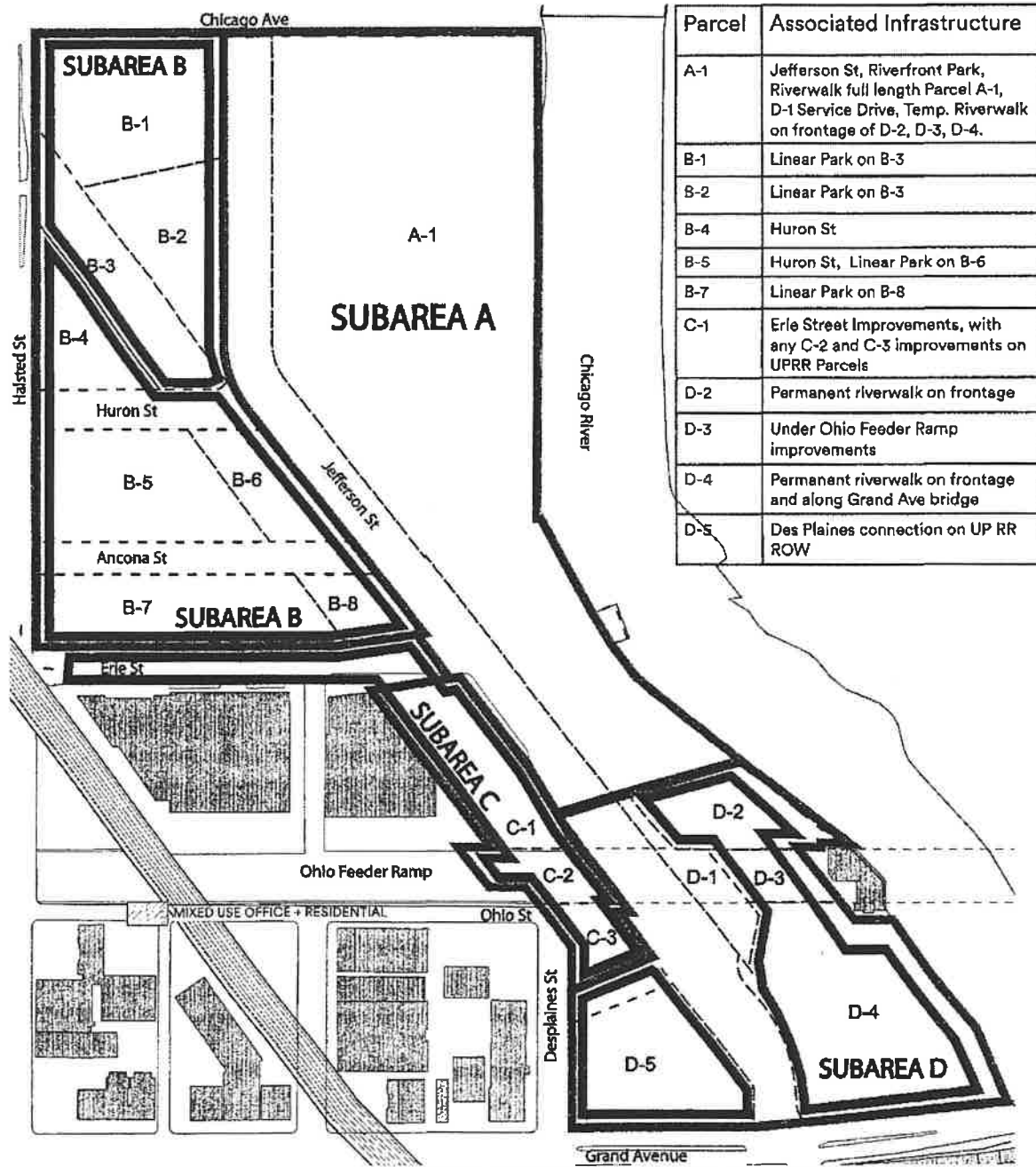


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Introduced: September 21, 2022
 Plan Commission: December 12, 2022

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DEVELOPMENT PLAN WITH ASSOCIATED INFRASTRUCTURE REQUIREMENTS

Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

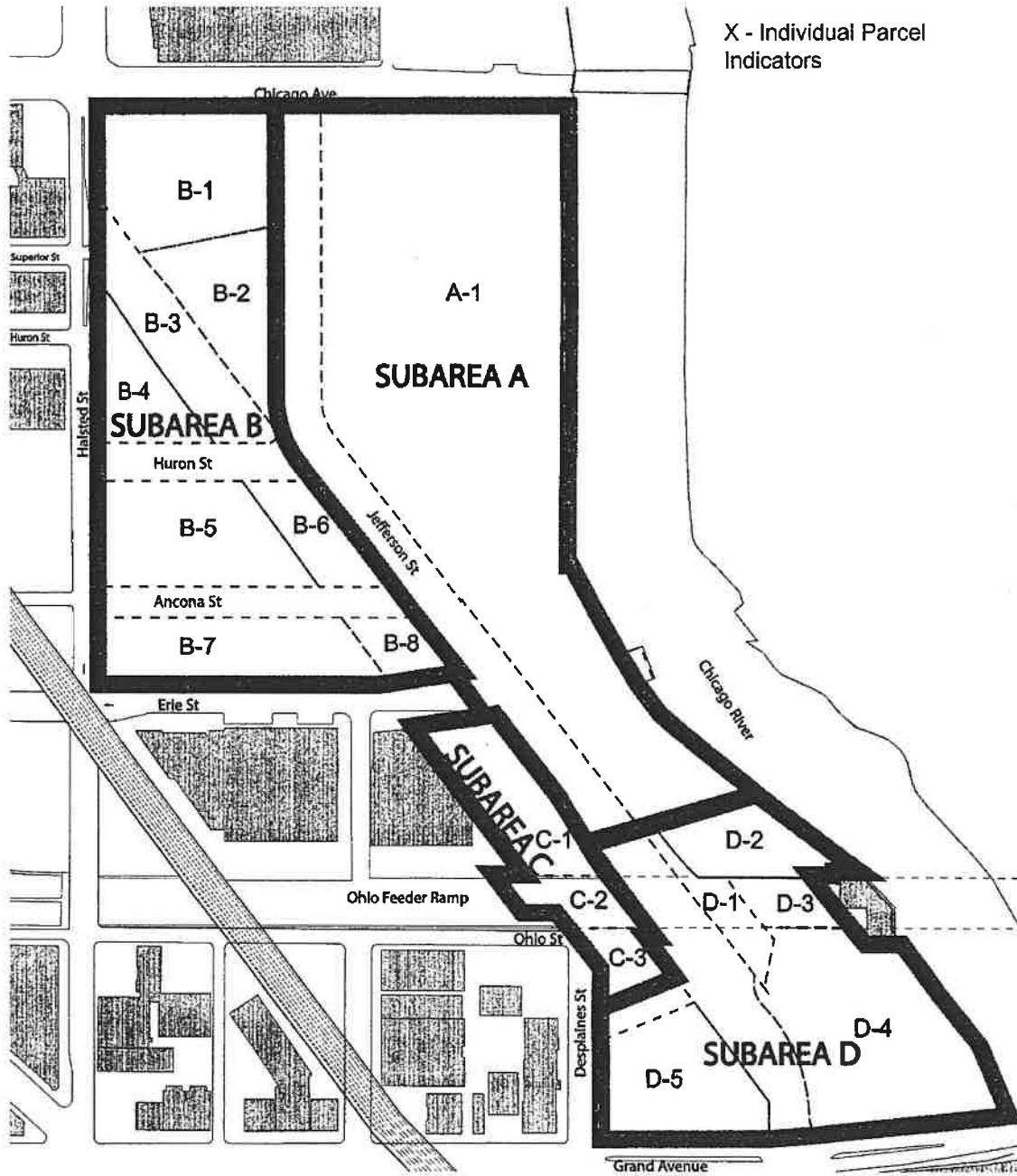
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0' 125' 250'



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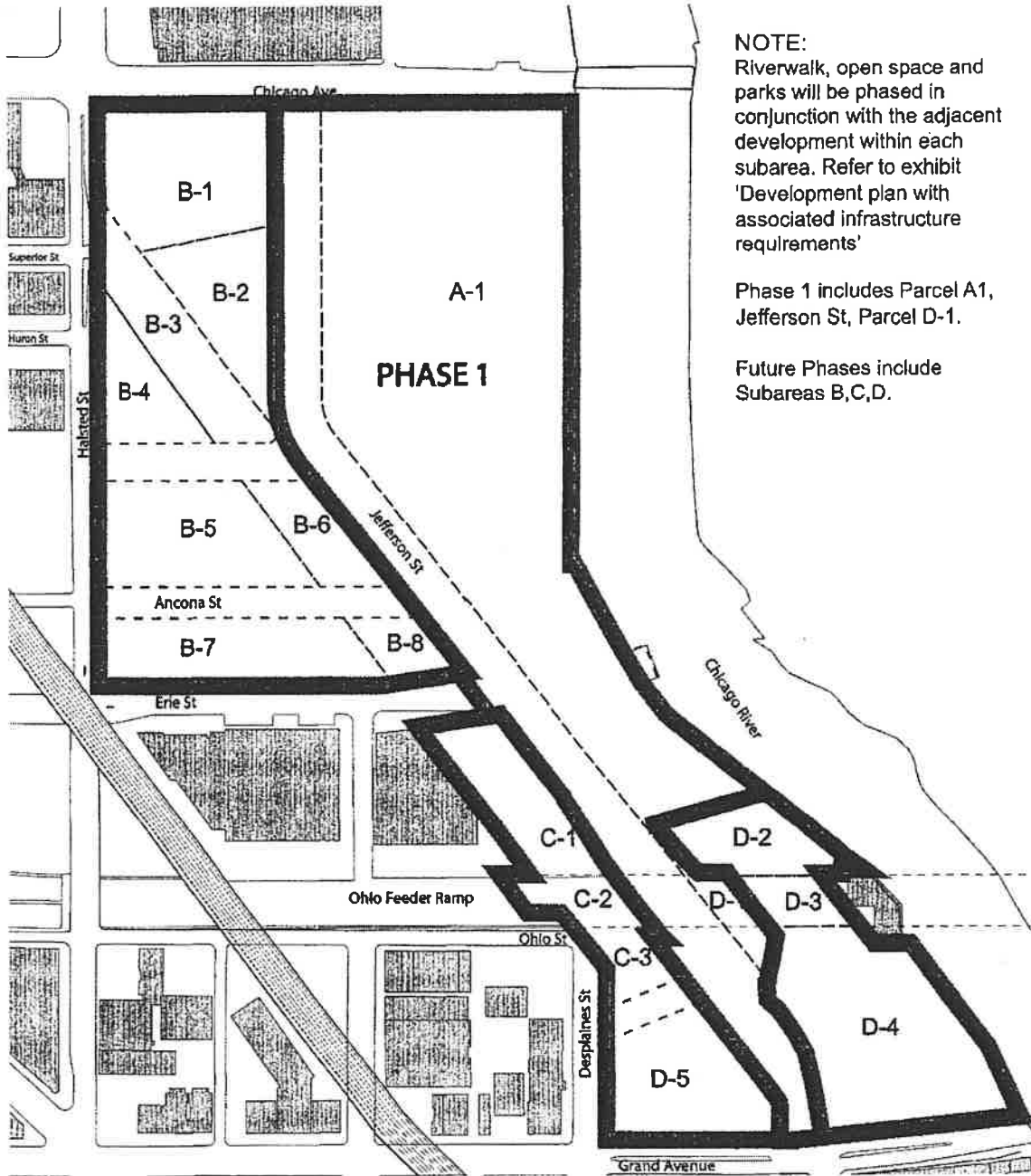


SUBAREA AND PARCELIZATION PLAN

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NOTE:
 Riverwalk, open space and parks will be phased in conjunction with the adjacent development within each subarea. Refer to exhibit 'Development plan with associated infrastructure requirements'

Phase 1 includes Parcel A1, Jefferson St, Parcel D-1.

Future Phases include Subareas B,C,D.

DEVELOPMENT PHASING DIAGRAM

0' 125' 250'

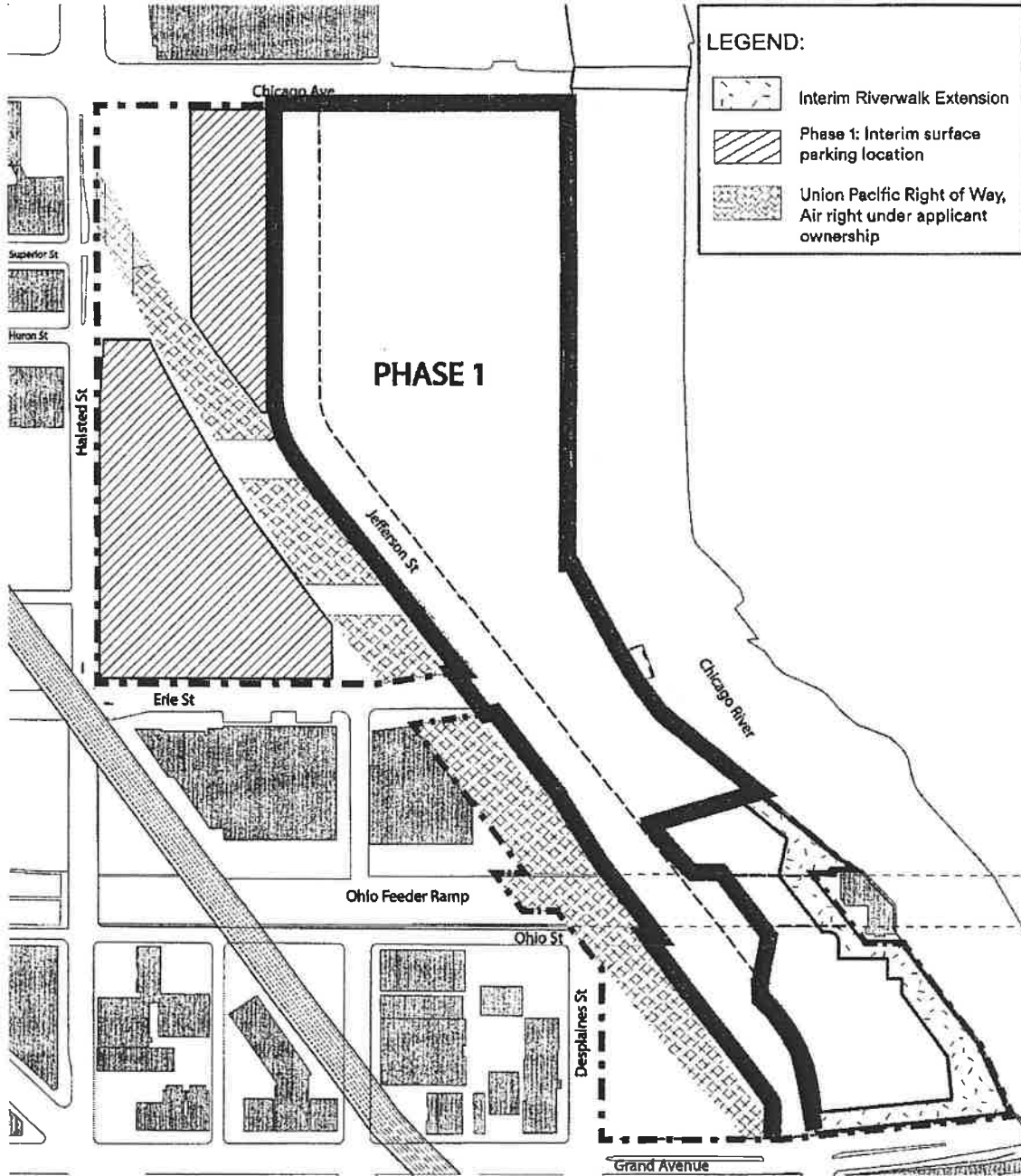


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


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LEGEND:

-  Interim Riverwalk Extension
-  Phase 1: Interim surface parking location
-  Union Pacific Right of Way, Air right under applicant ownership

Note: Refer to detailed interim parking site plan

PHASE 1 - INTERIM SITE PLAN FOR PARKING AND RIVERWALK

0' 125' 250'

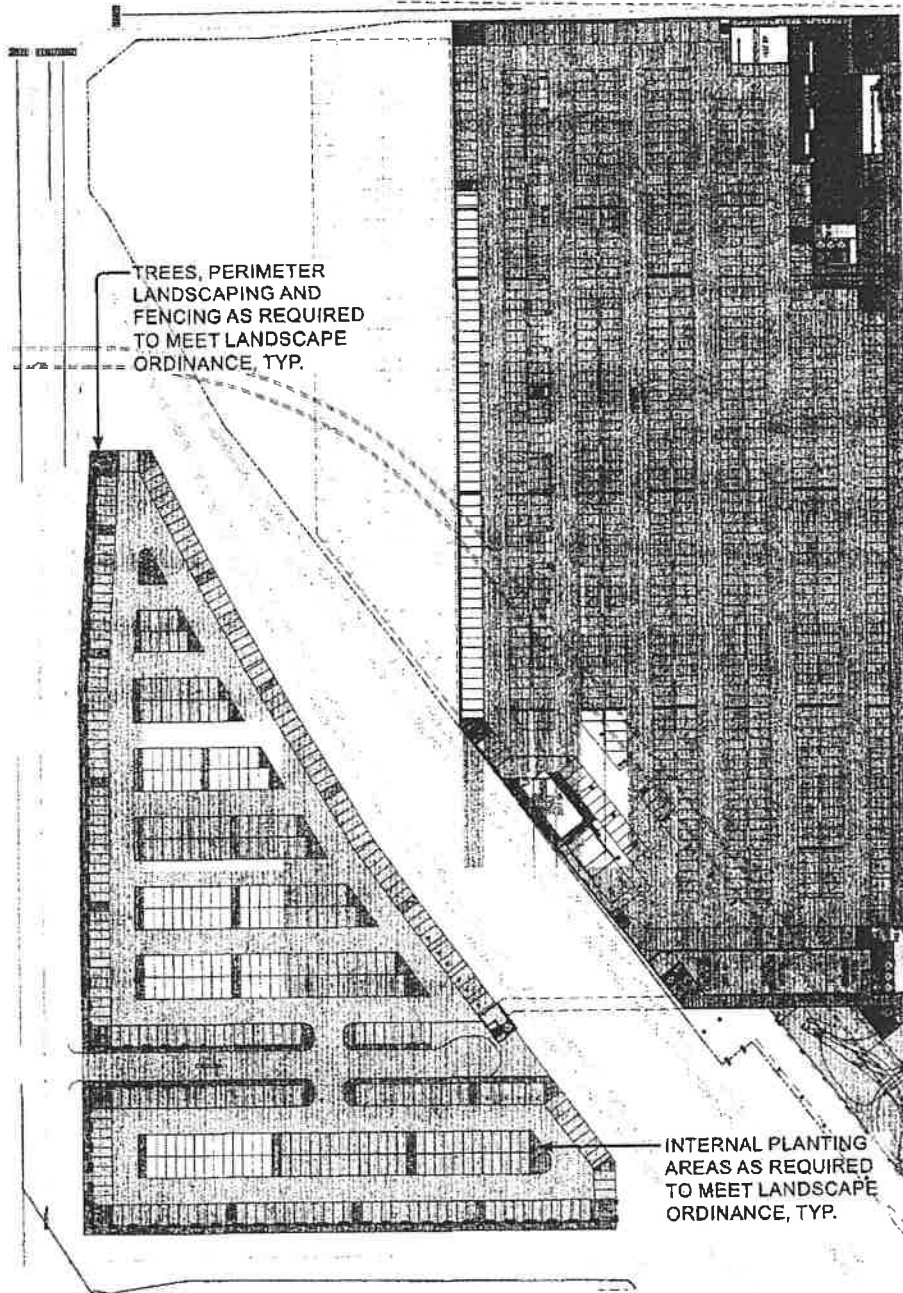


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Note: Interim Parking lots to comply with City of Chicago Landscape Ordinance

PHASE 1 - INTERIM SITE PLAN FOR PARKING

0' 125' 250'

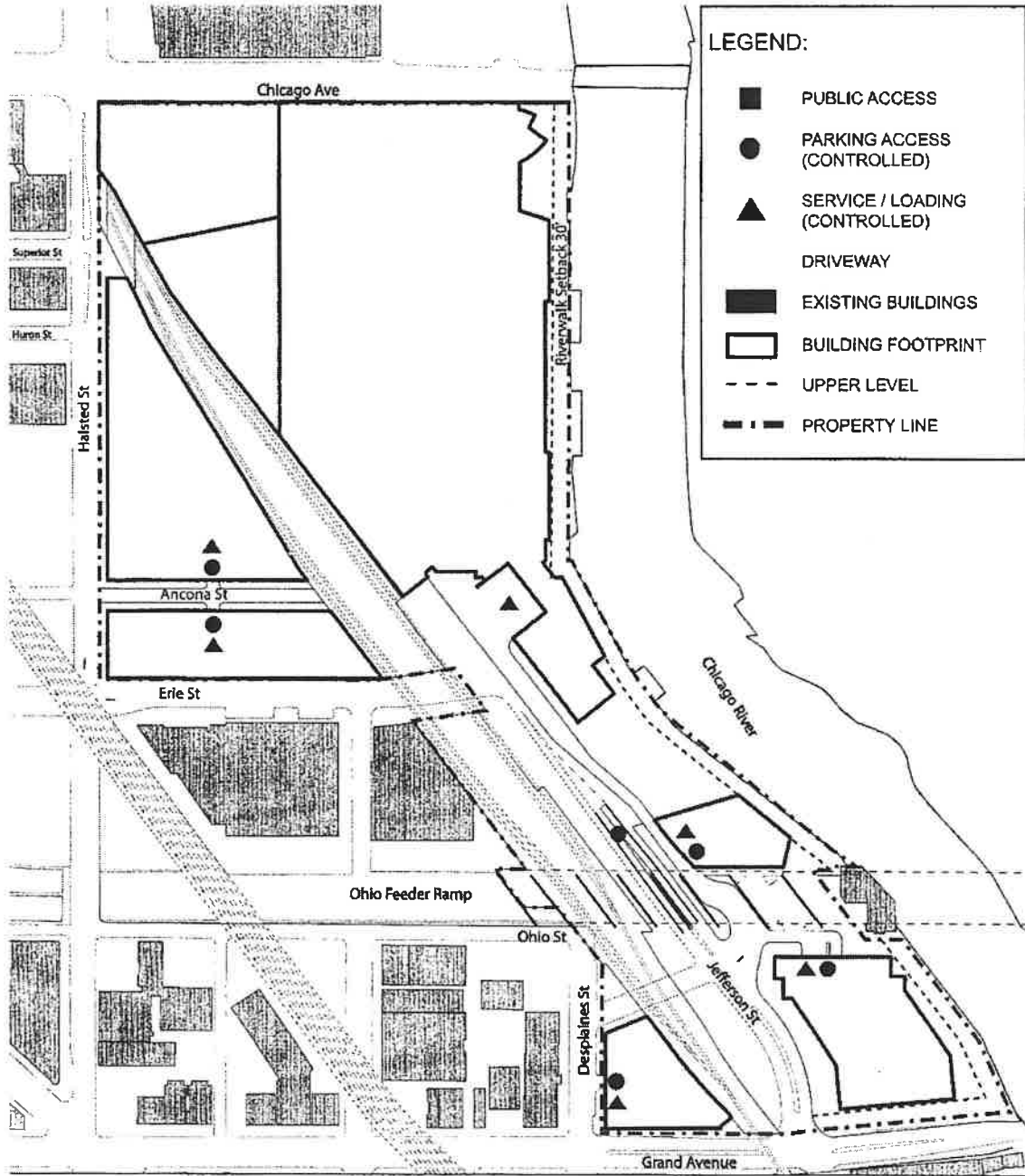


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**SITE ACCESS AND LOADING PLAN LEVEL 1
(+0.0 TO +30.0 CCD)**

0' 125' 250'

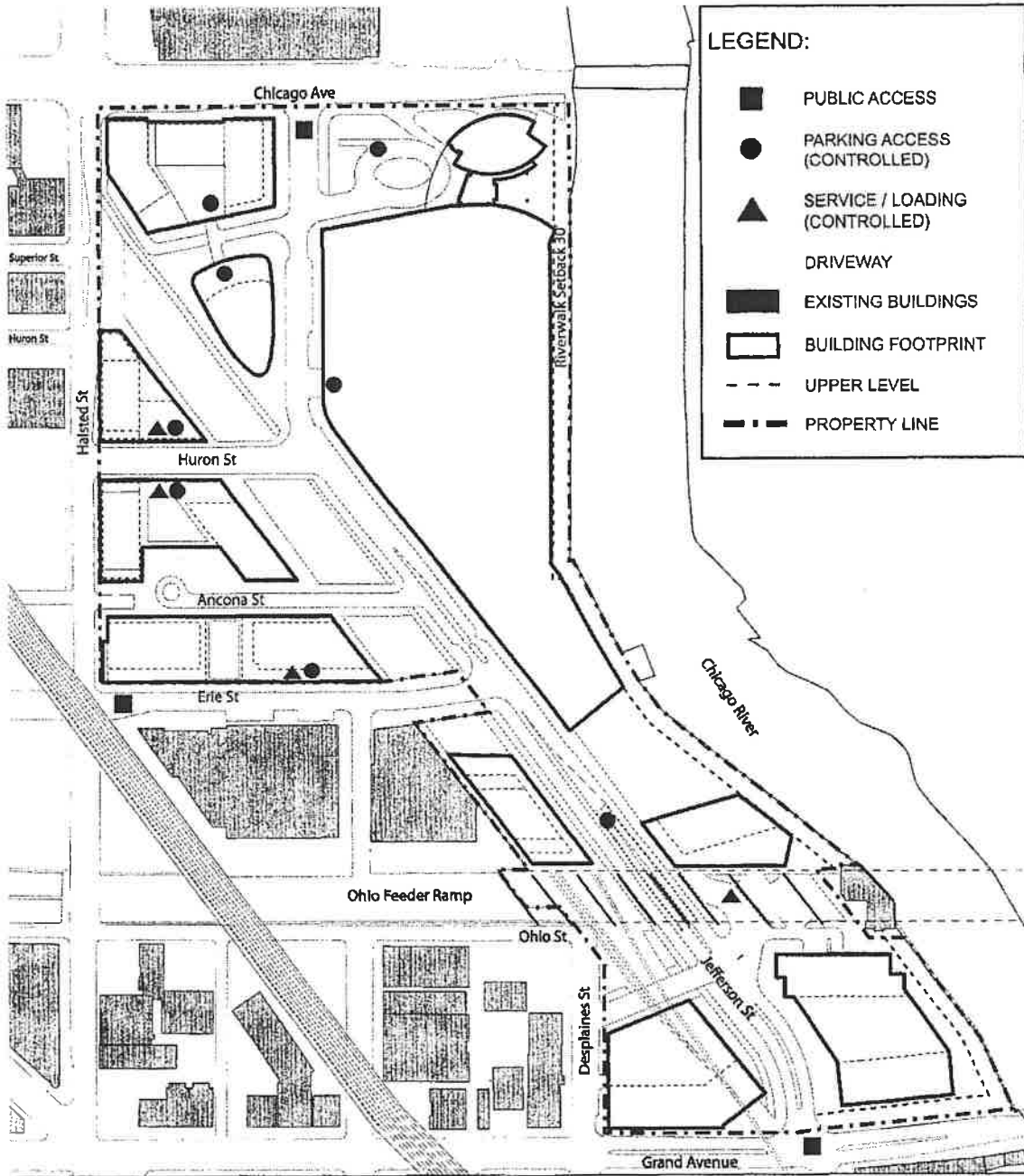


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SITE ACCESS AND LOADING PLAN LEVEL 2 (+30.0 TO +45.0 CCD)

0' 125' 250'

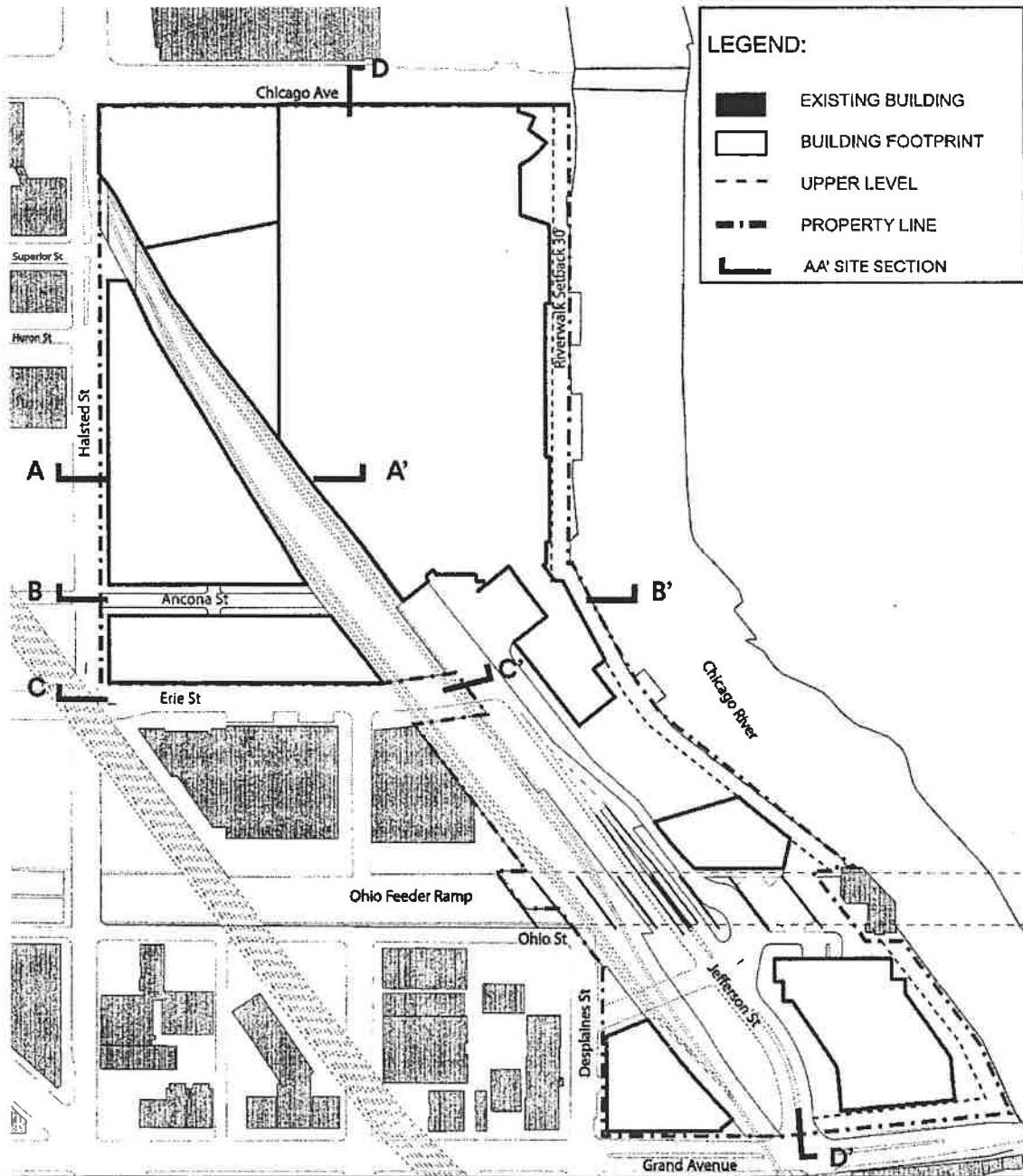


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SITE LEVEL PLAN: LEVEL 1 (+0.0 TO +30.0 CCD)

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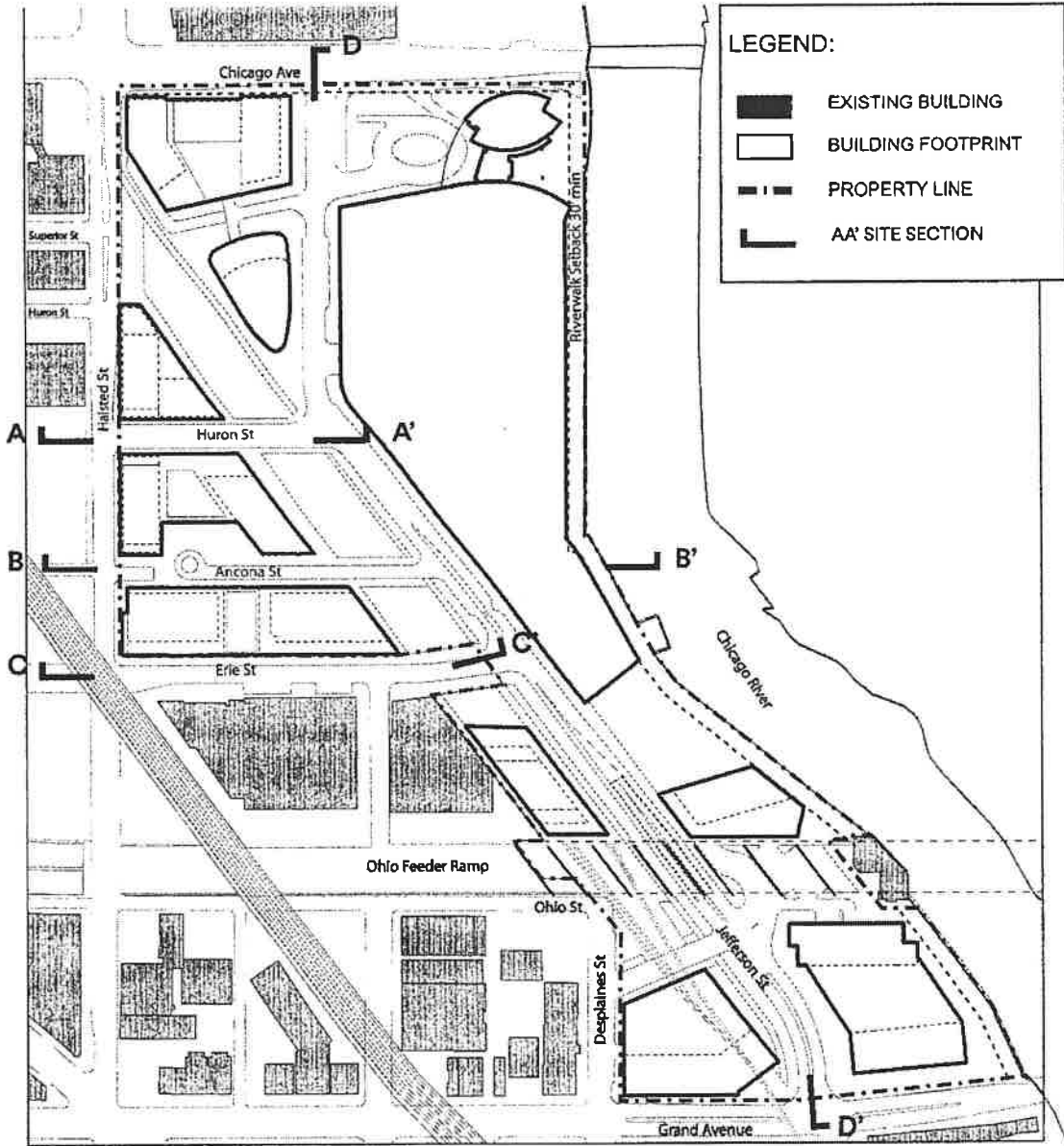


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SITE LEVEL PLAN: LEVEL 2 (+38.0 TO +45.0 CCD)

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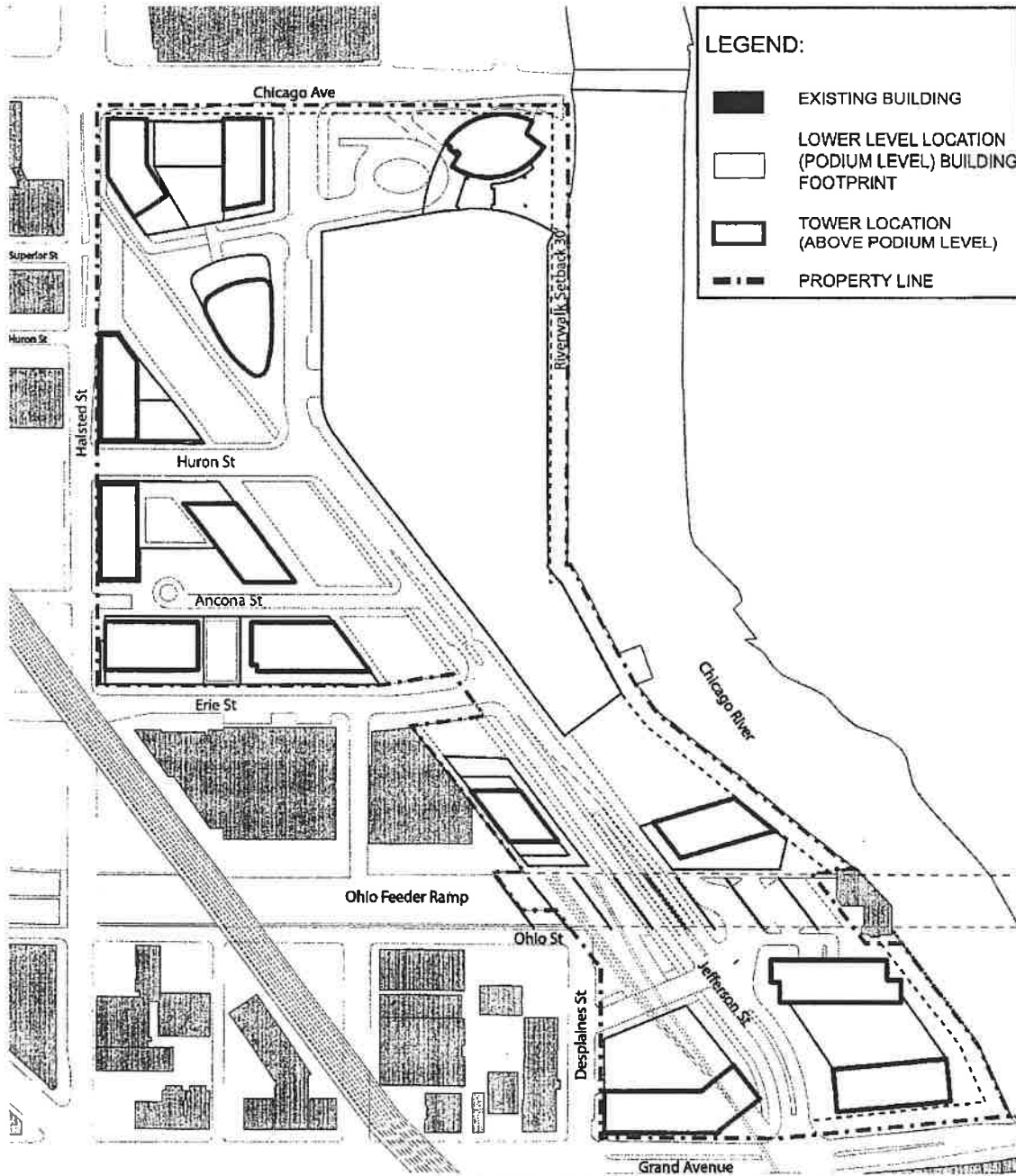


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SITE LEVEL PLAN: LEVEL 3 (+45.0 CCD)

0' 125' 250'

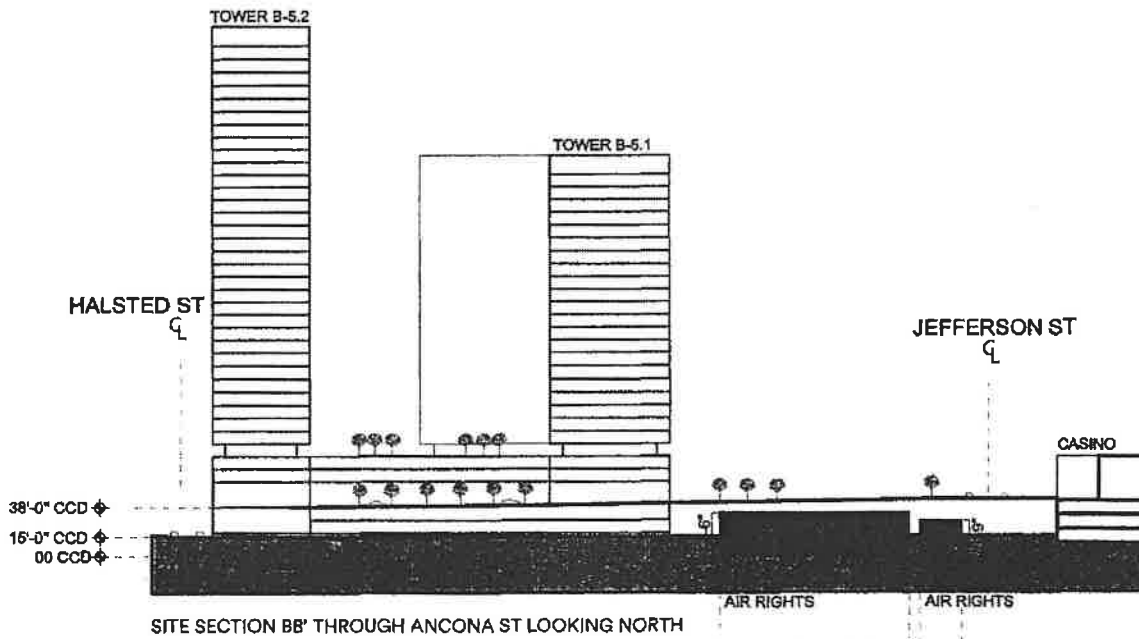
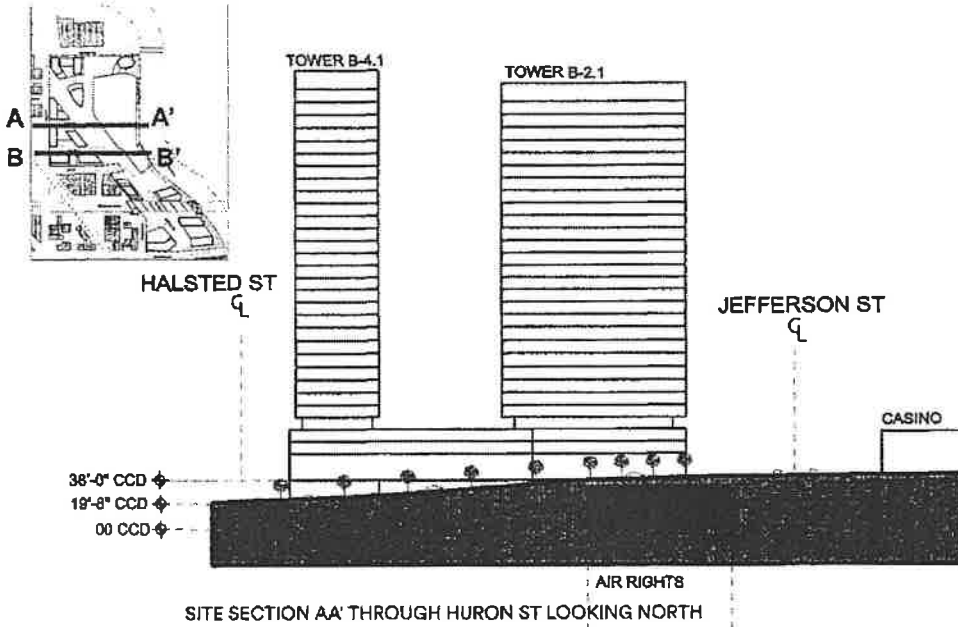


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SITE SECTION PLAN (1 OF 2)

0' 125' 250'

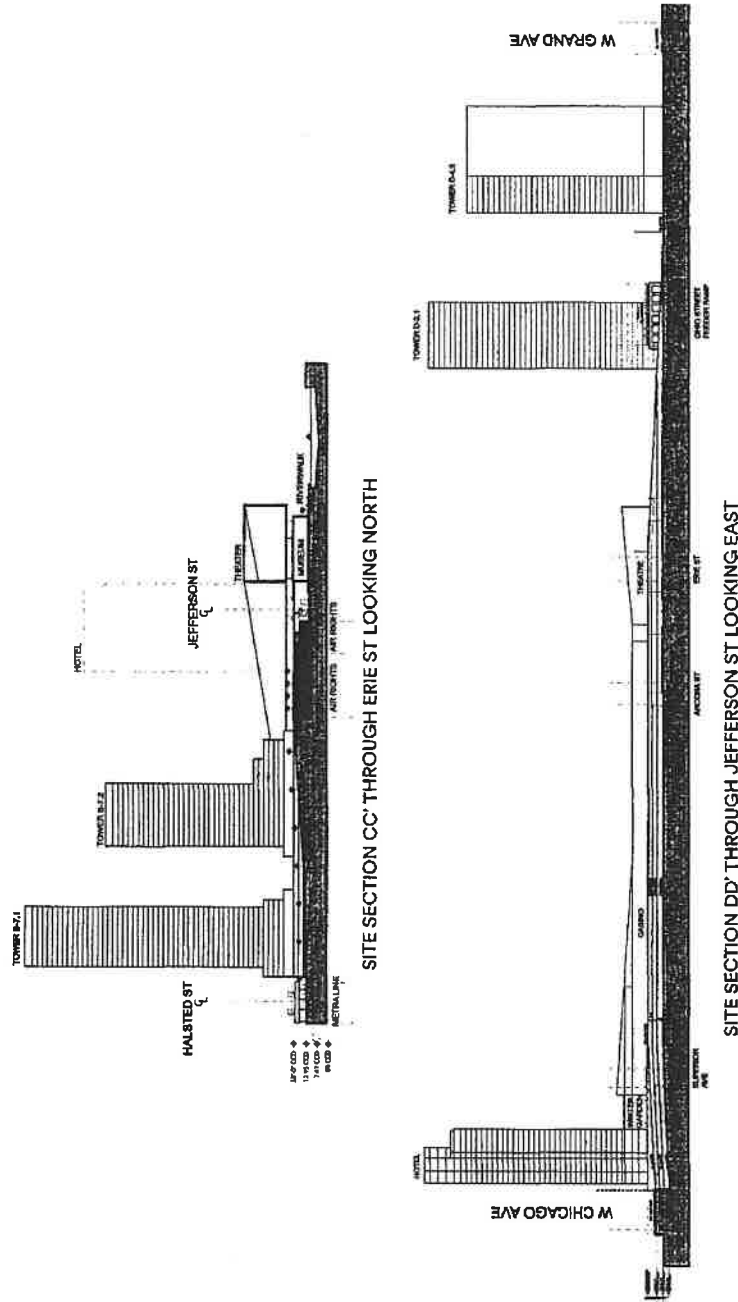
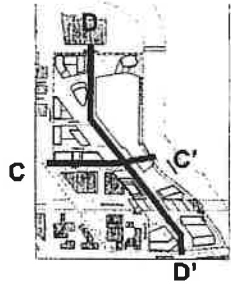


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SITE SECTION PLAN (2 OF 2)

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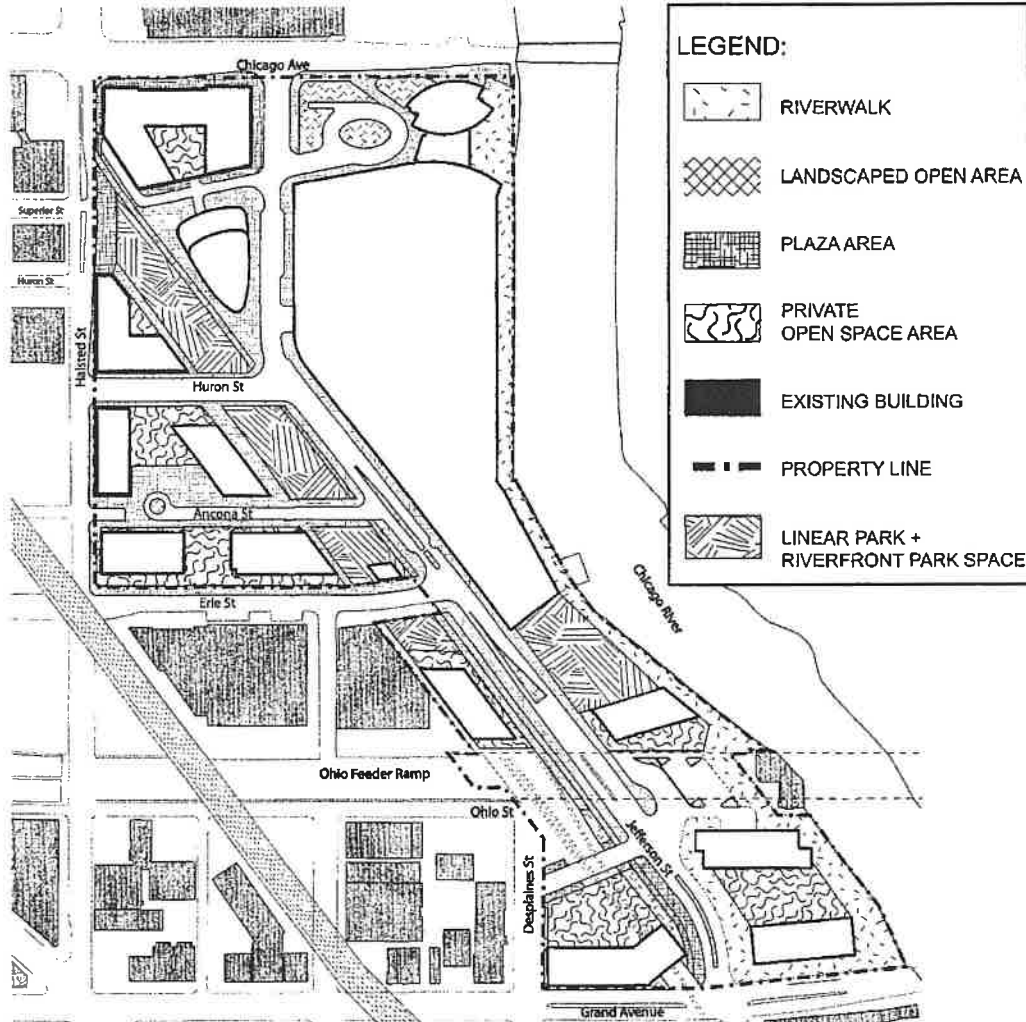
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OPEN SPACE CALCULATIONS	GSF	Acres	
Publicly Accessible Open Space			
- Riverwalk (includes natural areas and landscaped stormwater features)	148,793	3.42	32% of Public Open Space
- Landscaped Open Area (includes natural areas and landscaped stormwater features)	110,960	2.55	24% of Public Open Space
- Plaza Area/Elevated Plaza (includes natural areas and landscaped stormwater features)	199,045	4.57	43% of Public Open Space
Subtotal: Publicly Accessible Open Space	465,660	10.54	35% of Total Site Area
Subtotal: Private and Common Open Space	123,678	2.84	9% of Total Site Area
Total Open Space (Public & Private Space)	598,500	13.37	45% of Total Site Area

PUBLIC SPACE OPEN PLAN

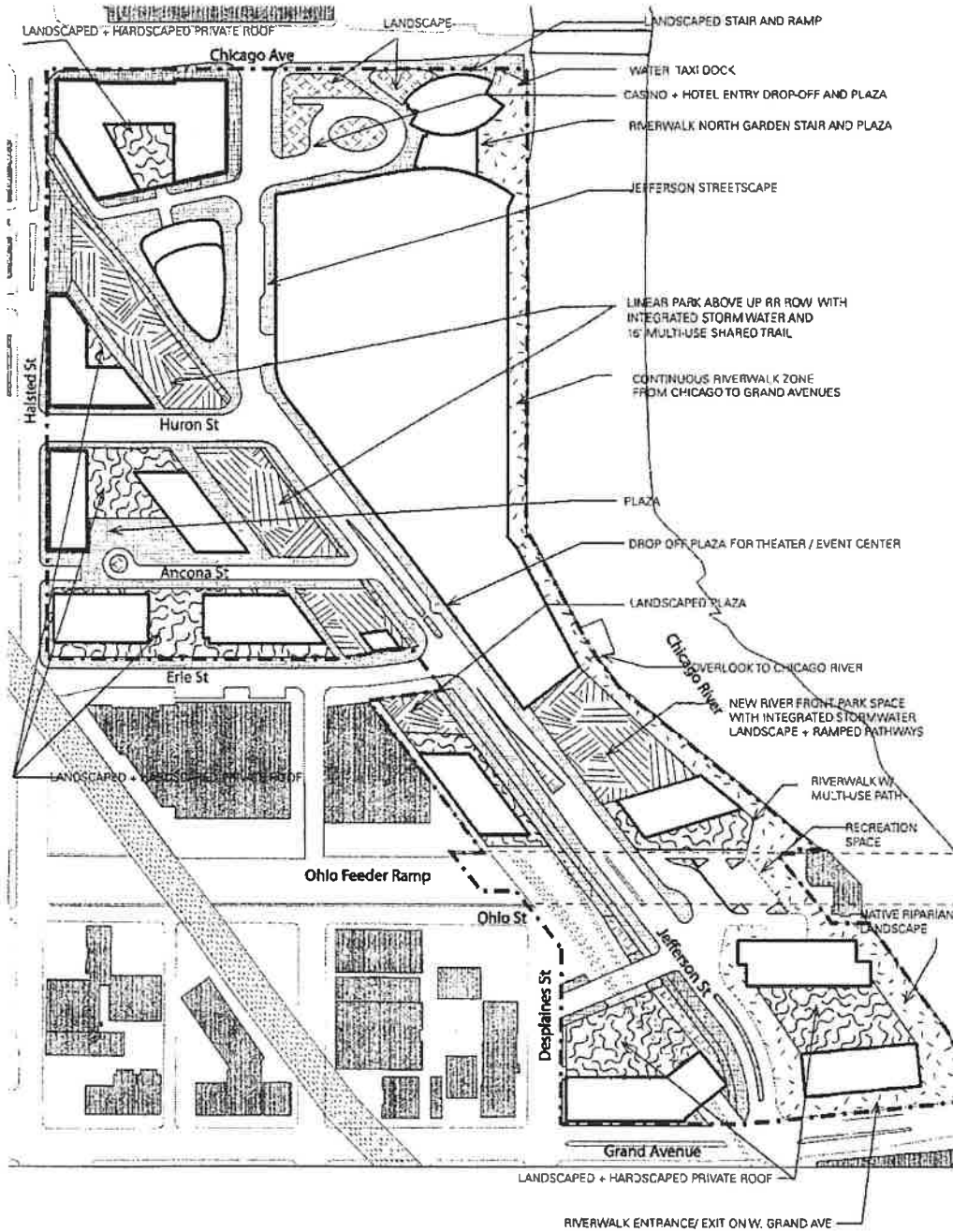
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GENERALIZED LANDSCAPE PLAN

0' 125' 250'

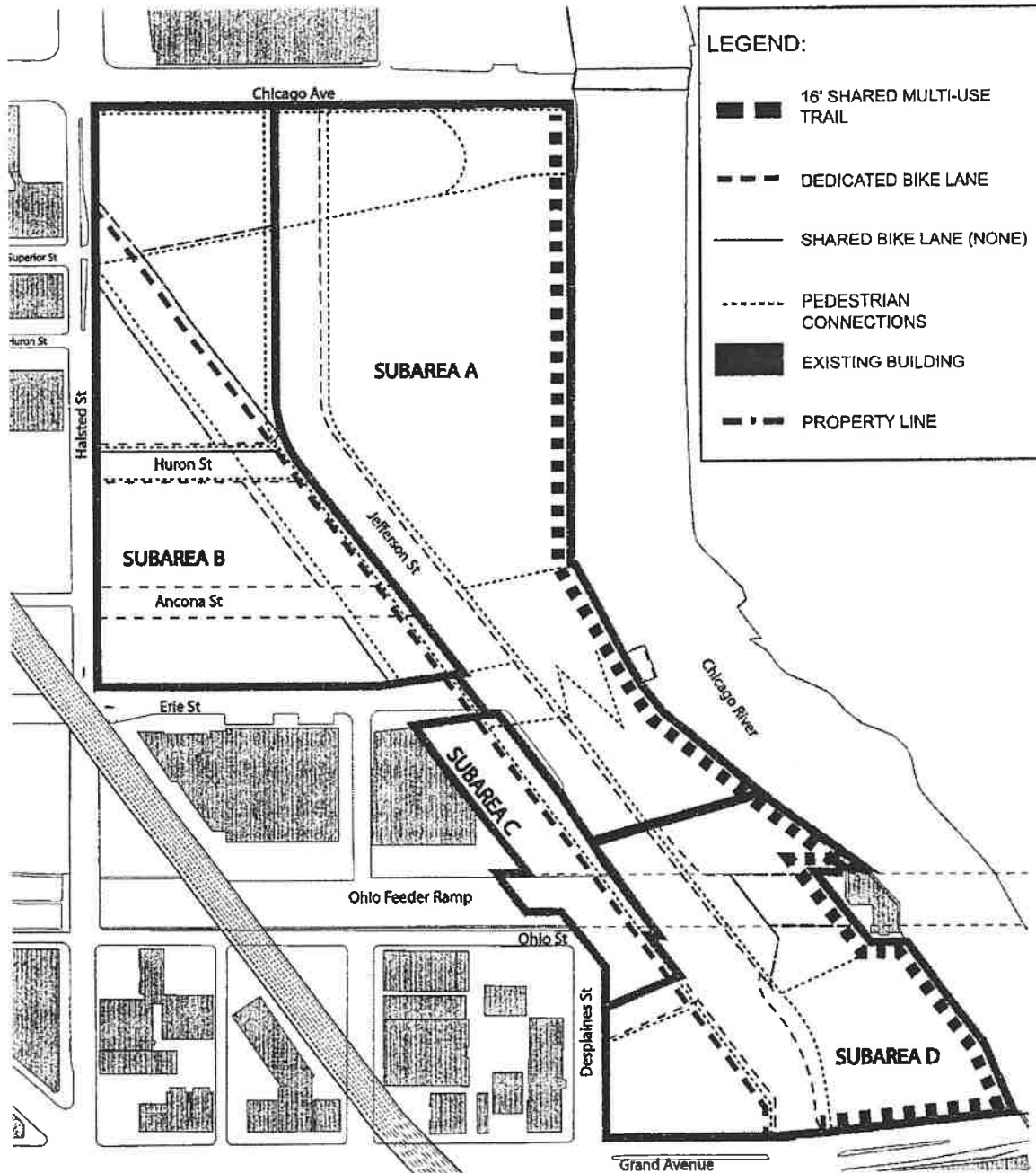


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BIKE AND PEDESTRIAN CONNECTIVITY PLAN -ULTIMATE BUILD OUT

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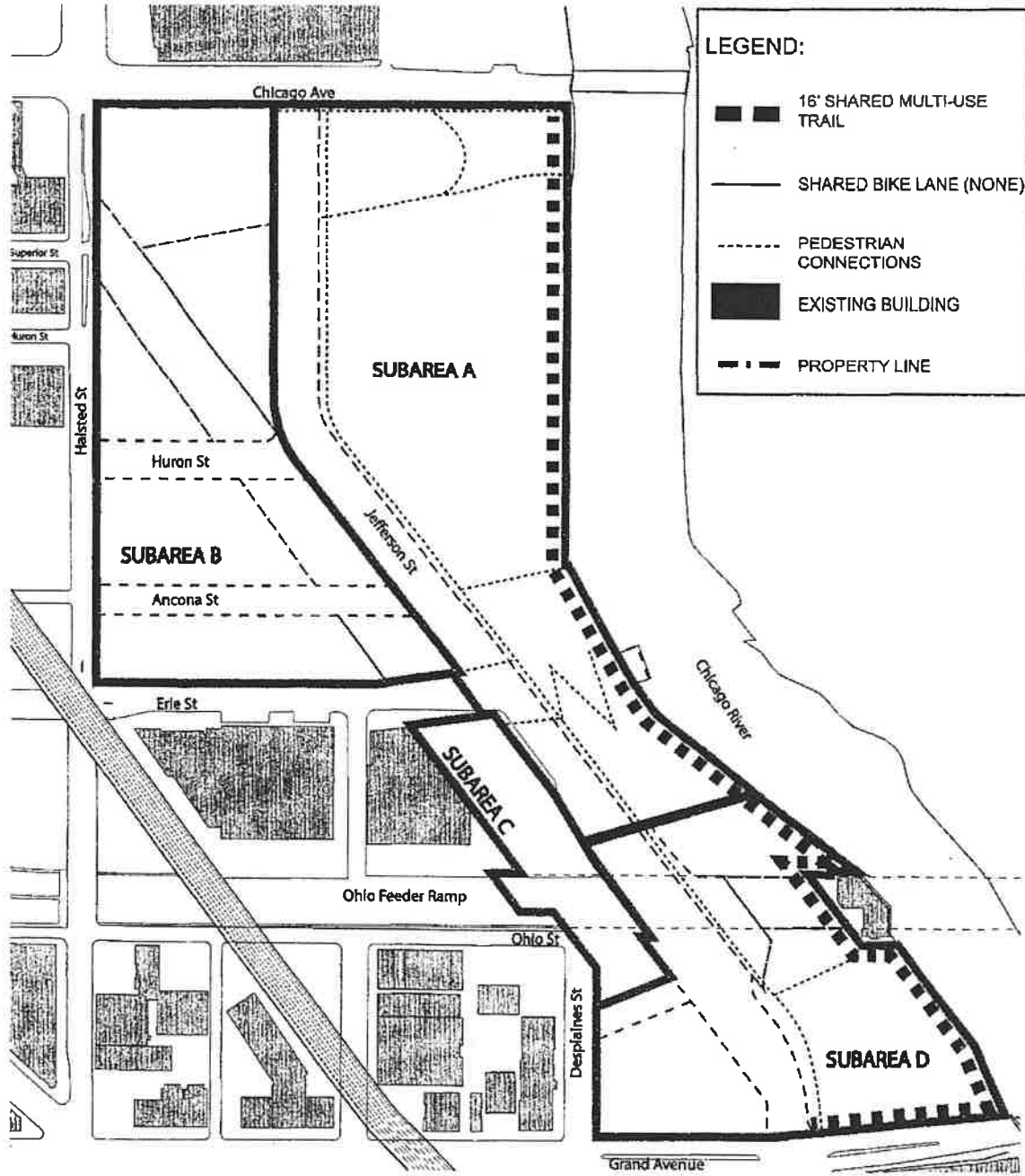
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PHASE 1-BIKE AND PEDESTRIAN CONNECTIVITY PLAN

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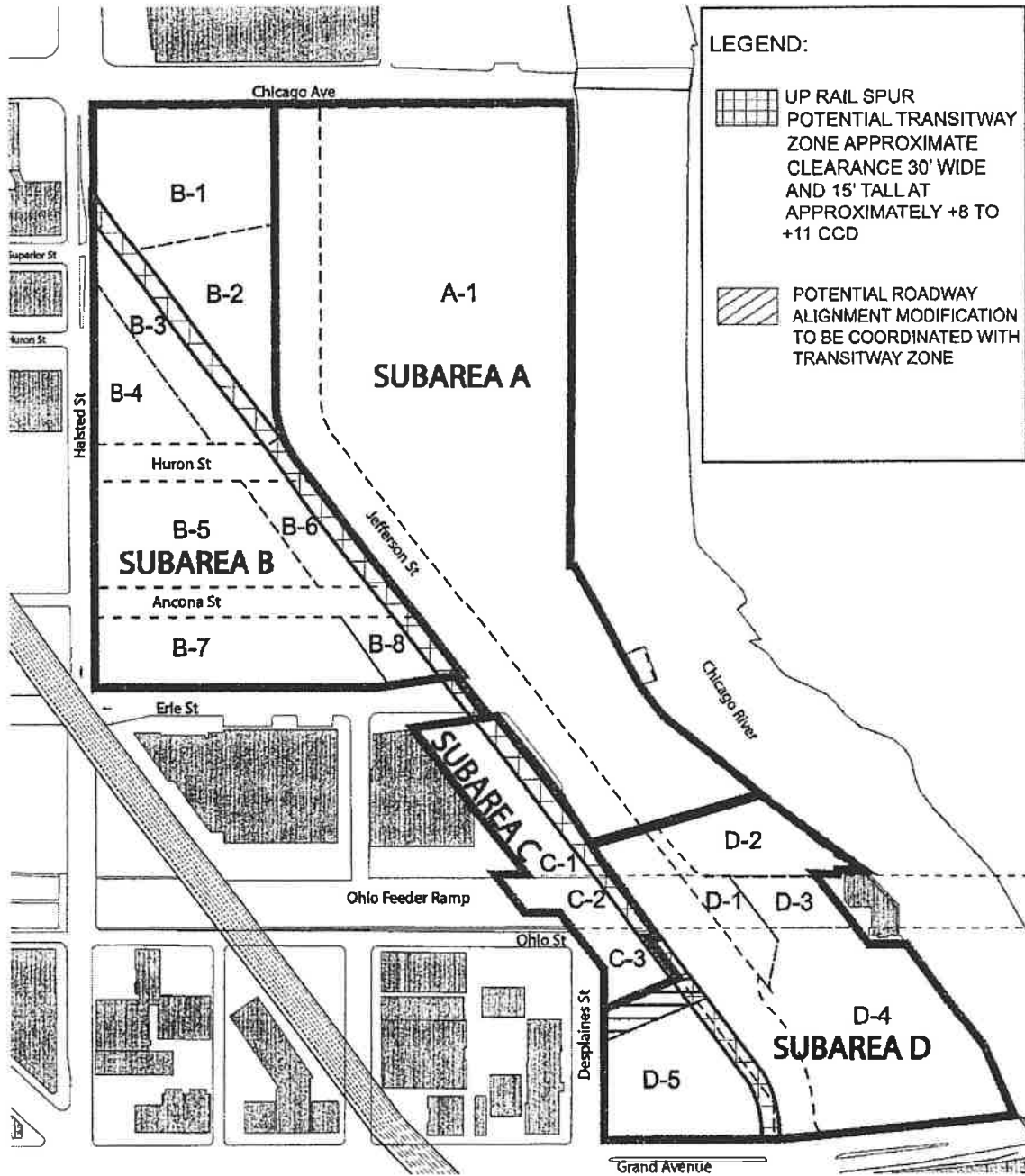


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TRANSITWAY ACCOMMODATION PLAN

0' 125' 250'



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All development within this planned development must substantially comply with the Design Guidelines of the North Branch Framework adopted by the Chicago Plan Commission on May 18, 2017, or as amended, the Chicago River Corridor Design Guidelines and Standards, or as amended, and with the design standards and guidelines outlined in the Zoning Ordinance, Section 17-8-0900 Standards and Guidelines. The guidelines listed below provide additional standards for buildings and public spaces to complement the specific context of this planned development:

GENERAL DEVELOPMENT GUIDELINES

- Building siting and base design shall define the street walls along Jefferson Street, Chicago Avenue, and Halsted Street, with setbacks allowed for plazas and open spaces.
- Jefferson Street shall be developed as a central street with wide sidewalks, street trees and landscaping to promote stormwater management.
- Active ground floor uses shall be integral to buildings along the west side of Jefferson Street, Chicago Avenue, and Halsted Street, and shall include active uses such as retail, lobbies, or office space.
- Active ground-floor uses shall have direct access to publicly-accessible open spaces and the riverwalk where feasible.
- Buildings at the corner of Halsted Street and Chicago Avenue shall incorporate massing, siting, and façade designs to express and reinforce a district gateway.
- In general, vehicular pick-up and drop-off shall be oriented away from Chicago Avenue and Grand Avenue. Drop off zones on Halsted to be coordinated with CDOT and CTA. Parking and service entrances for the casino and other buildings along Jefferson Street will be provided via access drives or driveways. Pick-up and drop-off locations within the public way may be incorporated only where curbside space exists for that use.
- Curb-cuts should not exceed 20' in width and parking entries should be integrated with the building's architecture through the use of similar materials, patterns, textures, and color. Curb cuts shall be coordinated with CDOT and may be widened within CDOT's standards if required to provide adequate access for service, loading, and shuttle services.
- To preserve access to light, a minimum separation of 40' between towers is required. Where site constraints permit, towers will be oriented to maximize passive cooling, natural lighting, and energy efficiency.
- Undeveloped parcels may support interim uses including, but no limited to recreational open space, dog friendly areas, and surface parking. Interim use plan improvements and time frame will require review and approval by DPD.

BUILDING ENVELOPE DESIGN GUIDELINES

- Building designs shall achieve a varied and distinctive skyline.
- Mechanical penthouses and any upper level parking floors shall be screened with high-quality architectural elements consistent with the overall building facade.
- Parking levels shall be screened adequately to obscure car headlights and sound from neighboring buildings.
- Podium and tower designs shall relate to each other to provide a cohesive expression.
- Podium roofs will incorporate active amenity decks and landscaping as appropriate to building uses.
- Balconies and any required ventilation shall be integrated within the design of the building facade.
- Tenant signage for each building should be considered as part of the facade design to ensure consistency of placement, size, materials, and method of illumination.

MATERIAL GUIDELINES FOR BUILDINGS

- Buildings will not be clad with low quality materials and materials with low aesthetic value such as Exterior Insulation and Finish System (EIFS), exterior exposed Concrete Masonry Units (CMU), thin brick, or residential-type vinyl and metal siding.
- Buildings will employ architectural materials consistent with contemporary building practices, such as high quality wall systems in glass, metal, masonry, high-quality architectural concrete, or hardwood. Glazing shall not be highly reflective or mirrored and be bird friendly design.
- Building envelopes will support environmentally responsible design by reducing heat loads, improving energy efficiency, maximizing occupant comfort, and using sustainable materials.
- Podium and ground-floor level materials and design facing the streets, publicly accessible open spaces, and the riverwalk will be detailed to enhance the pedestrian environment and will be complimentary to the context.
- Ground-floor facades will have a high percentage of clear / un-tinted glazing along the primary street frontages.

LANDSCAPE GENERAL GUIDELINES

- A riverfront publicly accessible park will connect directly to the riverwalk and include gathering places, a lawn, passive play spaces, native landscaping, opportunities for direct viewing of river, and stormwater infiltration.
- Landscaped stairs and ramps will connect the riverwalk and Jefferson Street, and provide view corridors from Halsted Street to the river. Programming will include a variety of uses such as seating, play areas, a location for public art, and a dog park.
- The space under the Ohio Feeder Ramp will be activated with recreational uses that connect to the riverwalk park spaces.
- Publically accessible open spaces will be designed to the applicable standards of Chicago Park District Parks.
- The riverwalk will connect to the north side of Chicago Avenue using space provided by CDOT under the new Chicago Avenue Bridge.
- The riverwalk will connect to the street level intersection of Grand Avenue and Jefferson Street, and not preclude space for future Riverwalk connection under Grand Avenue bridge.
- Landscaping throughout will incorporate stormwater management best practices to detain, clean, and reduce the volume of stormwater discharge.
- Landscaped areas will enhance local habitat for wildlife through use native plantings.
- In key locations, floating wetlands can be located along the riverwalk where feasible.

DESIGN GUIDELINES

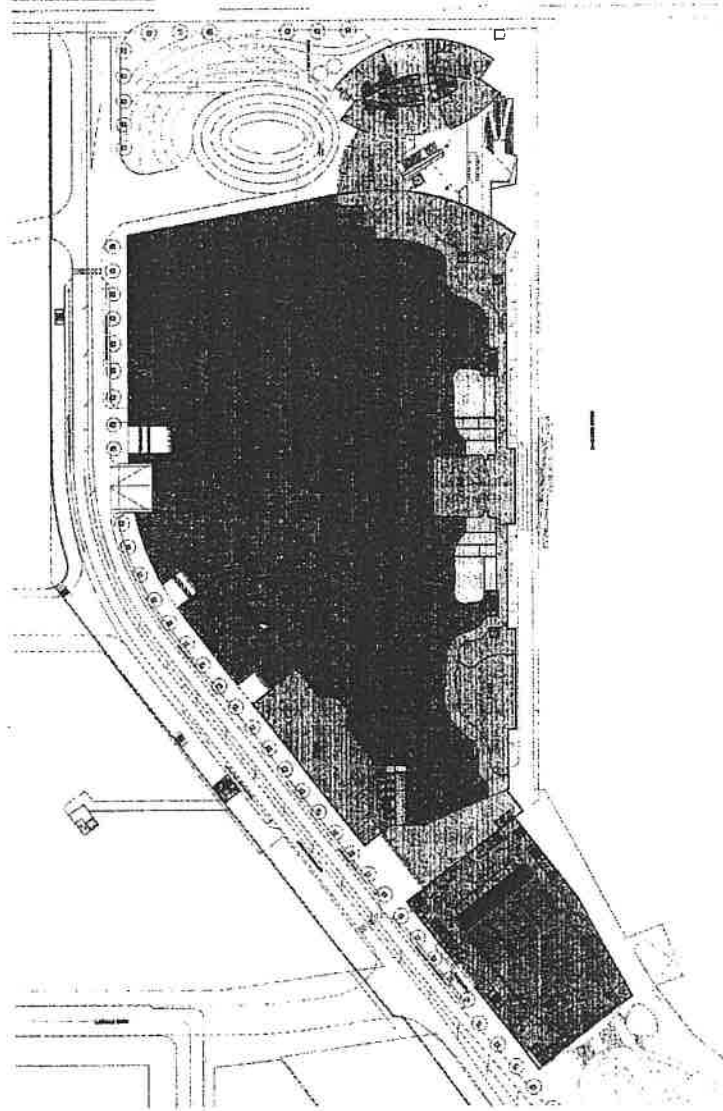
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0' 125' 250'



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PHASE 1 FLOOR PLAN - L1

0' 125' 250'

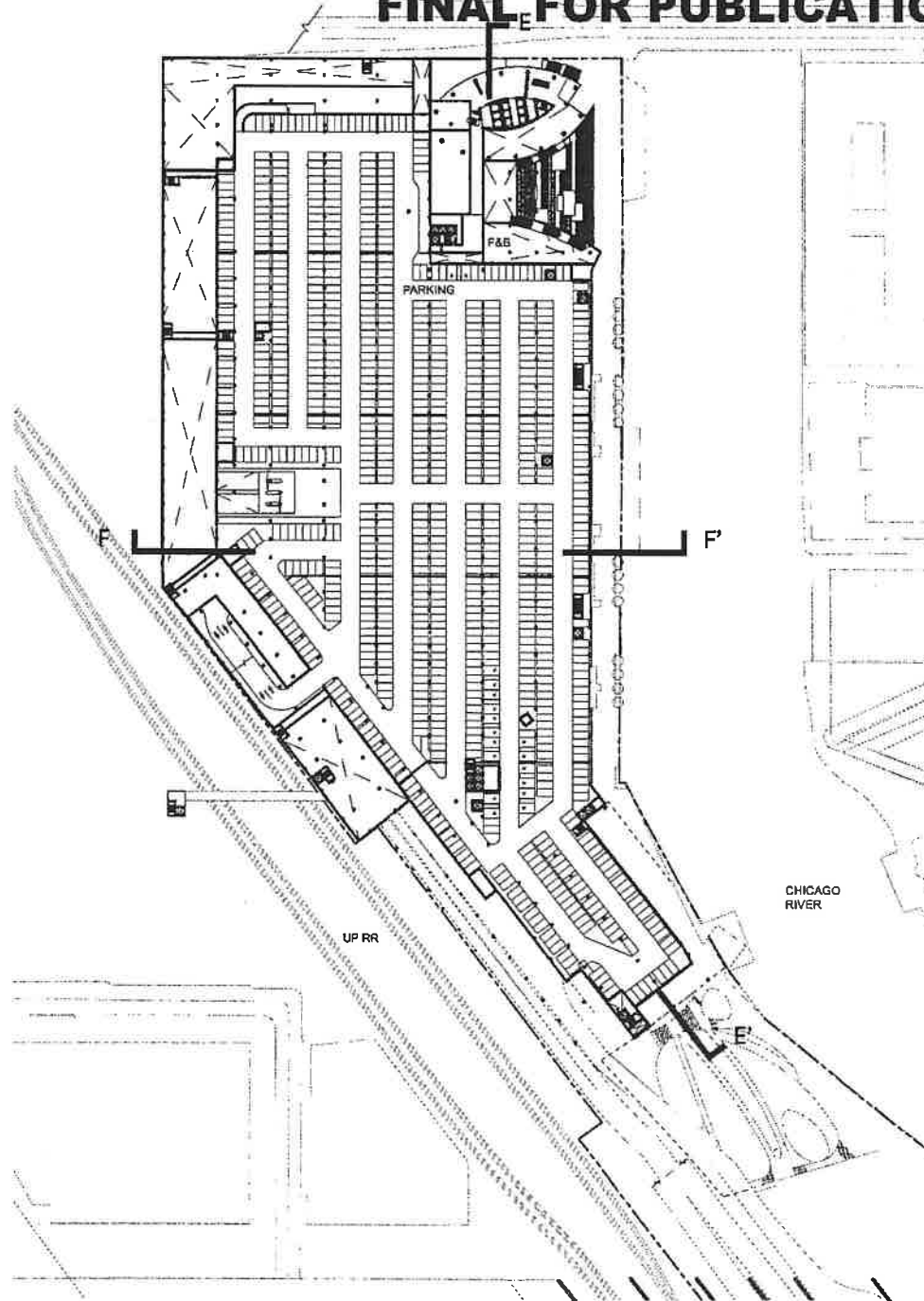


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PHASE 1 FLOOR PLAN - P1

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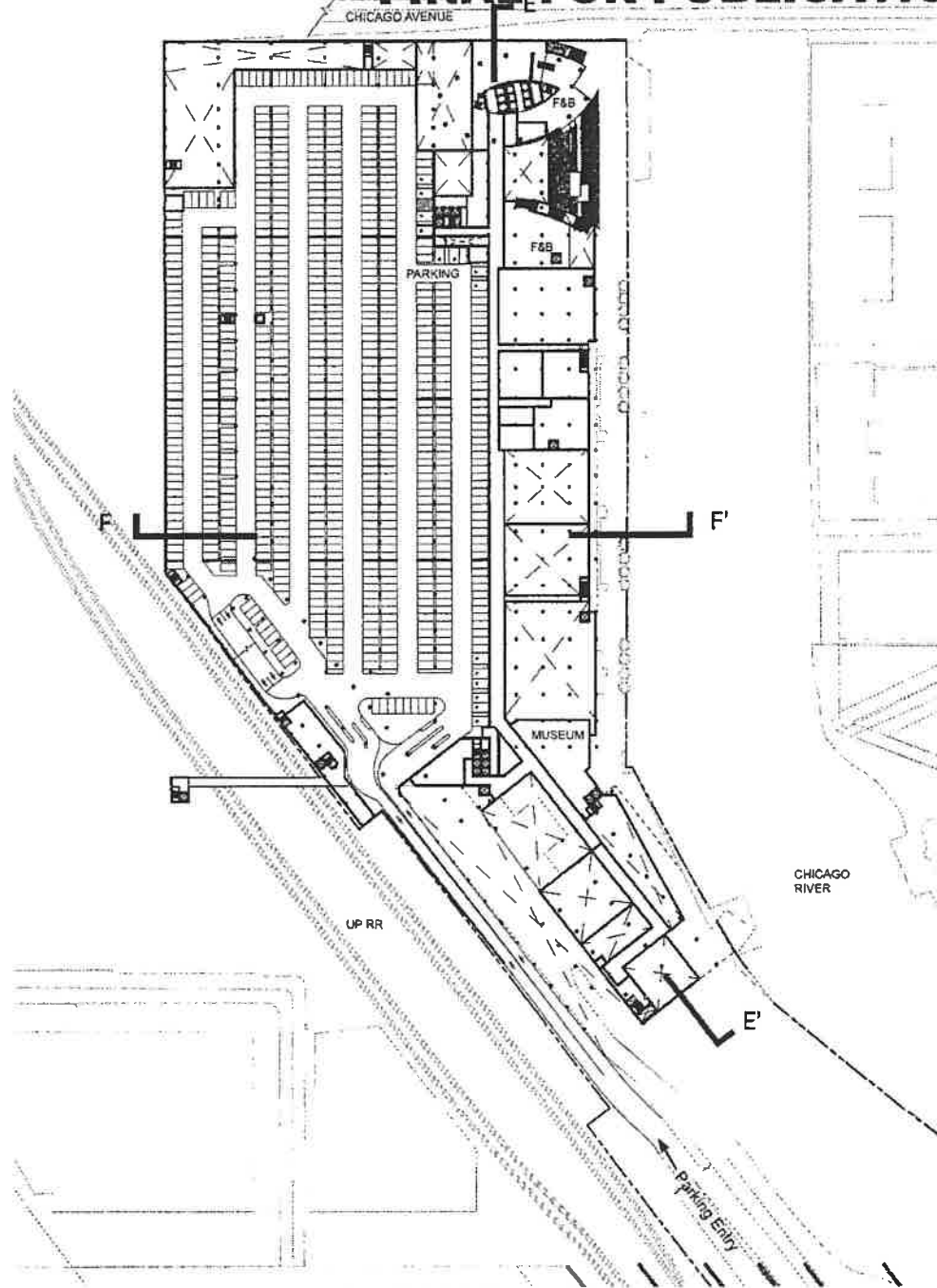


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PHASE 1 FLOOR PLAN - P2

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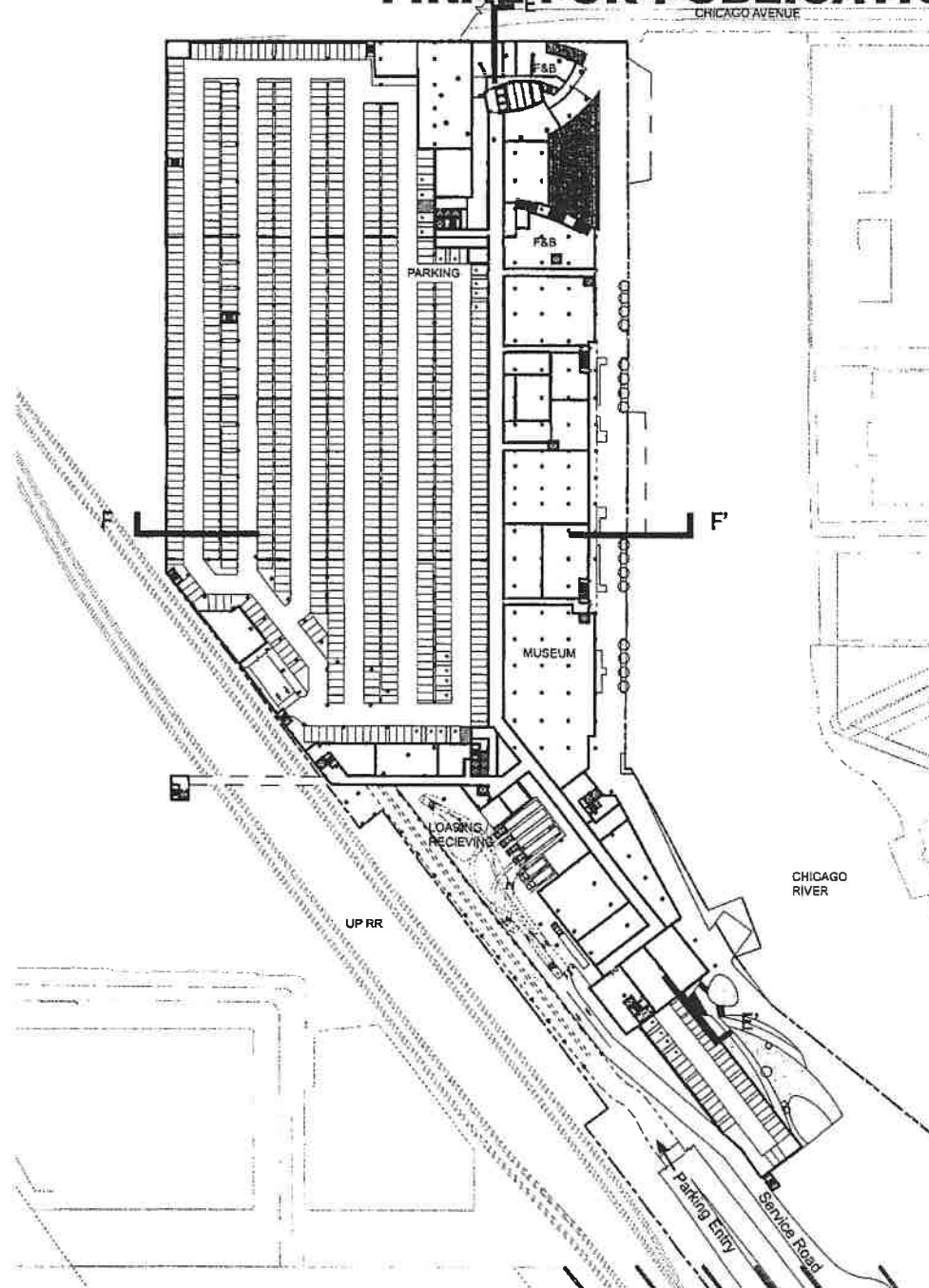


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PHASE 1 FLOOR PLAN - P3

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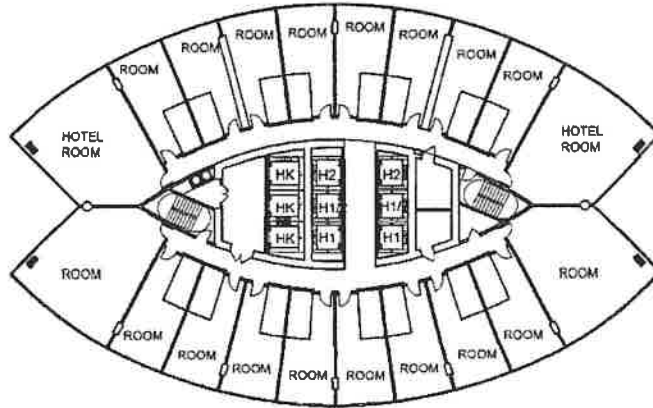


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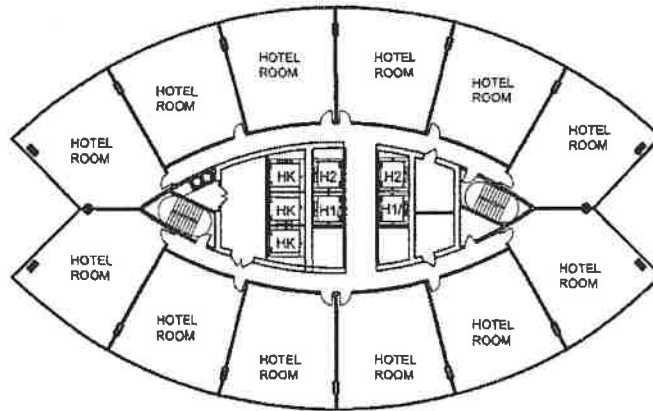
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HOTEL FLOOR PLAN 1



HOTEL FLOOR PLAN 2

PHASE 1 FLOOR PLAN - TYPICAL HOTEL PLANS

0' 25' 50'

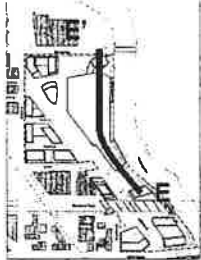


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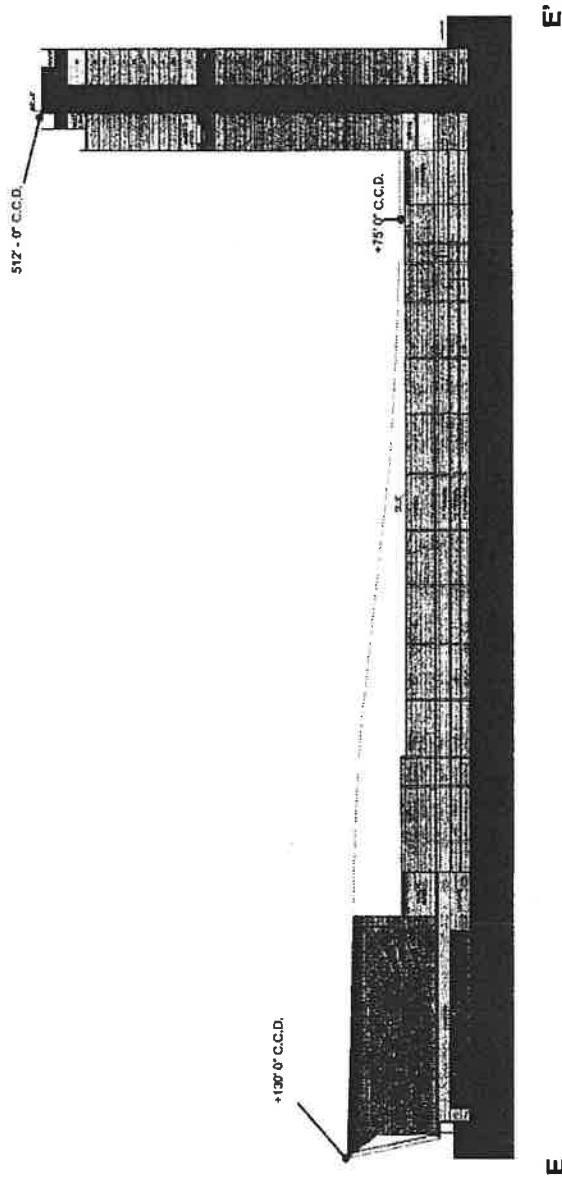
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Address: **643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.**

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KEYPLAN



EE NORTH / SOUTH BUILDING SECTION

PHASE ONE BUILDING SECTION - 1 (OF 2)

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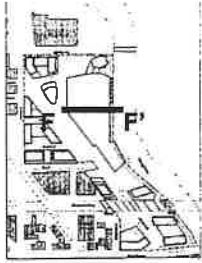
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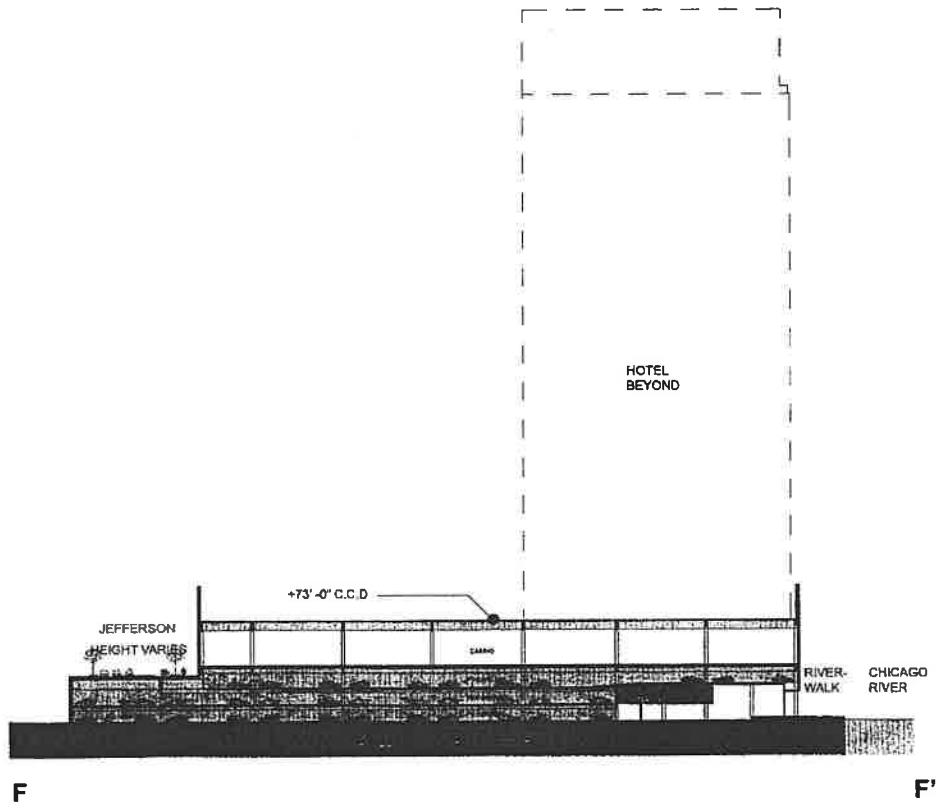


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KEYPLAN



FF EAST / WEST BUILDING SECTION

PHASE ONE BUILDING SECTION - 2 (OF 2)

0' 125' 250'

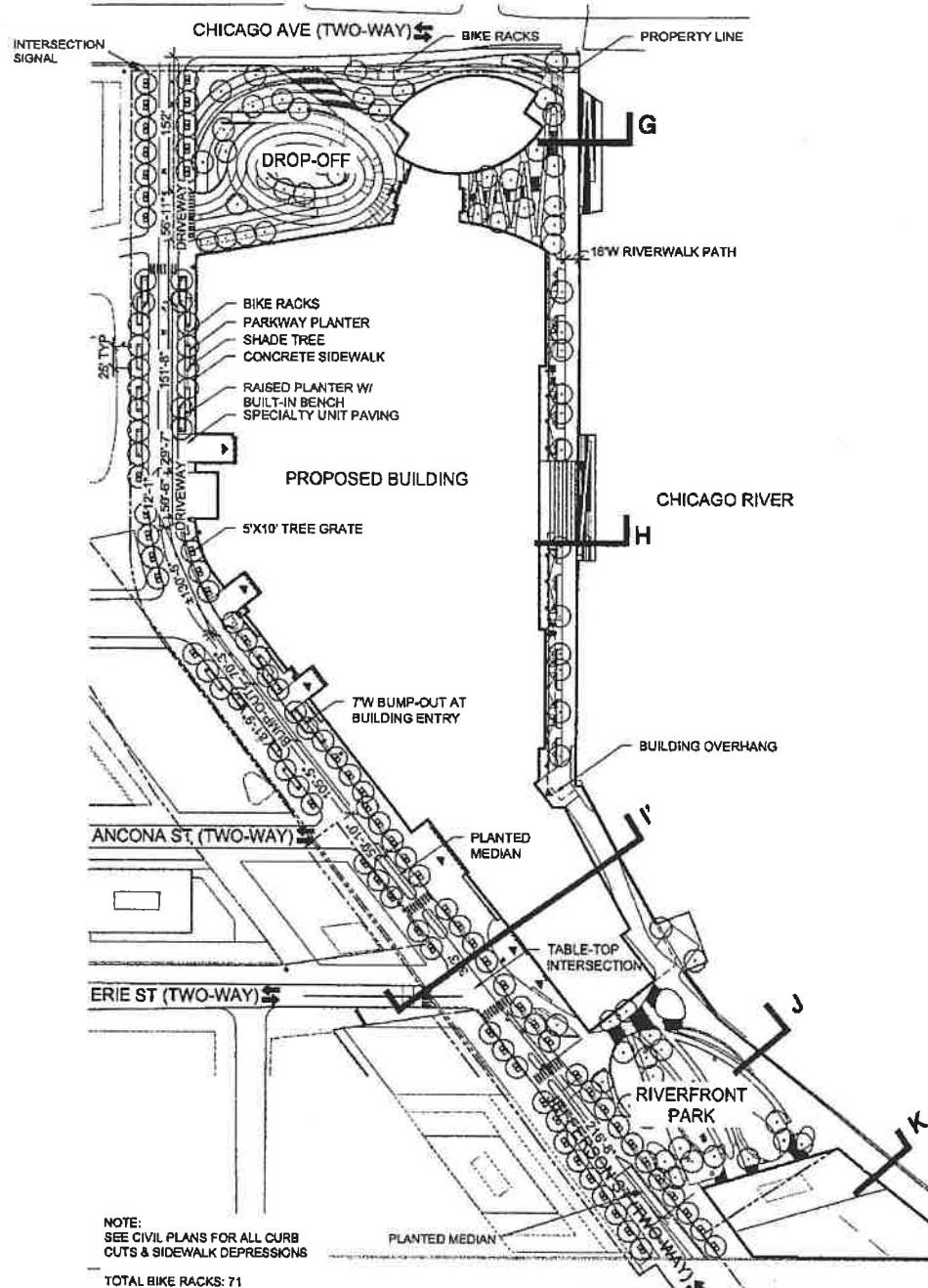


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Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: September 21, 2022
 Plan Commission: December 12, 2022

FINAL FOR PUBLICATION



PHASE 1 PROPOSED LANDSCAPE PLAN

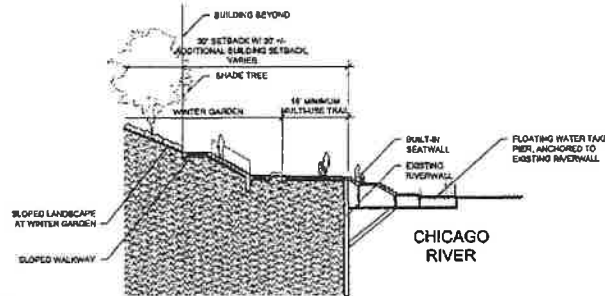
Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 601-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: September 21, 2022
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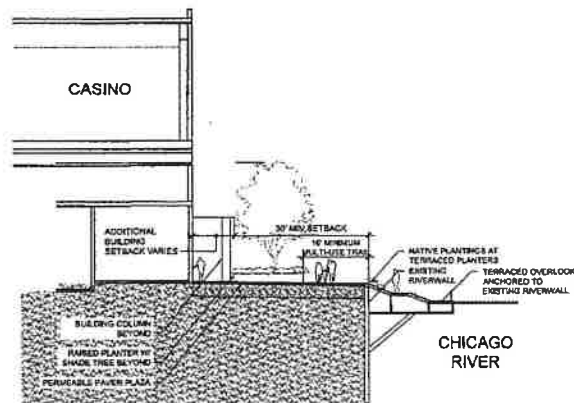
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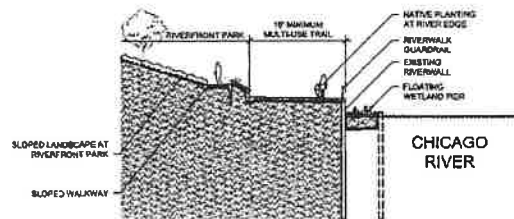
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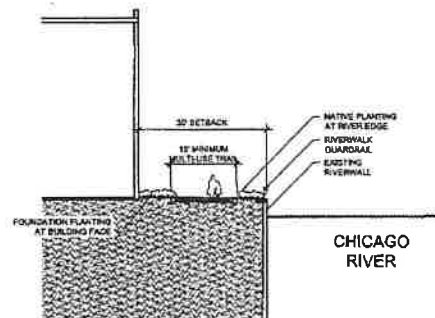
SECTION G



SECTION H



SECTION J



SECTION K

PHASE 1 RIVERWALK SITE SECTIONS

0' 125' 250'



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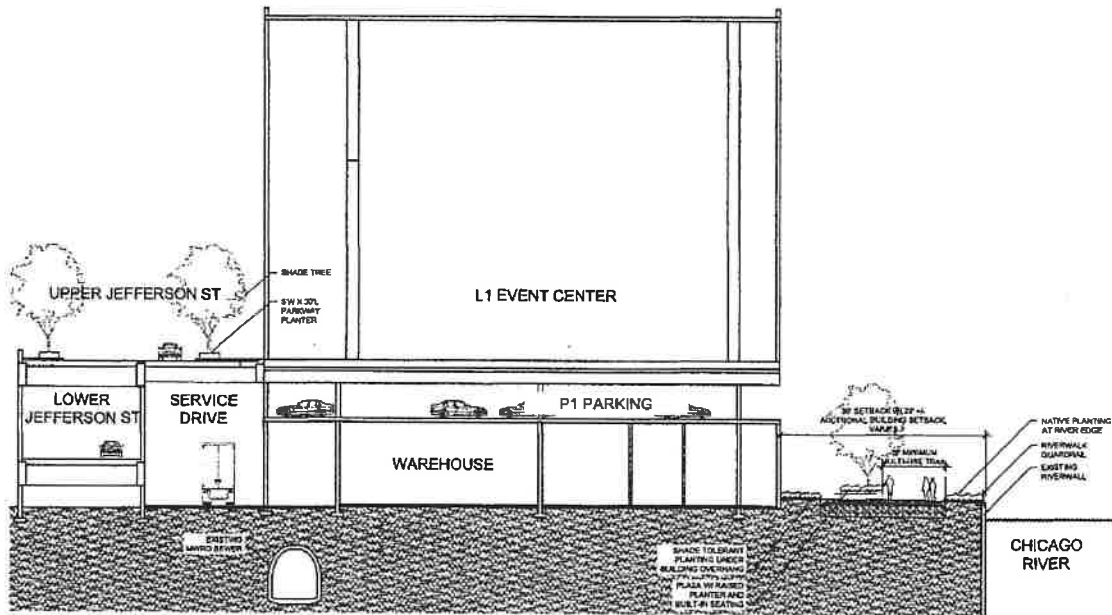
Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
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Introduced: September 21, 2022
Plan Commission: December 12, 2022

FINAL FOR PUBLICATION



KEYPLAN



SECTION I-I'

PHASE 1: RIVERWALK SITE SECTION I-I'

0' 125' 250'

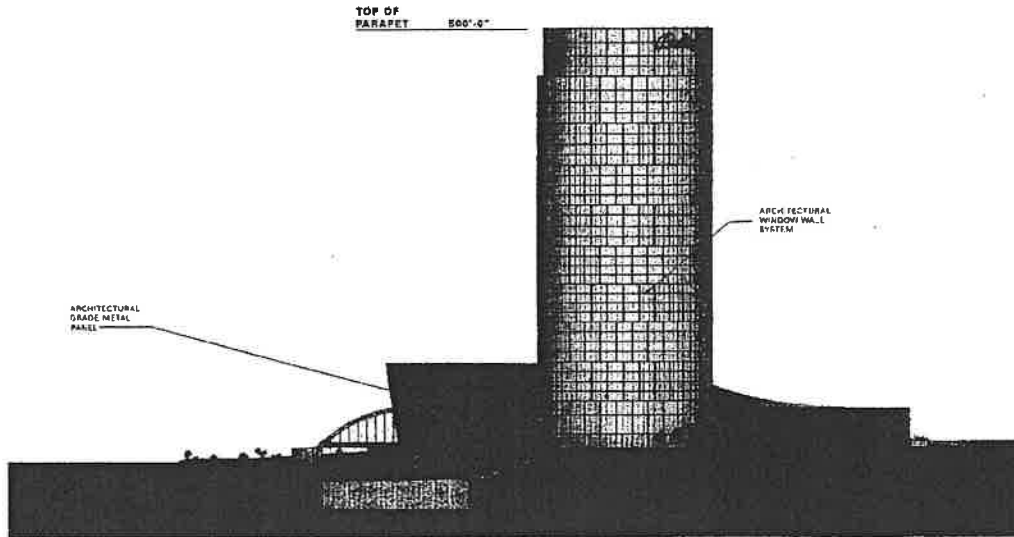


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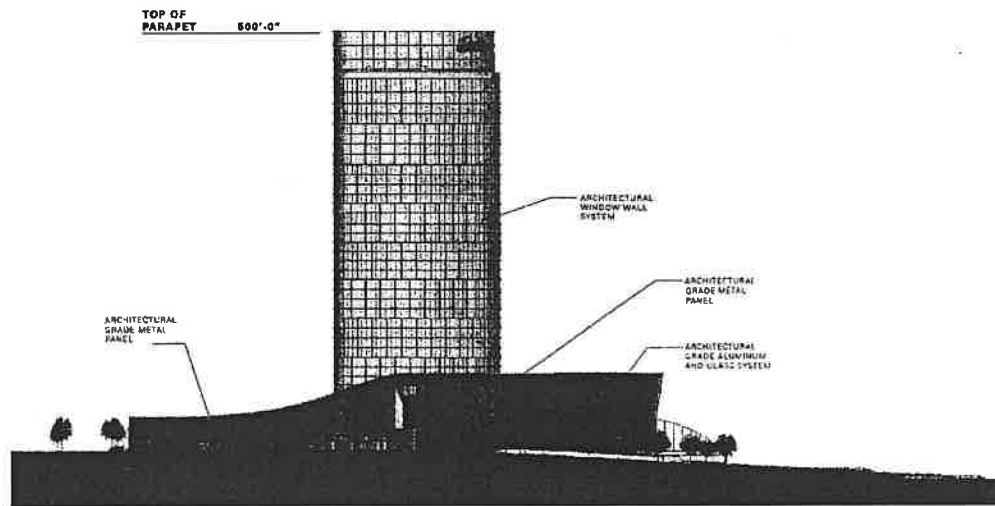
Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
 Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: September 21, 2022
 Plan Commission: December 12, 2022

FINAL FOR PUBLICATION



NORTH ELEVATION



SOUTH ELEVATION

PHASE ONE BUILDING ELEVATIONS - NORTH / SOUTH

0' 125' 250'

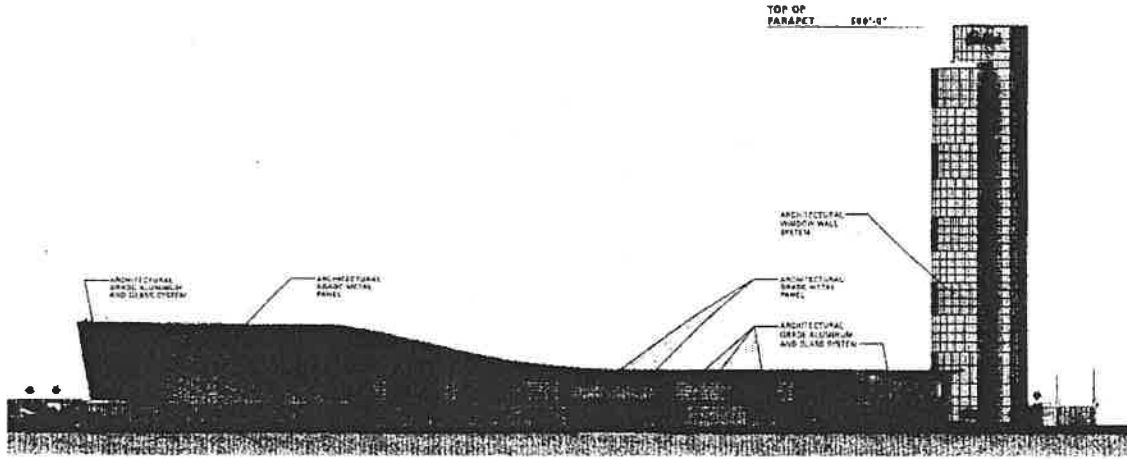


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SCB

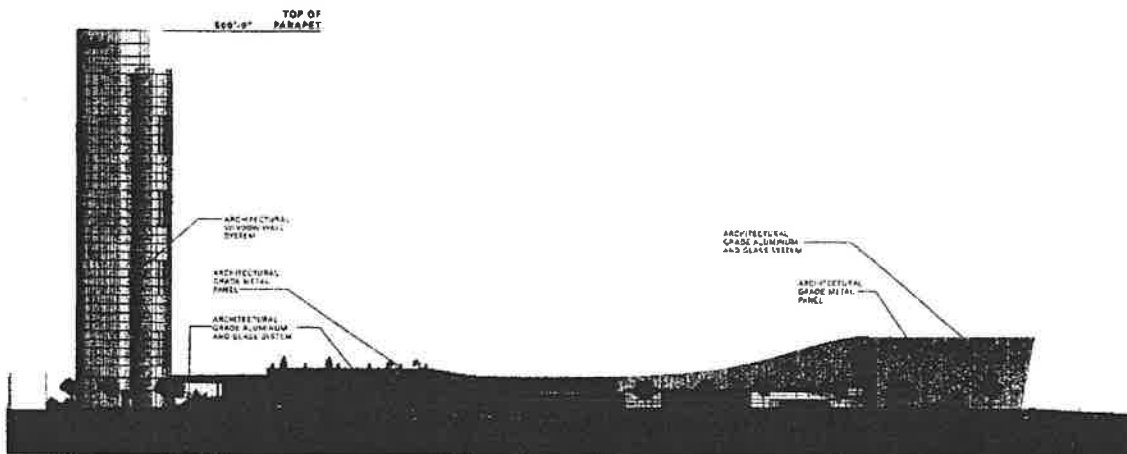
Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: September 21, 2022
Plan Commission: December 12, 2022

FINAL FOR PUBLICATION



EAST ELEVATION



WEST ELEVATION

PHASE ONE BUILDING ELEVATIONS - EAST / WEST

0' 125' 250'



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Applicant: BALLY'S CHICAGO OPERATING COMPANY, LLC
Address: 643-741 W. CHICAGO AVE.; 641-739 N. HALSTED ST.; 632-740 W. ERIE ST.; 627-661 W. ERIE ST.; 501-531 N. DESPLAINES ST.; & 524-630 W. GRAND AVE.

Introduced: September 21, 2022
Plan Commission: December 12, 2022

Reclassification Of Area Shown On Map No. 1-F.
(As Amended)

WBRPD 1426

(Application No. 19405)

(Common Address: 643 -- 741 W. Chicago Ave., 641 -- 739 N. Halsted St.,
632 -- 740 W. Erie St., 627 -- 661 W. Erie St., 501 -- 531 N.
Desplaines St. And 524 -- 630 W. Grand Ave.)

[SO2017-7058]

Be It Ordained by the City Council of the City of Chicago:

SECTION 1. That the Chicago Zoning Ordinance be amended by changing all of the DS-5 Downtown Service District symbols and indications as shown on Map Number 1-F in the area bounded by:

beginning at West Chicago Avenue; the north branch of the Chicago River; a line 74.5 feet north of the north line of West Ohio Street extended; a line 451 feet east of the centerline of North Desplaines Street extended; a line 67.50 feet north of the north line of West Ohio Street extended; a line from a point 67.50 feet north of the north line of West Ohio Street extended and 411.96 feet east of the centerline of North Desplaines Street to a point, 507.03 feet east of the centerline of North Desplaines Street and 13.0 feet south of the south line of West Ohio Street extended; a line 13.0 feet south of the south line of West Ohio Street extended; the north branch of the Chicago River; West Grand Avenue; North Desplaines Street; the westerly right-of-way line of the Chicago and Northwestern Railway; the north line of West Ohio Street; a line from a point 60.57 feet west of the westerly right-of-way line of the Chicago and Northwestern Railway along the north line of West Ohio Street to a point and from a point 55.79 feet west of the westerly right-of-way line of the Chicago and Northwestern Railway along a line 67.50 feet north of the north line of West Ohio Street to a point (said line runs diagonally to the northwest for a distance of 84.92 feet); a line 67.50 feet north of the north line of West Ohio Street; the westerly right-of-way line of the Chicago and Northwestern Railway, the south right-of-way line of relocated West Erie Street; the westerly right-of-way line of the Chicago and Northwestern Railway; the northerly right-of-way line of relocated West Erie Street; North Union Avenue; and North Halsted Street (ToB),

to those of a DX-5 Downtown Mixed-Use District.

SECTION 2. That the Chicago Zoning Ordinance be amended by changing all of the DX-5 Downtown Mixed-Use District symbols and indications as shown on Map Number 1-F in the area bounded by:

beginning at West Chicago Avenue; the north branch of the Chicago River; a line 74.5 feet north of the north line of West Ohio Street extended; a line 451 feet east of the centerline of North Desplaines Street extended; a line 67.50 feet north of the north line of West Ohio Street extended; a line from a point 67.50 feet north of the north line of West Ohio Street extended and 411.96 feet east of the centerline of North Desplaines Street to a point, 507.03 feet east of the centerline of North Desplaines Street and 13.0 feet south of the south line of West Ohio Street extended; a line 13.0 feet south of the south line of West Ohio Street extended; the north branch of the Chicago River; West Grand Avenue; North Desplaines Street; the westerly right-of-way line of the Chicago and Northwestern Railway; the north line of West Ohio Street; a line from a point 60.57 feet west of the westerly right-of-way line of the Chicago and Northwestern Railway along the north line of West Ohio Street to a point and from a point 55.79 feet west of the westerly right-of-way line of the Chicago and Northwestern Railway along a line 67.50 feet north of the north line of West Ohio Street to a point (said line runs diagonally to the northwest for a distance of 84.92 feet); a line 67.50 feet north of the north line of West Ohio Street; the westerly right-of-way line of the Chicago and Northwestern Railway; the south right-of-way line of relocated West Erie Street; the westerly right-of-way line of the Chicago and Northwestern Railway; the northerly right-of-way line of relocated West Erie Street; North Union Avenue; and North Halsted Street (ToB)

to those of an Air Rights Waterway Business-Residential Planned Development which is hereby established in the area above described, subject to such Use and Bulk Regulations as set forth in the Plan of Development herewith attached and made a part hereof and to no others.

SECTION 3. This ordinance shall be in full force and effect from and after its passage and due publication.

Plan of Development Statements referred to in this ordinance read as follows:

AIR RIGHTS WATERWAY BUSINESS RESIDENTIAL PLANNED DEVELOPMENT NO. 1426PLANNED DEVELOPMENT STATEMENTS

1. The area delineated herein as Air Rights Waterway Business Residential Planned Development Number 1426, ("Planned Development") consists of approximately 1,115,091 net square feet of property (25.60 acres) which is depicted on the attached Planned Development Boundary and Property Line Map ("Property") and is owned and controlled by the Applicant, IL-777 West Chicago Avenue, LLC (the "Applicant").
2. The requirements, obligations and conditions contained within this Planned Development shall be binding upon the Applicant, its successors and assigns and, if different than the Applicant, the legal title holders and any ground lessors. All rights granted hereunder to the Applicant shall inure to the benefit of the Applicant's successors and assigns and, if different than the Applicant, the legal title holder and any ground lessors. Furthermore, pursuant to the requirements of Section 17-8-0400 of the Chicago Zoning Ordinance, the Property, at the time of application for amendments, modifications or changes (administrative, legislative or otherwise) to this Planned Development are made, shall be under single ownership or designated control. Single designated control is defined in Section 17-8-0400 of the Zoning Ordinance, provided, however, (a) that for so long as the Applicant or any affiliate thereof owns or controls any part of the Property, any application to the City for any such changes or modifications (administrative, legislative or otherwise) must in all cases be additionally authorized by the Applicant and (b) such change or modification (administrative, legislative or otherwise) shall comply with the terms of any zoning rights agreement, deed restriction, or other written agreement by the Applicant or between owners or designated controlling parties of the Property or portions thereof. Where portions of the improvements located on the Property have been submitted to the Illinois Condominium Property Act, the term "owner" shall be deemed to refer solely to the condominium association of the owners of such portions of the improvements and not to the individual unit owners therein. Nothing herein shall prohibit or in any way restrict the alienation, sale or any other transfer of all or any portion of the Property or any rights, interests or obligations therein including any ground or air-rights leases. Upon any alienation, sale or any other transfer of all or any portion of the Property or the rights therein including any ground or air-rights leases (but not including an assignment or transfer of rights pursuant to a mortgage or otherwise as collateral for any indebtedness) and solely with respect to the portion of the Property so transferred, the term "Applicant" shall be deemed amended to apply to the transferee thereof (and its beneficiaries if such transferee is a land trust) and the seller or transferor thereof (and its beneficiaries if such seller or transferor is a land trust) shall thereafter be released from any and all obligations or liability hereunder; provided, however that the Applicant's right to authorize changes or modifications to this Planned Development for so long as it owns or controls all or any portion of the Property as set forth in clause (a) of this Statement Number 2 above shall not be deemed amended or transferred to apply to a

transferee (or its beneficiaries as aforesaid) unless expressly assigned in a written instrument executed by the original Applicant hereunder.

3. All applicable official reviews, approvals or permits are required to be obtained by the Applicant or its successors, assignees or grantees. Any dedication or vacation of streets or alleys or grants of easements or any adjustment of the right-of-way shall require a separate submittal to the Department of Transportation on behalf of the Applicant or its successors, assign or grantees.

Any requests for grants of privilege, or any items encroaching on the public way, shall be in compliance with the Planned Development. Ingress or egress shall be pursuant to the Planned Development and may be subject to the review and approval of the Departments of Planning and Development and Transportation. Closure of all or any public street or alley during demolition or construction shall be subject to the review and approval of the Department of Transportation.

All work proposed in the public way must be designed and constructed in accordance with the Department of Transportation Construction Standards for Work in the Public Way and in compliance with the Municipal Code of the City of Chicago. Prior to the issuance of any Part II approval, the submitted plans must be approved by the Department of Transportation. Pursuant to a negotiated and executed Perimeter Restoration Agreement ("Agreement") by and between the Department of Transportation's Division of Infrastructure Management and the Applicant, the Applicant shall provide improvements and restoration of all public way adjacent to the property, which may include, but not be limited to, the following as shall be reviewed and determined by the Department of Transportation's Division of Infrastructure Management:

- Full width of streets
- Full width of alleys
- Curb and gutter
- Pavement markings
- Sidewalks
- ADA crosswalk ramps
- Parkway & landscaping

The Perimeter Restoration Agreement must be must be executed prior to any Department of Transportation and Planned Development Part II review permitting. The Agreement shall reflect that all work must comply with current Rules and Regulations and must be designed and constructed in accordance with the Department of Transportation's Construction Standards for work in the Public Way and in compliance with the Municipal Code of Chicago Chapter 10-20. Design of said improvements should follow the

Department of Transportation's Rules and Regulations for Construction in the Public Way as well as The Street and Site Plan Design Guidelines. Any variation in scope or design of public way improvements and restoration must be approved by the Department of Transportation.

- The Applicant commits to provide and fully-fund the following Phase 1a infrastructure improvements during the course of the Phase 1a development and in accordance with Statement 8F below. All infrastructure improvements will be subject to review and approval by the Department of Transportation and must be designed and constructed in accordance with the Department of Transportation Construction Standards for Work in the Public Way and in compliance with the Municipal Code of the City of Chicago.
 - Installation of a new traffic signal at the proposed site access driveway (to be known as Jefferson Street) at Grand Avenue.
 - Installation of a new traffic signal at Grand Avenue and Desplaines Street
 - Installation of a new traffic signal at Halsted Street and Ohio Street, should Ohio Street be converted to two-way traffic flow between Halsted Street and Desplaines Street by the development team in coordination with the alderman's office.
 - Installation of countdown pedestrian signals at the intersection of Grand Avenue and Orleans Street.
 - Replacement of the current traffic signal controller with an ATC 1000 controller at the following intersections:
 - Grand Avenue/Milwaukee Avenue/Halsted Street
 - Grand Avenue and Kingsbury Street
 - Grand Avenue and Orleans Street
 - Dedication of all public right of way for roadways throughout the site must be planned for each phase and completed in accordance with the Development Phasing Diagram. Unless otherwise indicated on the Right-of-Way Adjustment Map, private roadways will not be acceptable for a development of this size. All application fees and the construction of the public infrastructure to be dedicated are the responsibility of the Applicant. Furthermore transit way planning and implementation will continue throughout the life of the project and coordination between the Applicant and CDOT will be necessary.
4. This Plan of Development consists of 22 Statements: a Bulk Regulations Table; an Existing Zoning Map; an Existing Land Use Map; a General Land-Use Plan; a Planned Development Boundary and Property Line Map; a Right-of-Way Adjustment Map; a Subarea and Parcelization Plan; Development Phasing Diagram; a Site Access and Loading Plan (Levels 1 and 2); Site Level Plans (Levels 1, 2 and 3); a Public Open Space Plan; a Generalized Landscape Plan and Plant List; a Site Section Plan; a Phase 1a Riverwalk Section Plan; Bike and Pedestrian Connectivity Plan; Transit Way

Accommodation Plan; Design Guidelines; Phase 1a Parcel A and B Site and Landscape Plan; Phase 1a Parcel A Ground Floor Plan; Phase 1a Parcel A Floor Plans; Phase 1a Parcel A Building Elevations; Phase 1a Parcel B Ground Floor Plan; Phase 1a Parcel B Floor Plans; Phase 1a Parcel B Building Elevations all prepared by Solomon Cordwell Buenz Architects dated October 18, 2018, submitted herein. In any instance where a provision of this Planned Development conflicts with the Chicago Building Code, the Building Code shall control. This Planned Development conforms to the intent and purpose of the Zoning Ordinance, and all requirements thereto, and satisfies the established criteria for approval as a Planned Development. In case of a conflict between the terms of this Planned Development Ordinance and the Zoning Ordinance, this Planned Development Ordinance shall control.

5. The following uses are allowed in the Planned Development delineated herein:

Subarea A: Multi-Unit residential, dwelling units located both on and above the ground floor, Public and Civic uses, Commercial uses, warehousing, freight and storage uses as permitted in the DX-Downtown Mixed Use District, accessory parking and related accessory uses.

Subarea B and C: Residential uses including dwelling units located both on and above the ground floor, Public and Civic uses, and Commercial uses, warehousing, freight and storage uses, printing facility, as permitted in the DX Downtown Mixed Use District, with accessory parking and related accessory uses.

The following uses shall be prohibited within the Planned Development: recycling facilities, drive-through facilities, inter-track wagering facility, payday/title secured loan store, pawn shop, gas station and free standing wireless tower. In addition, the following uses shall be permitted in all subareas subject to the review and approval of the Department of Planning and Development:

- a. other than as prohibited above, broadcast and telecommunication structures, equipment and installations including parabolic transmitting and receiving antennae;
- b. public utility and public service uses necessary to serve the development including, but not limited to, district electrical generation and utility substations; and
- c. district cooling and heating.
- d. Day Care

6. On-Premise signs and temporary signs, such as business, construction and marketing signs, shall be permitted within the Planned Development, subject to the review and approval of the Department of Planning and Development. Off-Premise signs are prohibited within the boundary of the Planned Development.
7. For purposes of height measurement, the definitions in the Zoning Ordinance shall apply. The height of any building shall also be subject to height limitations, if any, established by the Federal Aviation Administration. Except as provided herein, the maximum height of any building within this Planned Development shall not exceed 650 feet.
8. The maximum permitted Floor Area Ratio ("FAR") for the Property and each subarea shall be in accordance with the attached Bulk Regulations Table. For the purposes of FAR calculations and measurements, the definitions in the Zoning Ordinance shall apply. The permitted FAR identified in the Bulk Regulations Table has been determined using a net site area of 1,115,091 square feet and a base FAR of 5.0.

The Applicant acknowledges that the project has received a bonus FAR of 2.6 pursuant to Sec. 17-4-1000 of the Zoning Ordinance. With this bonus FAR, the total FAR for the Planned Development is 7.6. In exchange for the bonus FAR, the Applicant is required to make a corresponding payment, pursuant to Sections 17.4-1003-B & C, prior to the issuance of the first building permit for any building in the Planned Development; provided, however, if the Planned Development is constructed in phases, the bonus payment may be paid on a pro rata basis as the first building permit for each subsequent new building or phase of construction is issued.

The improvements to be constructed on the Property will be subject to the following Neighborhood Opportunity Fund floor area bonus criteria; otherwise more specifically described in Sections 16-14-010, 17-4-1000 and other referenced portions of the Municipal Code of Chicago. The bonus payment will be split between three separate funds, as follows: 80% to the Neighborhoods Opportunity Fund, 10% to the Citywide Adopt-a-Landmark Fund and 10% to the Local Impact Fund, pursuant to Section 17-4-1003-D. Such funds will be utilized pursuant to Section 17-4-1004-B (Neighborhoods Opportunity), Section 17-4-1006-C (Citywide Adopt-a-Landmark) and Section 17-4-1005-C (Local Impact). The bonus payment will be recalculated at the time of payment (including partial payments for phased developments) and may be adjusted based on changes in median land values in accordance with Section 17-4-1003-C.3. Provided, however, the Commissioner of the Department of Planning and Development shall, at the request of the Applicant, be permitted to reduce the amount of the bonus payment for any parcel that does not seek an FAR above 5.0.

Funds that become available through the Neighborhood Opportunity Local Impact Fund may be used by CDOT for the design and construction of traffic signal improvements at Ogden Avenue and the northbound Kennedy Expressway on-ramp.

A. Portions of subareas B and C will be constructed along an elevated platform between + 18 and + 33 feet Chicago City Datum, but may be lower or higher depending on factors such as road levels ("Level 2"). Unless devoted to a principal use as defined in the Zoning Ordinance, floor area below the Level 2 shall be considered below grade and shall not be included in calculating the total number of square feet of development within this Planned Development.

B. Bulk and Density Regulations

1. Warehousing and storage uses permitted as principal uses in Statement Number 5 shall be limited to eight hundred thousand (800,000) square feet and shall be established in accordance with the DX-5 Downtown Mixed-Use District regulations existing on the effective date of this Planned Development.
2. With regard to areas devoted as a principal use to office, hotel, residential and retail sales and service uses, the following maximums shall apply;

Office, maximum floor area =	4,566,522 square feet
Hotel, maximum number of rooms/keys =	250
Residential, maximum number of dwelling units =	4,099 units
Retail sales and service, maximum floor area =	169,902 square feet

Provided, however, that in calculating these maximums, ballrooms, meeting rooms, exhibition space and eating facilities associated with a hotel use and located at or above the established Level 2 shall be chargeable against the maximum permitted floor area for principal retail sales and service of office uses.

3. The Applicant may increase the maximum number of dwelling units allowed within this planned development by converting a portion of the maximum allowed office space and/or a portion of the maximum allowed hotel rooms/keys. The Applicant may increase the maximum amount of office space by converting a portion of the maximum dwelling units and/or a portion of the maximum number of hotel rooms/keys. The Applicant may increase the maximum number of hotel rooms/keys to six hundred and fifty (650) rooms/keys by converting a portion of the maximum allowed office space and/or a portion of the maximum allowed

dwelling units. The Applicant may increase the maximum amount of retail space by converting a portion of the maximum allowed office space and/or portion of the maximum allowed dwelling units. At no point can the overall square footage devoted to residential development exceed 50% of the total allowable buildable square footage within this planned development.

Conversion Chart:

1. Eight hundred and fifty (850) square feet of office or retail space shall be equal to one (1) dwelling unit.
2. Two (2) Hotel rooms/keys shall be equal to one (1) dwelling unit.
3. Two (2) Hotel rooms/keys shall be equal to (850) square feet of office space.
4. One (1) square foot of office space shall equal one (1) square foot of retail space.

4. The number of efficiency units within this Planned Development shall not exceed thirty percent (30%) of the number of permitted dwelling units.
5. To the extent this Planned Development does not cover all items required for development, all development shall be in general conformity with the DX-5 Downtown Mixed-Use District regulations of the Chicago Zoning Ordinance.

C. Periphery Setbacks and Minimum Distance Between Buildings

1. Setbacks shall be in conformance with the site plans and Design Guidelines.
2. Periphery setback and distance provisions may be adjusted where required to allow flexibility of architectural or site design arrangement, subject to the approval of the Department of Planning and Development.

D. Parking

1. Minimum Requirements for uses:

Office: 0.3 spaces per one thousand (1,000) square feet.

Hotel: with more than 15,000 square feet of meeting, conference or banquet area: 1 space per 1,000 square feet.

Residential: 0.35 parking spaces shall be required per dwelling unit including efficiency units

Retail: 0.5 parking space per one thousand (1,000) square feet.

2. Location. All parking spaces required to serve buildings or uses shall be located on the same parcel as the building or use served, or (a) if a residential use, within six hundred (600) feet walking distance measured from the property line; or (b) if office use, within one thousand (1,000) feet walking distance measured from the property line.
 3. Vehicular entrances and exits to accessory automobile parking areas shall be located in general conformance with the Site Access and Loading Plans attached hereto. Provided, however, that temporary or relocated driveways shall be permitted within the Planned Development subject to the review and approval of the Department of Transportation and the Department of Planning and Development in accordance with Statement 13 below.
 4. Transit Served Locations. Any parcel within this Planned Development that qualifies as a transit served location as defined under Section 17-10-0102-B of the Zoning Ordinance may reduce the parking requirements for such parcel by the maximum amounts permitted under Section 17-10-0102-B and approval by DPD. As part of the review of such request to reduce the parking requirement for such parcel, the applicant must provide an exhibit identifying the parcel and its proximity to Public Transit. No approval can be granted without the submission of such document.
- E. Loading. Minimum off-street loading shall be provided in accordance with the regulations applicable in the DX-5 Downtown Mixed-Use District of the Chicago Zoning Ordinance existing on the effective date hereof. The location of loading berths shall be subject to the review of the Department of Transportation and the approval of the Department of Planning and Development.
- F. Construction of Public Roadway and Traffic Improvements. No certificate of occupancy shall be issued for any improvement located within a parcel in this Planned Development until such time as the Applicant for the certificate produces evidence that construction of the public roadway and traffic improvements (an improvement available for use by the public) related to the improvement within such parcel has been completed, is under construction or is under contract for construction, or that adequate access can be provided, all as certified by the Department of Transportation and approved by the Department of Planning and Development.
9. The Applicant acknowledges and agrees that the rezoning of the Property from the DS-5 Downtown Service District to the DX-5 Downtown Mixed Use District, and then to this Air Rights Waterway Business Residential Planned Development (PD), for construction of the Planned Development triggers the requirements of Section 2-45-115 of the

Municipal Code adopted in 2015 (the "Affordable Requirements Ordinance" or the "ARO"). The Applicant further acknowledges and agrees that the Property is currently located in the Near North/Near West Pilot Area, pursuant to Section 2-45-117 of the Municipal Code (the "Near North/Near West ARO Pilot Area Ordinance" or the "Pilot"). The Near North/Near West Pilot Area is divided into two zones: the Near North Zone and the Near West Zone. The Property is located in the Near North Zone. In the Near North Zone, the percentage of units in a residential housing project required to be affordable for a period of 30 years, whether rental or for-sale, is increased from 10% to 20% (the "ARO Units"). Any developer of a residential housing project in the Near North Zone, must provide the first 10% of units required to be affordable (the "First Units") either: (i) in the residential housing project, or (ii) with the approval of the commissioner of the Department of Planning and Development or any successor department thereto (the "Commissioner"), in an off-site location within two miles of the Property and in the same or a different higher income area or downtown district, or (iii) any combination of (i) and (ii). In addition, the developer must provide the second 10% of units required to be affordable (the "Additional Units") either: (i) in the residential housing project, or (ii) with the Commissioner's approval, in an off-site location anywhere within the Near North/Near West Pilot Area, regardless of distance from the residential housing project and regardless of the designation of the income area, or (iii) any combination of (i) and (ii). If the residential housing project receives financial assistance from the City, however, all ARO Units must be provided on-site. For the purposes of this Planned Development, any ARO units located within the boundaries of this Planned Development shall constitute on-site units. The Applicant proposes to construct the Planned Development in phases and construct all of ARO required affordable units on site. The first phase of the Planned Development (Phase 1a / Parcel A and Parcel B) consists of the construction of 1,250-1,500 residential units on Parcel A (north tower and south tower) and Parcel B (north and south towers). As a result, in the Near North Zone, , the Applicant's affordable housing obligation for Phase 1a is between 250-300 units (20% of 1,250 = 250, and 20% of 1,500 = 300). The Applicant has agreed to satisfy this affordable housing obligation for Phase 1a by providing between 42 - 50 ARO Units in the north tower of Parcel A and between, 42-50 ARO Units in the south tower on Parcel A, and the remaining ARO Units on Parcel B provided that no more than 80% of the housing units on Parcel B may be ARO Units. Developer agrees to use best efforts to lease 10% of the required affordable units to CHA. In other words, at least 20% of the total number of housing units in Parcel B must qualify as market-rate housing. The Applicant acknowledges that Section 2-45-115(U)(7) of the ARO requires affordable units to be constructed, completed, ready for occupancy, and marketed concurrently with or prior to the market rate units in a residential housing project or phase thereof. In order to ensure compliance with this requirement, the Applicant agrees that it must apply for and obtain all necessary building permits for Parcel B prior to or concurrently with the building permits for Parcel A, and shall construct the market-rate units and the ARO Units at a proportionate rate as specified in Section 2-45-115(U)(7). The obligation

survives the pilot. If the Applicant fails to comply with the foregoing requirement to develop Parcels A and B concurrently so as to maintain the proper ratio of affordable to market-rate units, then the Applicant shall be required to comply with the ARO on an individual building basis (i.e., each building must satisfy the ARO requirements on a stand-alone basis as if each building is a separate residential housing project). The Applicant may develop one of the towers on Parcel B with affordable senior housing units, as long as those units are developed in keeping with the ARO Rules online at www.cityofchicago.org/aro. In accordance with the ARO Pilot, the Applicant shall be required to lease the First Units to households earning up to 60% of the Chicago Primary Metropolitan Statistical Area median income ("AMI") at prices affordable to households at such income level, and the Additional Units to households earning up to 80% or 100% of AMI, as determined by rule and approved by the Commissioner at prices affordable to households at such income level, including any units leased to CHA. If the Applicant subsequently reduces (or increases) the number of housing units in the project, or elects to build a for-sale project instead of a rental project, the Applicant shall update and resubmit the Affordable Housing Profile Form to the Department of Planning and Development ("DPD") for review and approval. DPD may adjust the requirements to reflect any such change without amending the Planned Development. Prior to the issuance of any building permits for any residential building in the Planned Development, including, without limitation, excavation or foundation permits, the Applicant must execute and record an affordable housing agreement in accordance with Section 2-45-115(L). The final unit mix and distribution must be approved by DPD prior to the recording of this agreement. The terms of the affordable housing agreement and any amendments thereto are incorporated herein by this reference. The Applicant acknowledges and agrees that the affordable housing agreement will be recorded against the Planned Development, or the applicable portion thereof, and will constitute a lien against such property. The Commissioner may enforce remedies for any breach of this Statement 9, including any breach of any affordable housing agreement, and enter into settlement agreements with respect to any such breach, subject to the approval of the Corporation Counsel, without amending the Planned Development. The Applicant must submit all future proposals to meet the ARO under subsequent phases to the Department of Planning and Development ("DPD") for review and approval.

10. The Applicant acknowledges the importance of the Chicago River as a resource for both commerce and recreation and acknowledges the City's goals of improving the appearance, quality and accessibility of the river, as contained in the waterway planned development guidelines in the Zoning Ordinance (Section 17-8-0912) and the Chicago River Corridor Design Guidelines and Standards or as amended. The Applicant also acknowledges the river development Design Guidelines of the North Branch Framework Plan that builds upon the North Branch Industrial Corridor's unique natural and built environment. To further these goals, the Applicant agrees, as set forth in the Public Open Space Plans, to:
 - (a) provide an expanded river setback with an average width of between 30ft and 60 ft

with a continuous 16-ft wide multi-purpose riverwalk as shown in the Public Open Space Plan and General Landscape Plan, and where at no point is the river setback less than 30-foot; (b) provide a variety of active uses and river overlooks as indicated on said Plans; (c) permit the connection of such riverwalk under Chicago Avenue and under Grand Avenue to the riverwalk of adjacent properties when adjacent properties are similarly improved; and (d) permit the future placement of a pedestrian bridge along the riverwalk in the general area of the former Erie Street bridge abutment over the Chicago River to connect to Montgomery Ward Park.

The Applicant shall permit un-gated and unobstructed public access to the river setback, and provide informational and wayfinding signage at all entries that the riverwalk is open to the public, free of charge, during normal park hours from 6:00am to 11:00pm every day of the year. All improvements within the river setback for each development parcel must be substantially complete prior to receipt of Certificate of Occupancy for the first principal building located on the east side of Jefferson Street within each parcel as shown on the Development Phasing Diagram. Planting may be delayed, if consistent with good landscape practice, but not longer than one year following receipt of the occupancy certificate.

The Applicant shall also design and construct the central riverfront park and south riverfront park depicted on the Public Open Space Plan (hereinafter the "Park"). The south riverfront park under and adjacent to the Ohio Feeder Ramp includes active recreation features, play areas, and greenspace, which will be completed, under construction or under contract for construction prior to receipt of Certificate of Occupancy for the first principal building within Parcel A and as part of the scope of work in Phase Ia. Provided, however, such timing shall be subject to coordination with and the granting of all necessary rights and permits by the County of Cook which has easement rights for and below the Ohio Feeder Ramp and the Illinois Department of Transportation. The central riverfront park will include activated programming along the east side of Jefferson Street and landscaping to minimize blank terraced walls from Jefferson Street up to the plaza area west of Jefferson Street. The central riverfront park on the east side of Jefferson Street will be completed prior to receipt of Certificate of Occupancy for the first principal building within Parcels H or J as shown on the Development Phasing Diagram. The central riverfront park on the west side of Jefferson Street is split between two parcels, Parcels E and F. For constructability, that portion of the central riverfront park west of Jefferson Street located in Parcels E and F will be completed prior to receipt of Certificate of Occupancy for the first principal building within each parcel Parcel E and F, respectively, as shown on the Development Phasing Diagram. Upon completion of the Park, the Applicant shall be responsible for maintaining and managing the Park for the purposes set forth herein, including ensuring that the Park's landscaping is well maintained, that the vegetation and plantings are kept in healthy condition and that the Park facilities and play areas are clean, well lit, litter free and clear of snow from

hardscaped areas and debris. The Applicant shall provide sufficient liability insurance coverage necessary for the operation of the Park for public use. The Applicant shall provide informational and wayfinding signage at all entries that the Park is open to the public, free of charge, during normal park hours from 6:00am to 11:00pm every day of the year.

Prior to issuance of building permits for the first principal building within each Subarea, the Applicant will enter into a development and maintenance agreement with the City of Chicago for the construction, maintenance, and management of the Park. The development and maintenance agreement obligations shall be binding upon the Applicant, its successors and assigns, including but not limited to a master business or homeowners maintenance association whose purpose includes maintaining the Parks and/or the riverwalk. Upon completion of the Park, the parcel of land developed and devoted to open space shall be memorialized in a public access easement agreement with and for the benefit of the City of Chicago or a not for profit accredited land conservancy trust, as approved by the City, whose purpose is to hold open space public access easements. Any reasonable costs associated with establishing, monitoring, or stewardship of the public access easement shall be the responsibility of the Applicant, and would need to be contributed at the time when the easement is granted.

11. Upon review and determination, "Part II Review", pursuant to Section 17-13-0610 of the Zoning Ordinance, a Part II Review Fee shall be assessed by the Department of Planning and Development. The fee, as determined by staff at the time, is final and binding on the Applicant and must be paid to the Department of Revenue prior to the issuance of any Part II approval.
12. The Site and Landscape Plans for future development parcels and phases for each parcel shall be in substantial conformance with the Landscape Ordinance and any other corresponding regulations and guidelines, including Section 17-13-0800. Final landscape plan review and approval will be by the Department of Planning and Development. Any interim reviews associated with site plan review or Part II reviews, are conditional until final Part II approval.

Prior to the issuance by the Department of Planning and Development of a determination pursuant to Section 7-13-0610 of the Chicago Zoning Ordinance ("Part II approval") for development or redevelopment of any development parcels within the Planned Development, including revised development plans for Parcels A and B, other than alterations to existing buildings which, unless otherwise permitted by the Department of Planning and Development, do not increase their height or alter their footprints, a site plan for the proposed development, including parking areas and related information ("Site Plan Submittal"), shall be submitted to the Commissioner of the Department of Planning and Development for approval. Review and approval of the Site Plan Submittal

by the Commissioner is intended to assure that specific development proposals conform with the Planned Development, are consistent with the intent of the Design Guidelines and to ensure coordination of any public improvements required by this Planned Development.

No Part II approval for work for which a Site Plan Submittal is required hereunder shall be granted until the Site Plan Submittal has been approved by the Commissioner and reviewed by the Chicago Plan Commission at a Public hearing. The hearing conducted by the Plan Commission shall be for review purposes only and no approval or recommendation shall be required in order for the Commissioner to issue an approval of the Site Plan Submittal. Further, all Part II submittals shall be in compliance with the Chicago Landscape Ordinance. Provided the Site Plan Submittal required hereunder is in general conformance with this Planned Development and the Design Guidelines, and provided Applicant has timely provided all Site Plan Submittals, the Commissioner shall issue such site plan approval and the Plan Commission shall conduct its review hearing within days from receipt of the Site Plan Submittal. Following approval of a Site Plan Submittal by the Commissioner, the approved plan shall be kept on permanent file with the Department of Planning and Development and shall be deemed to be an integral part of this Planned Development. The approved Site Plan Submittal may be changed by the provisions of Section 17-13-0611 of the Chicago Zoning Ordinance. In addition, because of the long-term phasing of the Planned Development, design aspects of the Planned Development such as phasing, parcel sizes, building footprints, vehicular access drives and tower locations may be modified during the Site Plan Submittal approval process, but shall still be considered to be in general conformance with this Planned Development provided such modifications do not violate the bulk and density regulations of this Planned Development and are consistent with the Design Guidelines. A Site Plan Submittal shall, at a minimum, provide the following information with respect to the proposed improvements:

- a. the boundaries of the property;
- b. the footprint of the improvements;
- c. location and dimensions of all parking spaces and loading berths;
- d. preliminary landscaping plan prepared by a landscape architect with final landscaping plan to be approved during Part II review;
- e. all pedestrian circulation routes;
- f. the location of any adjacent public improvements;

- g. a signage plan for any building where retail or theater uses would be present above the ground level;
- h. preliminary building sections and elevations of the improvements with a preliminary building materials list; and
- i. statistical information applicable to the property limited to the following:
 - (1) floor area and floor area ratio;
 - (2) uses to be established;
 - (3) building heights;
 - (4) all setbacks, required and provided;
 - (5) floor area devoted to all uses (e.g. office, retail etc.);
 - (6) number of dwelling units;
 - (7) number of parking spaces;
 - (8) number of loading spaces/berths
 - (9) If mutually agreed upon by DPD and the applicant a School Impact Study may be required with the first Site Plan Submittal.

A Site Plan Submittal shall include such other information as may be necessary to illustrate conformance with the applicable provisions of this Planned Development and the Design Guidelines. In addition, as part of the site plan review process for each phase of development, an updated traffic and parking study shall be submitted when it is determined by the Commissioners of the Department of Planning and Development and Department of Transportation that such an updated report is required.

13. In order to encourage architectural diversity and excellence in design, the Applicant will also ensure that each Site Plan Submittal substantially complies with the North Branch Framework and Design Guidelines (NBF) as part of the Part II Review process. Revisions and modifications to any previously approved site plan, landscape plan or building elevations must be substantially consistent with the NBF. Furthermore, Applicant and the Department, at either party's request, may continue to evolve the design of the building elevations on Subarea A (Parcel A and B and C), Subarea B (Parcel C, D, E, F and G) and Subarea C (Parcels H,I,J,K and L); changes to such

elevations, if any, shall, if mutually agreed, be approved by the Department administratively as a Minor Change.

14. The Applicant acknowledges the potential alignments for a multi-modal trail, transit-way and other potential transportation improvements ("Transportation Improvements") through and adjacent to the Property. The Applicant has identified a conceptual location of the Transportation Improvements on the Transit Way Accommodation Plan attached hereto and agrees to provide an easement on, across and through such area of the Property as necessary to accommodate the Transportation Improvements as provided herein. The Applicant and the City of Chicago agree to cooperate on the final location of the Transportation Improvements through the Planned Development site. The Transportation Improvements are meant to improve connectivity and access to the Planned Development site and to other development sites near the Planned Development. The Transportation Improvements shall not adversely affect operations, security of the buildings or diminish the development rights within the Planned Development. In the event that the Transportation Improvements are incorporated into any of the right of ways being dedicated to the public as part of this Planned Development, the Applicant shall be permitted to recapture the square footage of such portion of right of way previously or to be dedicated and increase the net site area of the Planned Development. Any modifications that result from these Transportation Improvements shall be made pursuant to the minor change provisions of Section 17-13-0611.
15. The Applicant shall comply with Rules and Regulations for the Maintenance of Stockpiles promulgated by the Commissioners of the Departments of Streets and Sanitation, Fleet and Facility Management and Buildings, under Section 13-32-085 of the Municipal Code, or any other provision of that Code.
16. The terms and conditions of development under this Planned Development ordinance may be modified administratively, pursuant to section 17-13-0611-A of the Zoning Ordinance by the Zoning Administrator upon the application for such a modification by the Applicant, its successors and assigns and, if different than the Applicant, the legal title holders and any ground lessors. In addition to any other modifications that are otherwise permitted pursuant to Section 17-13-0611-A of the Chicago Zoning Ordinance, an increase in the maximum building height within any subarea by ten percent (10%) or less shall constitute a permitted modification hereunder.
17. The Applicant acknowledges that it is in the public interest to design, construct and maintain the project in a manner which promotes, enables and maximizes universal access throughout the Property. Plans for all buildings and improvements on the Property shall be reviewed and approved by the Mayor's Office for People with Disabilities to ensure compliance with all applicable laws and regulations related to access for persons with disabilities and to promote the highest standard of accessibility.

18. The Applicant acknowledges that it is in the public interest to design, construct, renovate and maintain all buildings in a manner that provides healthier indoor environments, reduces operating costs, conserves energy and maximizes the preservation of natural resources. The Applicant agrees to install a stormwater management system that captures, cleans and discharges the majority of the on-site stormwater (as allowed by City regulations, approved by the Department of Water Management and technically feasible) to the Chicago River. At the time of a hearing before the Chicago Plan Commission, the Applicant agrees to be in compliance with the City of Chicago Sustainable Development Policy set forth by the Department of Planning and Development in effect at the time the Part II review process is initiated for each improvement (phase, subarea or sub parcel) that is subject to the aforementioned Policy and must provide documentation verifying compliance. The method of compliance must include sustainable strategies that are consistent with the North Branch Design Guidelines.

19. The Applicant acknowledges that it is the policy of the City to maximize opportunities for Minority and Women-owned Business Enterprises ("M/WBEs") and city residents to compete for contracts and jobs on construction projects approved through the planned development process. To assist the city in promoting and tracking such M/WBE and city resident participation, an Applicant for planned development approval shall provide information at three points in the city approval process. First, the Applicant must submit to DPD, as part of its application for planned development approval, an M/WBE Participation Proposal. The M/WBE Participation Proposal must identify the Applicant's goals for participation of certified M/WBE firms in the design, engineering and construction of the project, and of city residents in the construction work. The city encourages goals of 26% MBE and 6% WBE participation (measured against the total construction budget for the project or any phase thereof), and (ii) 50% city resident hiring (measured against the total construction work hours for the project or any phase thereof). The M/WBE Participation Proposal must include a description of the Applicant's proposed outreach plan designed to inform M/WBEs and city residents of job and contracting opportunities. Second, at the time of the Applicant's submission for Part II permit review for the project or any phase thereof, the Applicant must submit to DPD (a) updates (if any) to the Applicant's preliminary outreach plan, (b) a description of the Applicant's outreach efforts and evidence of such outreach, including, without limitation, copies of certified letters to M/WBE contractor associations and the ward office of the alderman in which the project is located and receipts thereof; (c) responses to the Applicant's outreach efforts, and (d) updates (if any) to the Applicant's M/WBE and city resident participation goals. Third, prior to issuance of a Certificate of Occupancy for the project or any phase thereof, the Applicant must provide DPD with the actual level of M/WBE and city resident participation in the project or any phase thereof, and evidence of such participation. In addition to the forgoing, DPD may request such additional information as the department determines may be necessary or useful in evaluating the

extent to which M/WBEs and city residents are informed of and utilized in planned development projects. All such information will be provided in a form acceptable to the Zoning Administrator. DPD will report the data it collects regarding projected and actual employment of M/WBEs and city residents in planned development projects twice yearly to the Chicago Plan Commission and annually to the Chicago City Council and the Mayor.

20. Applicant commits to provide and fully-fund the Phase I traffic demand measures and infrastructure improvements, in accordance with and as detailed in Statement 3 above (all such measures and improvements are collectively referred to herein as the "Improvements"). The funding, design and provision of all such Improvements are the responsibility of the Applicant and its successors and assigns, subject to review and installation scheduling by the Department of Transportation. The Applicant must design and construct the Improvements in accordance with the Department of Transportation Construction Standards for work in the Public Way and in compliance with the Municipal Code of Chicago.

The Applicant shall enter into an agreement with the Department of Transportation to implement the provisions of this Statement 20. Such agreement shall be recorded against the Property prior to the issuance of the issuance of Part II approval for any Parcel in Phase I.

21. The Applicant acknowledges that the Property is located in the North Branch Industrial Corridor Conversion Area, and is undergoing a "rezoning" within the meaning of Chapter 16-8 of the Municipal Code (the "Industrial Corridor System Fund Ordinance"). As a result of this rezoning, the Planned Development is subject to the conversion fee provisions of the Industrial Corridor System Fund Ordinance. The purpose of the conversion fee is to mitigate the loss of industrial land and facilities in conversion areas by generating funds for investment in receiving industrial corridors in order to preserve and enhance the City's industrial base, support new and expanding industrial uses, and ensure a stable future for manufacturing and industrial employment in Chicago. For purposes of this Planned Development, the North Branch Industrial Corridor shall be deemed a receiving industrial corridor. The Applicant is required to pay the conversion fee in full prior to the issuance of the first building permit for any building in the Planned Development; provided, however, if the Planned Development is constructed in phases, the conversion fee may be paid on a pro rata basis upon issuance of the building permit for each subsequent new building or phase of construction. The amount of the conversion fee due upon the issuance of a building permit shall be calculated based on the fee rate in effect at the time of payment. The Applicant shall record a notice against the Property to ensure that the requirements of the Industrial Corridor System Fund Ordinance are enforced in accordance with Sec. 16-8-100.

22. This Planned Development shall be governed by Section 17-13-0612 of the Zoning Ordinance. Provided, however, this Planned Development shall not lapse and shall remain in full force and effect provided that the provisions of Section 17-13-0612-A are satisfied for any single development parcel within this Planned Development. Should this Planned Development ordinance lapse, the Zoning Administrator of the Department of Planning and Development shall initiate a Zoning Map Amendment to rezone the property back to the DX-5 Downtown Mixed-Use District.

[Existing Zoning Map; Existing Land-Use Map; General Land-Use Plan; Boundary and Property Line Map; Right-of-Way Adjustment Map; Subarea and Parcelization Plan; Development Phasing Diagram; Levels 1 and 2 Site Access and Loading Plans; Levels 1, Level 2/Podium and Level 3/Tower Site Level Plans; Public Open Space Plan; Generalized and Landscape Plan; Landscape Plant Lists; Site Section Plans; Bike and Pedestrian Connectivity Plan; Transitway Accommodation Plan; Design Guidelines Phase 1A.1 and 1A.2: Site and Landscape Plans (Parcel A and B); Phase 1A: Riverwalk Section Plan (Parcel A); Floor Plans; and Building Elevations referred to in these Plan of Development Statement printed on pages 88111 through 88148 of this *Journal*.]

Bulk Regulations and Data Table referred to in these Plan of Development Statements reads as follows:

19405

Air Rights Waterway-Business
Planned Development Bulk Regulations And Data Table No. 1426

Gross Site Area:	1,671,831	square feet	38.38	acres
Total Area of existing Right-of-Way:	361,253	square feet	8.29	acres
Total Area of Right-of-Way to be dedicated:	195,487	square feet	4.49	acres
Total Net Site Area:	1,115,091	square feet	25.60	acres

Total Maximum FAR:	7.6			
Maximum FAR Buildable Area:	8,474,692	square feet		

Net Site Areas by Sub-Area:				
Sub Area A:	254,092	square feet	5.83	acres
Sub Area B:	452,925	square feet	10.40	acres
Sub Area C:	408,075	square feet	9.37	acres

FAR Building Areas by Sub-Area				
Sub Area A:	1,825,053	square feet		
Sub Area B:	3,262,559	square feet		
Sub Area C:	3,387,080	square feet		

FAR by Sub-Area				
Sub Area A:	7.2			
Sub Area B:	7.2			
Sub Area C:	8.3			

Maximum Building Height by Sub Area				
Sub Area A:	650	feet		
Sub Area B:	600	feet		
Sub Area C:	650	feet		

Maximum Number of Residential Units				
Sub Area A:	1,582	units		
Sub Area B:	305	units		
Sub Area C:	2,212	units		

Maximum Number of Hotel Keys 250

Minimum Accessory Parking Spaces	
Office:	0.3 spaces per one thousand (1,000) square feet.
Hotel:	1 space per 1,000 square feet if more than 15,000 square feet of meeting, conference or banquet area.
Residential:	0.35 parking space shall be required per dwelling unit including efficiency units.
Retail:	0.5 parking space per one thousand (1,000) square feet.

Minimum Number of Bike Parking Spaces	
residential:	1 per 2 auto parking spaces
non-residential:	1 per 10 auto parking spaces

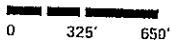
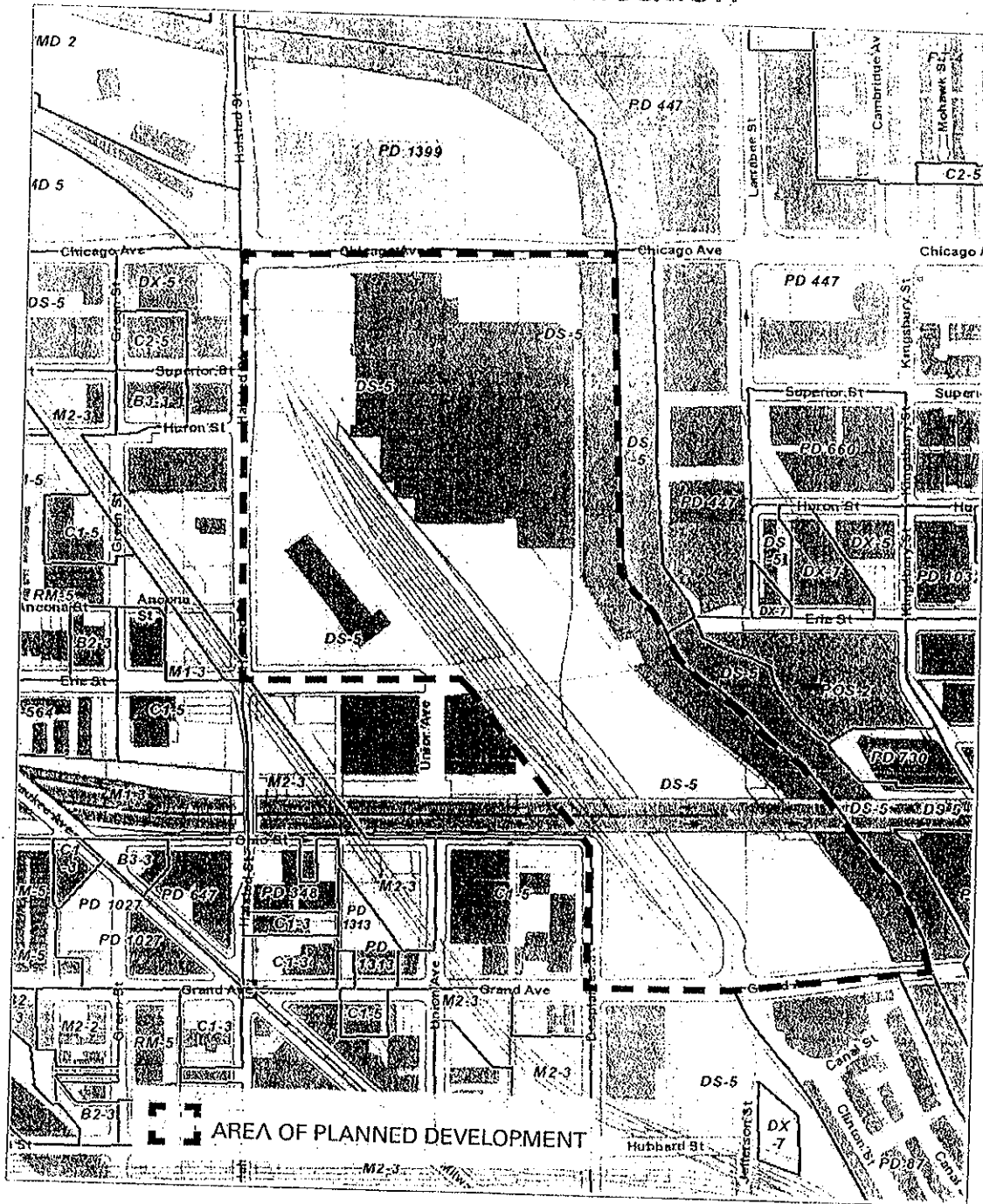
(1) Ordinance Maximums are subject to adjustment per Statement 8B of this Planned Development

(2) In accordance with Section 17-7-0406-C of the Chicago Zoning Ordinance, the base FAR for this Planned Development shall be 5.0, provided, however, the Applicant shall have the right to develop up to a maximum of 7.6 FAR in accordance with this Planned Development. For any development parcel that exceeds 5.0 FAR, the developer shall be required to pay a floor area bonus in accordance with statement 8A of this Planned Development. Individual development parcels may exceed an 7.6 FAR, provided the FAR for the entire property does not exceed 7.6.

(3) Parcel sizes may be modified as provided in Statement 12.

(4) The maximum FAR as allocated to the Sub-Areas may be transferred and shifted among the Sub-Areas pursuant to Section 17-13-0611 provided the overall FAR for the entire property does not exceed 7.6.

INITIAL PLAN APPLICATION



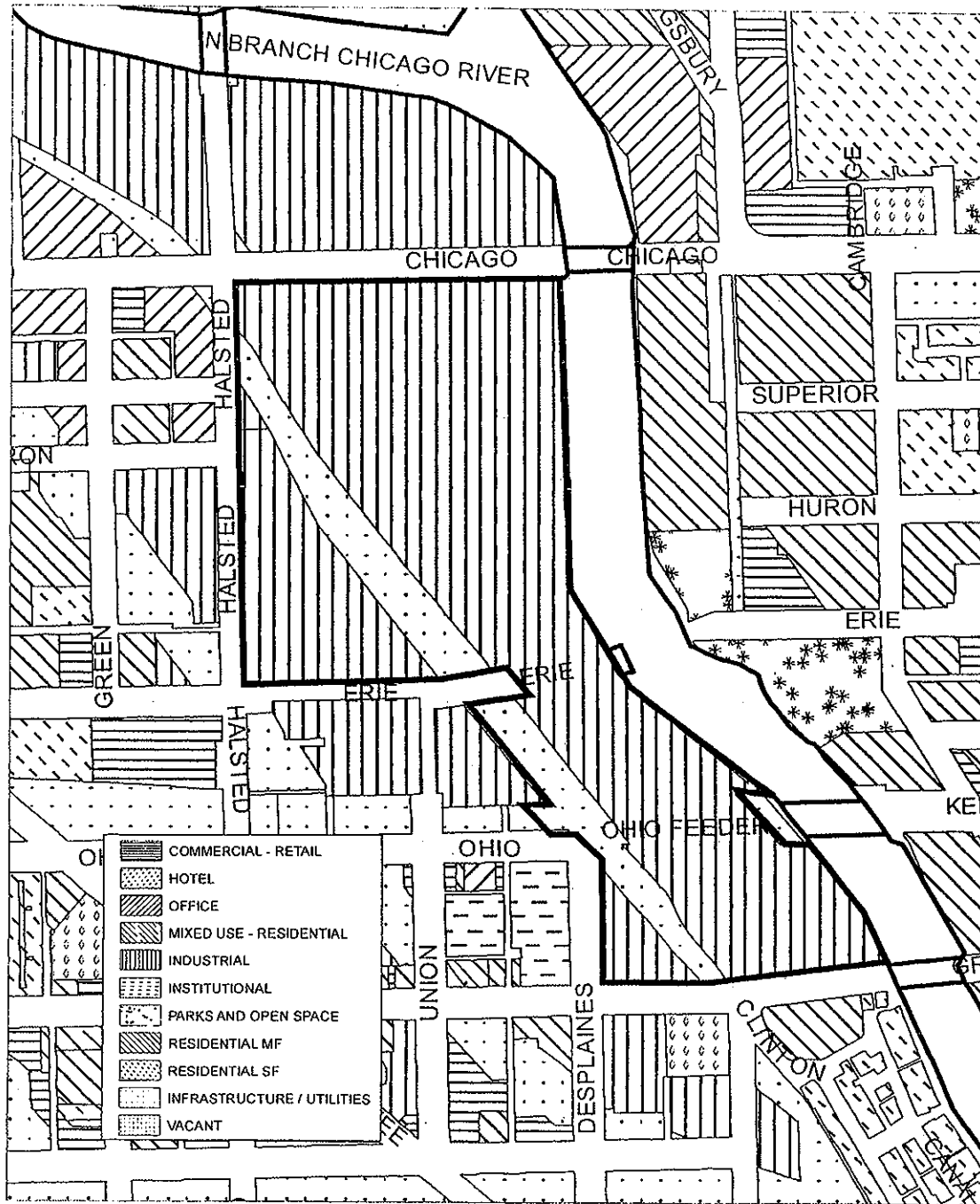
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EXISTING ZONING MAP

Applicant: IL-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 501-31 N. DESPLAINES STREET; 624-640 W. GRAND AVENUE

Project: 777 W CHICAGO AVE

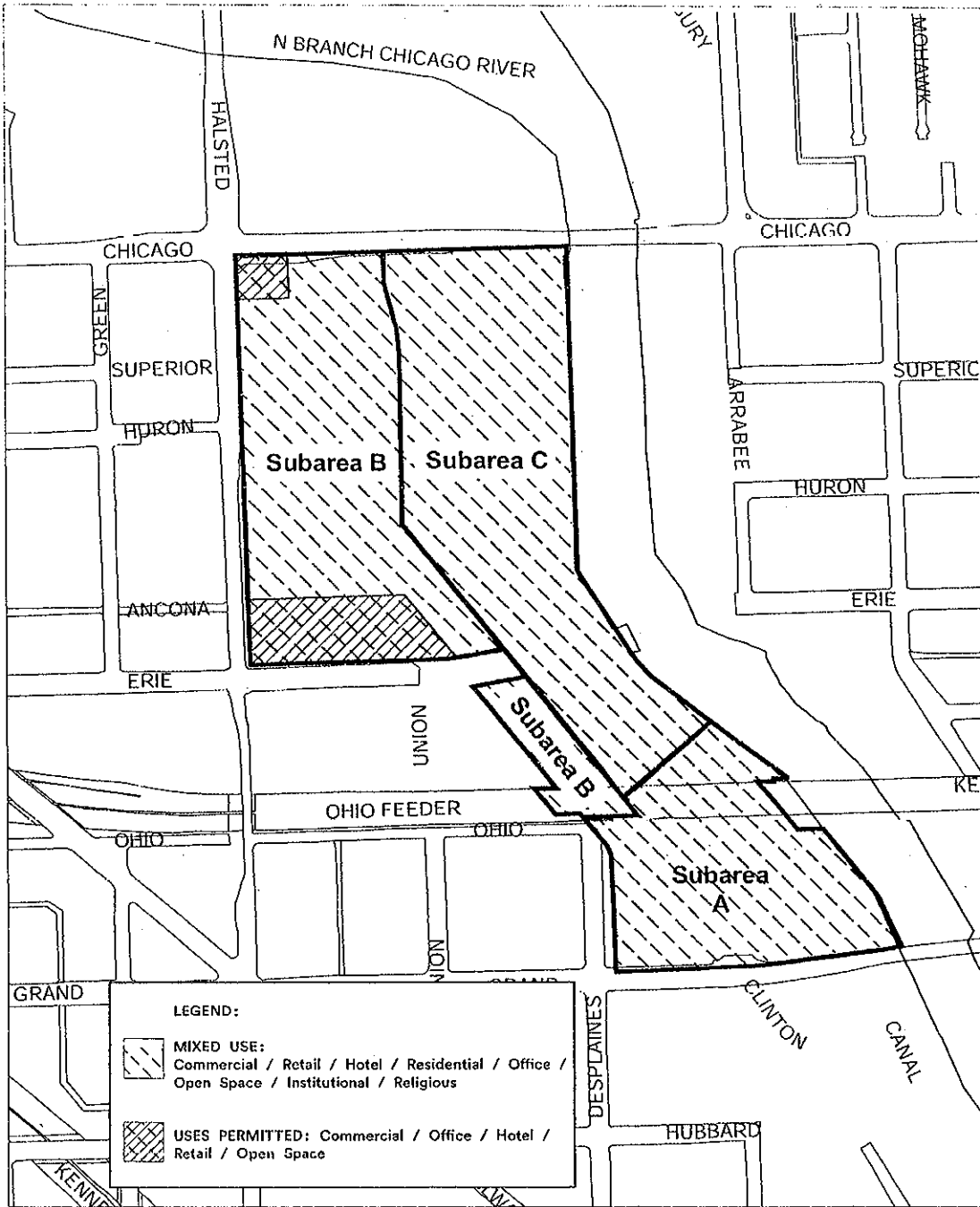
Date: 10 - 18 - 2018



EXISTING LAND USE MAP

Applicant: 1L-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE
 Date: 10 - 18 - 2018

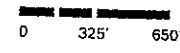
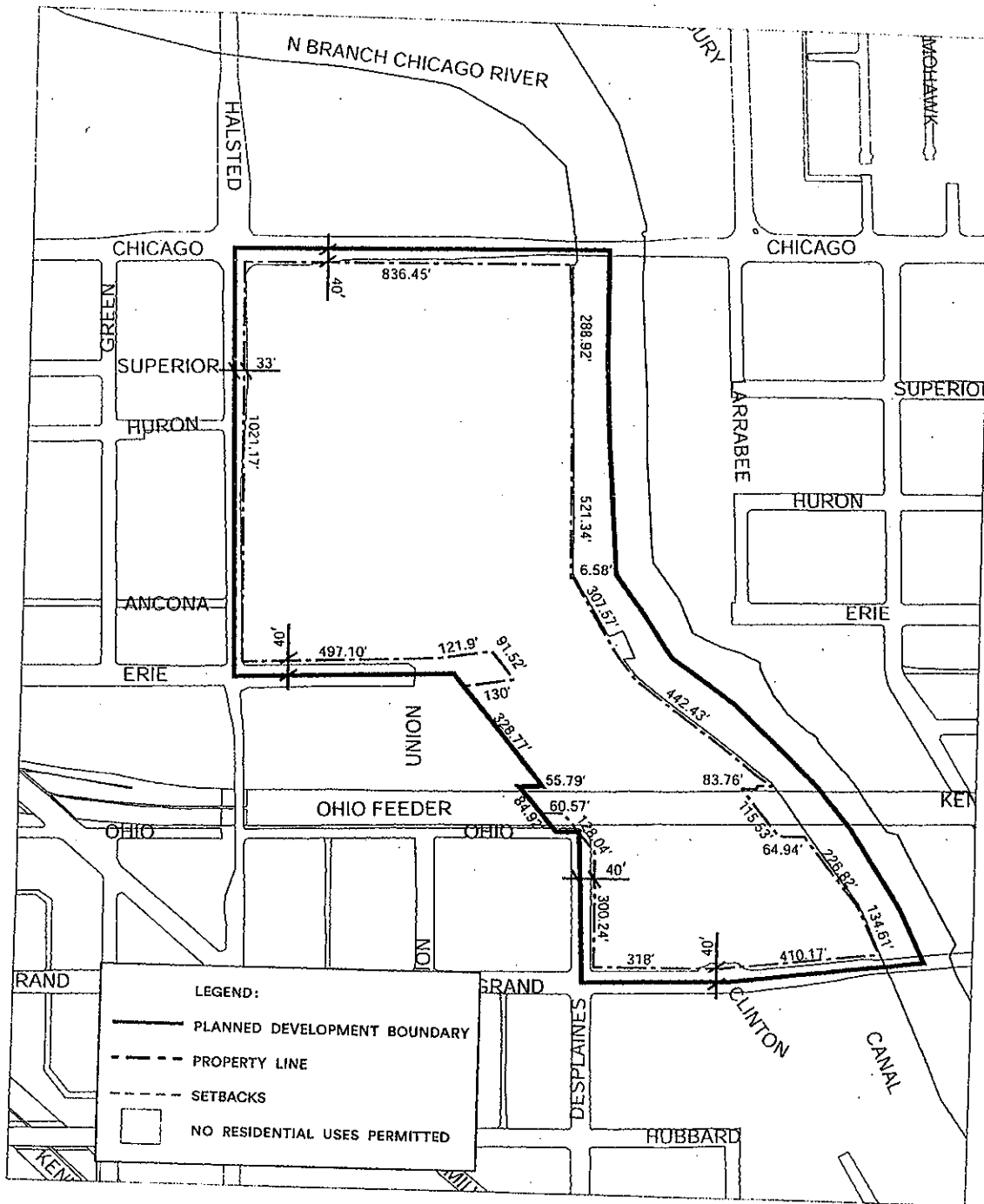


GENERAL LAND-USE PLAN

Applicant: 11-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

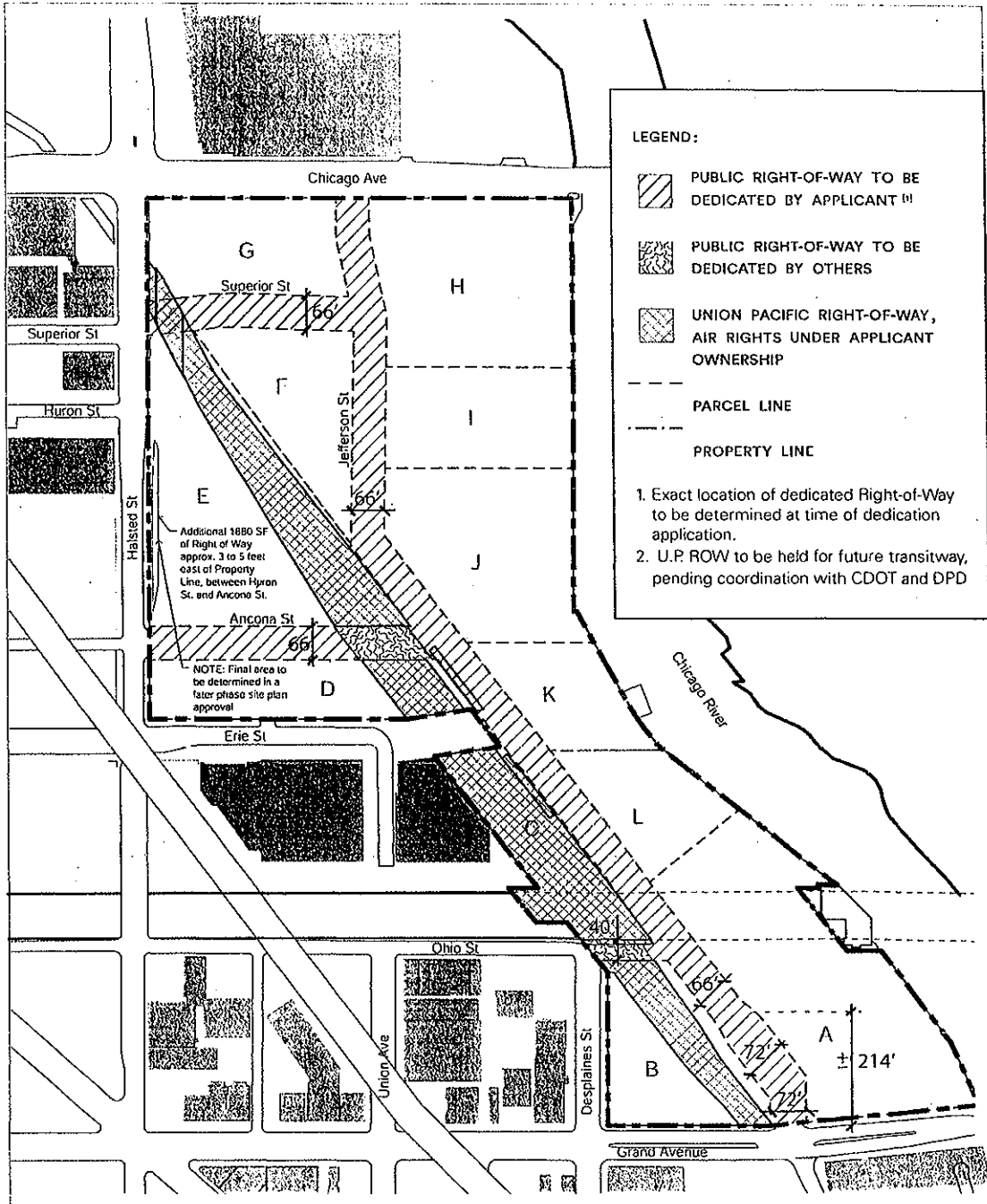
Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018






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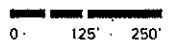
PLANNED DEVELOPMENT BOUNDARY AND PROPERTY LINE MAP
 Applicant: IL-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 601-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE
 Project: 777 W CHICAGO AVE
 Date: 10 - 18 - 2018



LEGEND:

-  PUBLIC RIGHT-OF-WAY TO BE DEDICATED BY APPLICANT ⁽¹⁾
-  PUBLIC RIGHT-OF-WAY TO BE DEDICATED BY OTHERS
-  UNION PACIFIC RIGHT-OF-WAY, AIR RIGHTS UNDER APPLICANT OWNERSHIP
- PARCEL LINE
- - - PROPERTY LINE

1. Exact location of dedicated Right-of-Way to be determined at time of dedication application.
2. U.P. ROW to be held for future transitway, pending coordination with CDOT and DPD



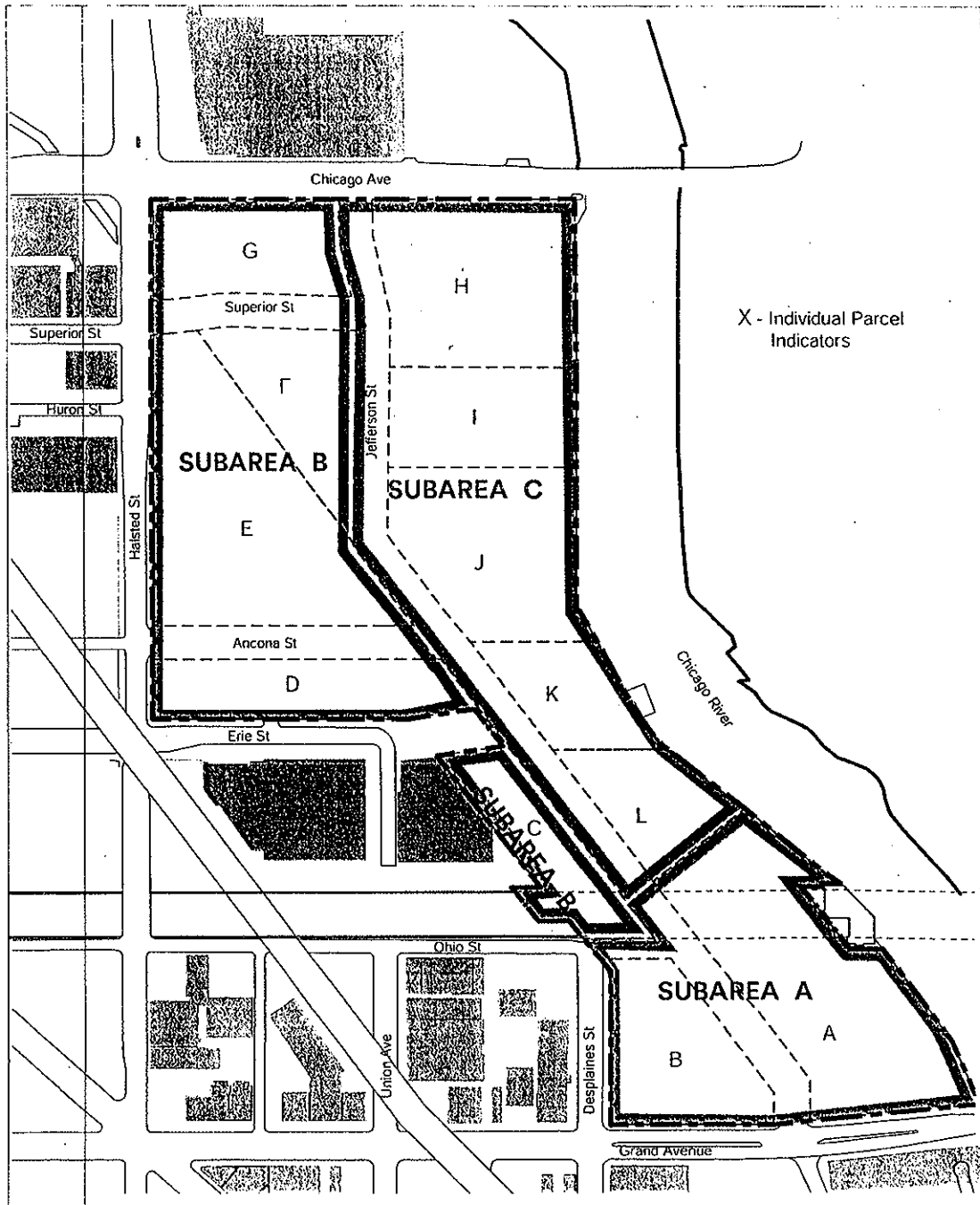
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RIGHT-OF-WAY ADJUSTMENT MAP

Applicant: IL-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 501-511 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018

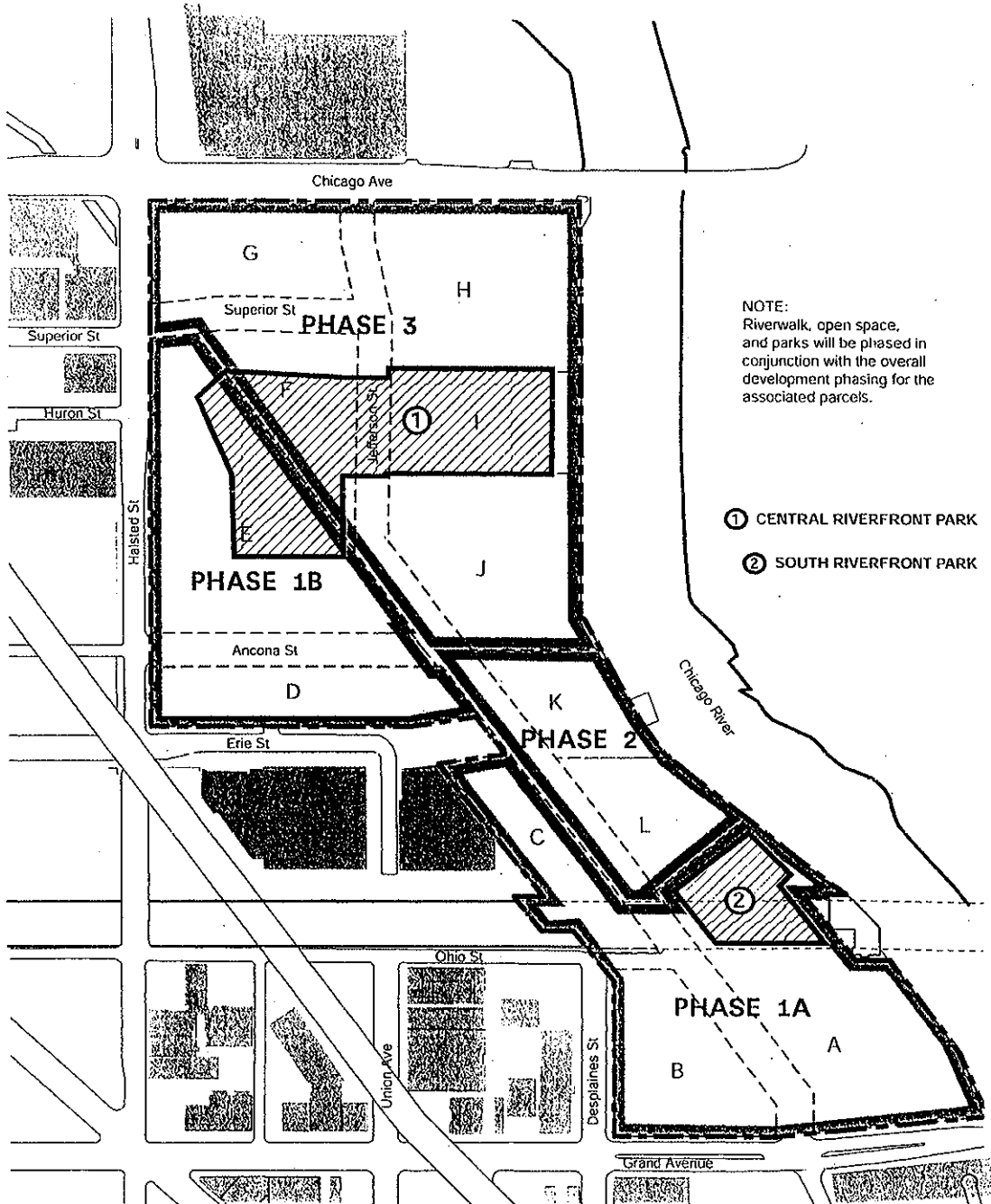


SUBAREA AND PARCELIZATION PLAN

Applicant: 1L-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018



NOTE:
Riverwalk, open space,
and parks will be phased in
conjunction with the overall
development phasing for the
associated parcels.

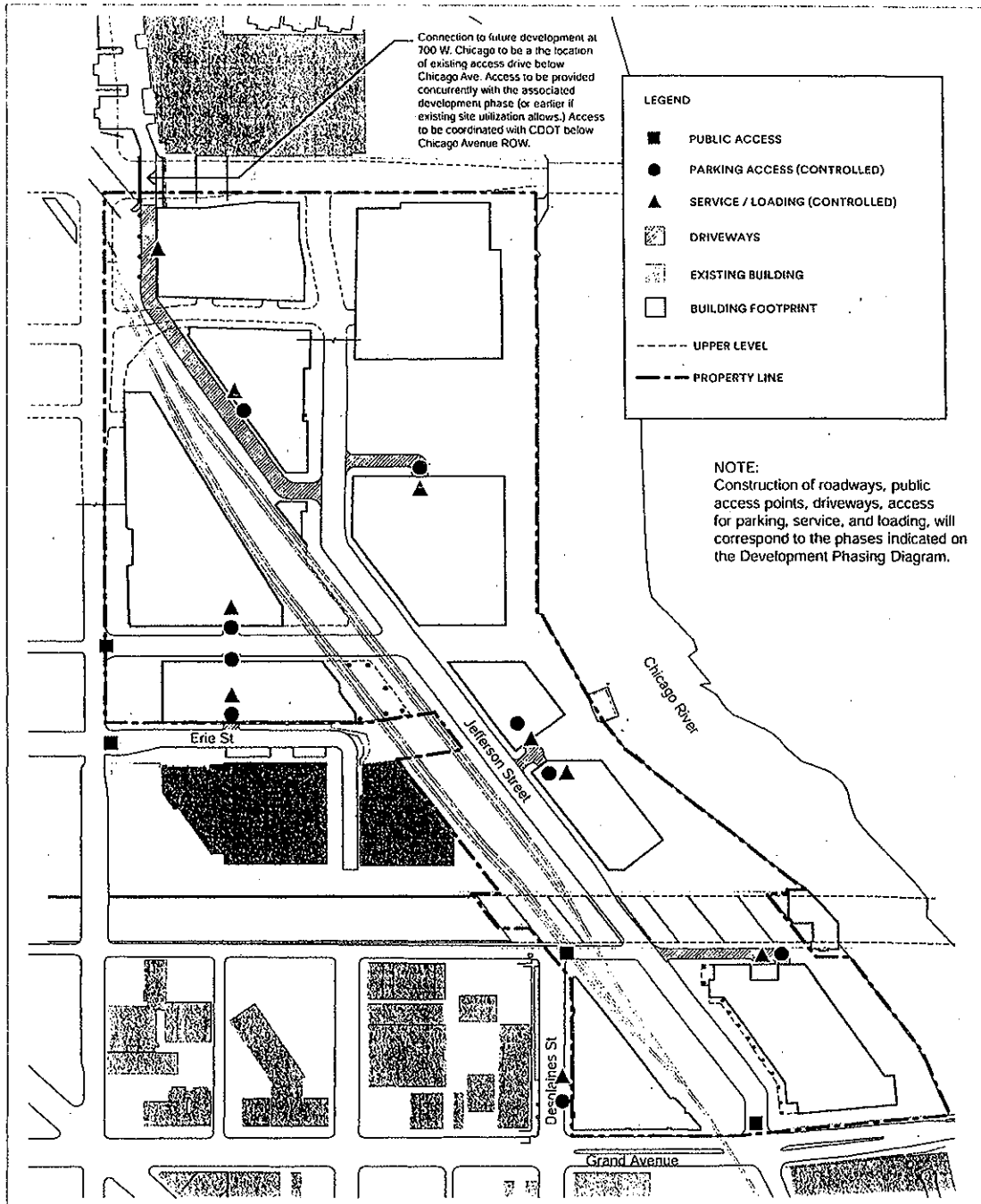
- ① CENTRAL RIVERFRONT PARK
- ② SOUTH RIVERFRONT PARK

DEVELOPMENT PHASING DIAGRAM

Applicant: IL-777 WEST CHICAGO AVENUE LLC
643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
601-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVENUE

Date: 10 - 18 - 2018



0 125' 250'

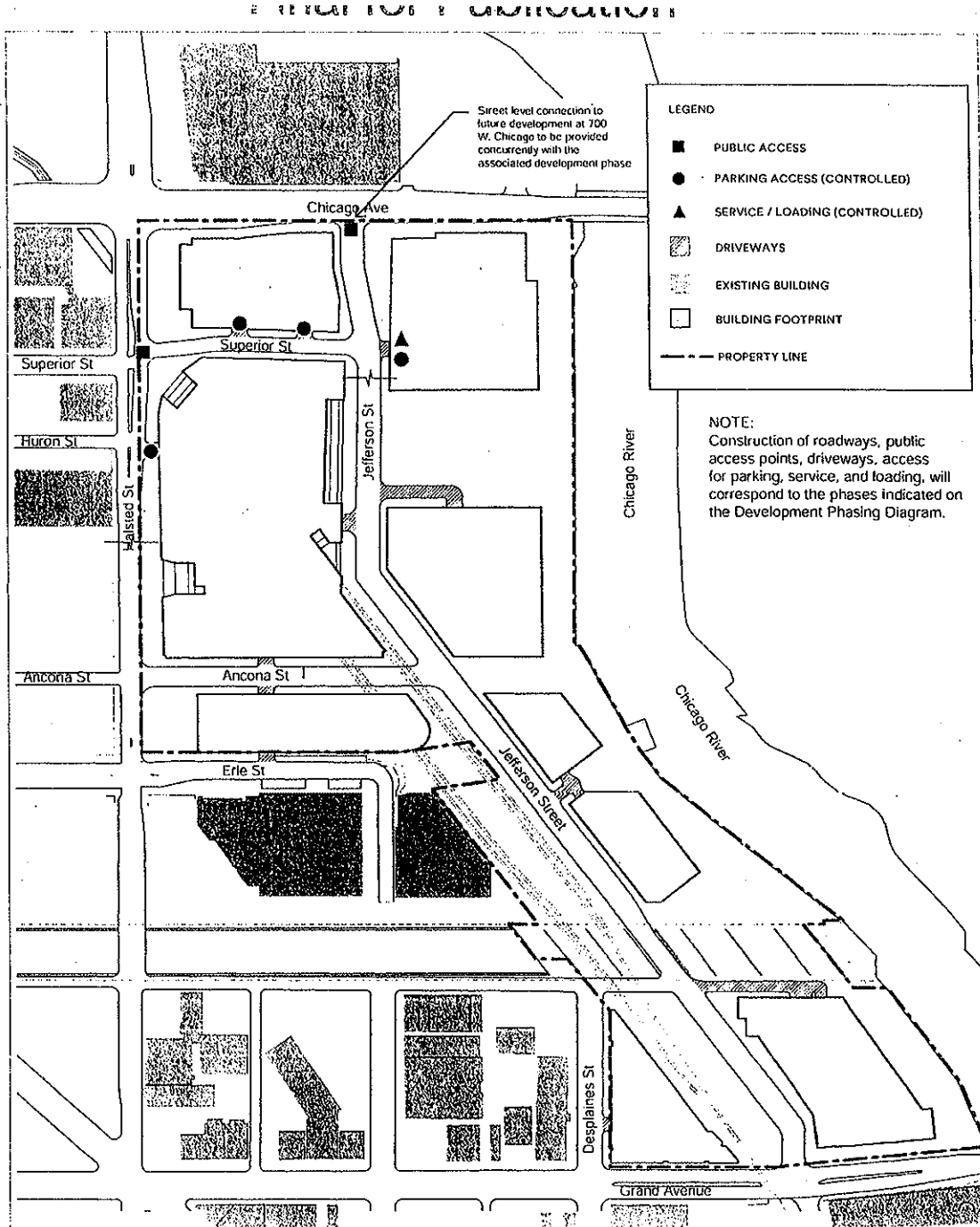


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SITE ACCESS AND LOADING PLAN: LEVEL 1 (+0.0 to +18.0 CCD)

Applicant: IL-777 WEST CHICAGO AVENUE LLC
643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE
Date: 10 - 18 - 2018



LEGEND

- PUBLIC ACCESS
- PARKING ACCESS (CONTROLLED)
- ▲ SERVICE / LOADING (CONTROLLED)
- ▨ DRIVEWAYS
- ▤ EXISTING BUILDING
- BUILDING FOOTPRINT
- - - PROPERTY LINE

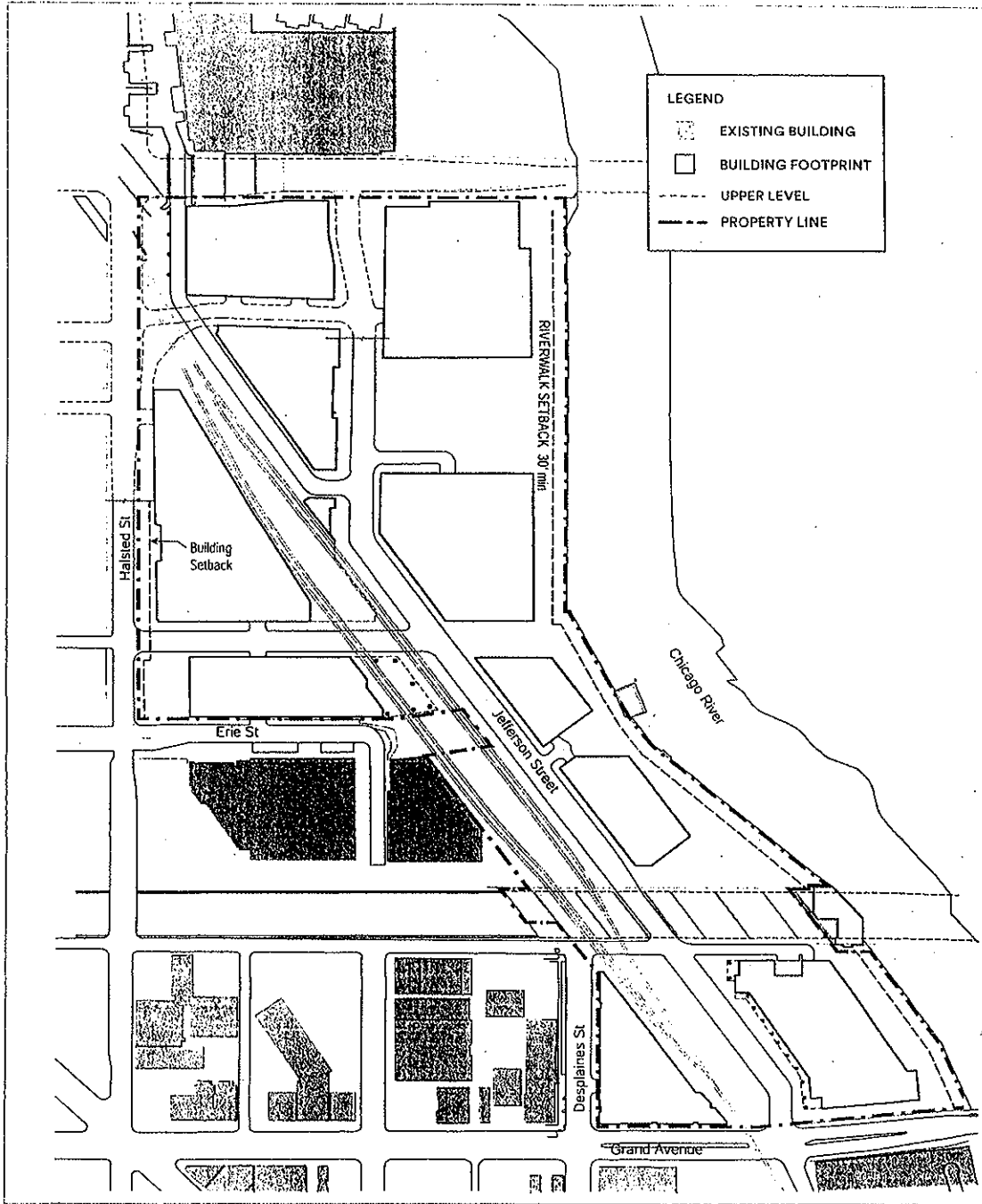
NOTE:
Construction of roadways, public access points, driveways, access for parking, service, and loading, will correspond to the phases indicated on the Development Phasing Diagram.

SITE ACCESS AND LOADING PLAN: LEVEL 2/PODIUM (+18.1 to +35.0 CCD)

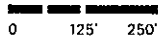
Applicant: IL-777 WEST CHICAGO AVENUE LLC
643-741 W. CHICAGO AVENUE; 643-739 N. HALSTED STREET;
632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018



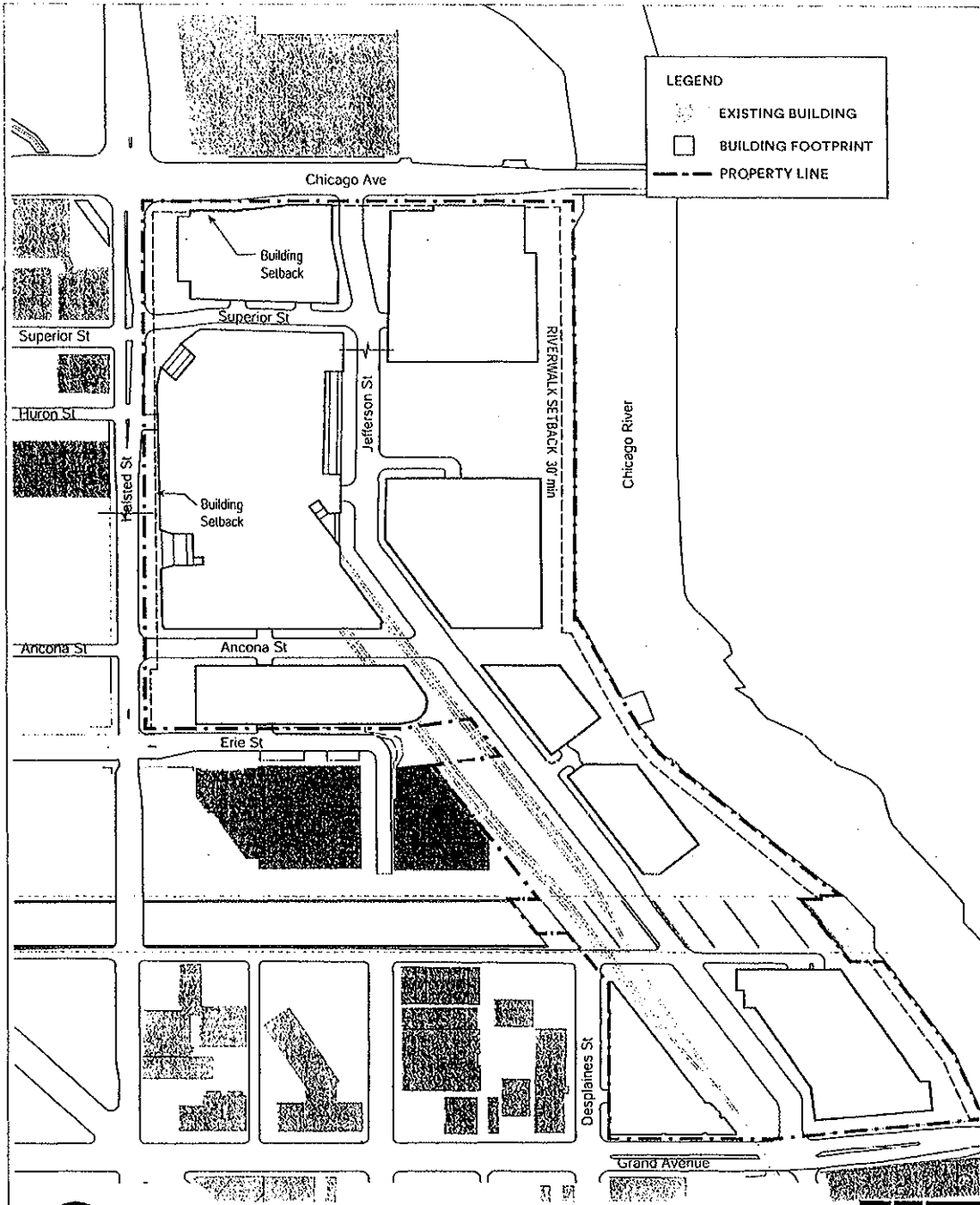
SITE LEVEL PLAN: LEVEL 1 (+0.0 to +18.0 CCD)



Applicant: IL-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 601-3: N. DESPLAINES STREET; 624-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018



0 125' 250'



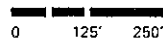
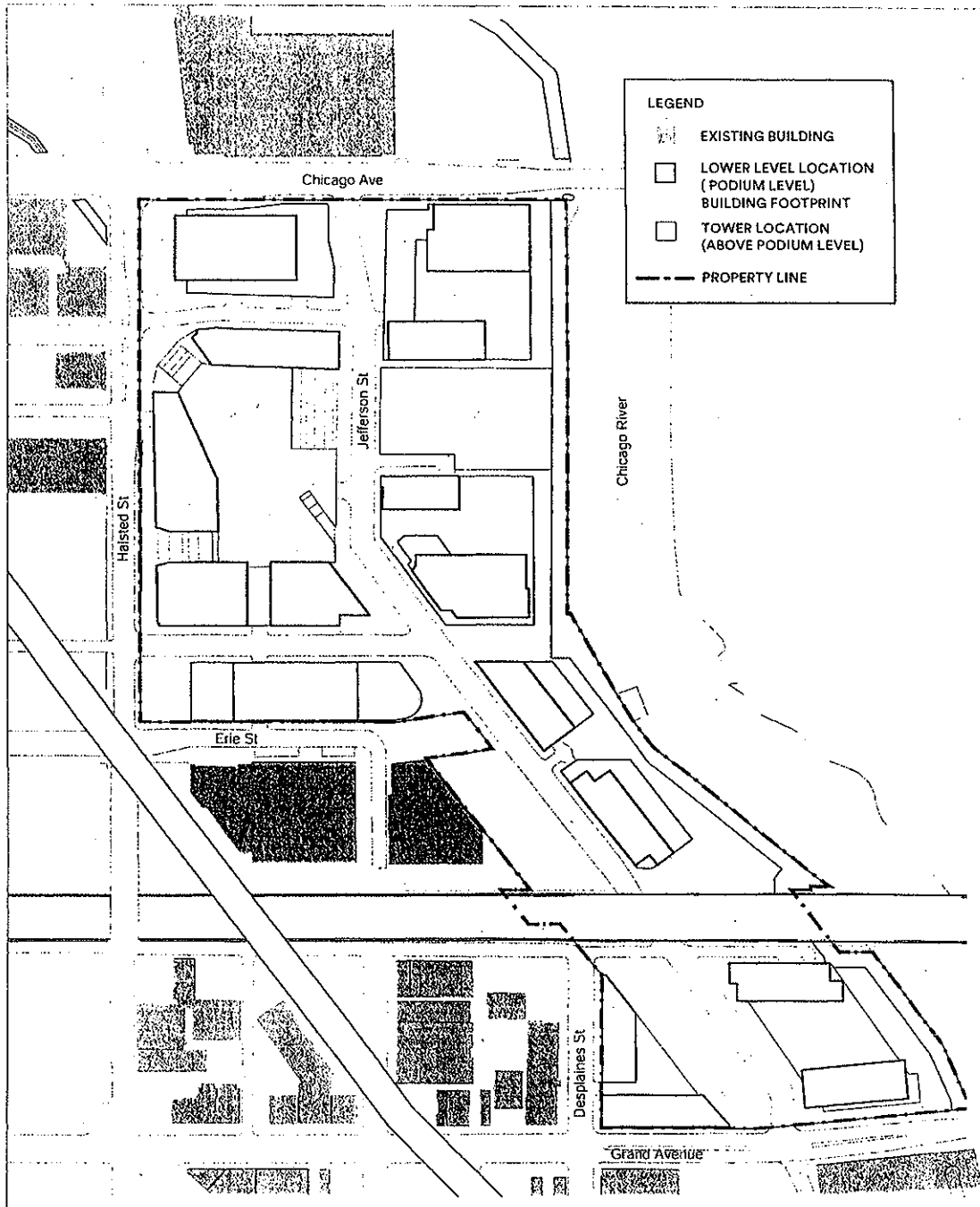
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SITE LEVEL PLAN; LEVEL 2 / PODIUM
(+18.1 to +35.0 CCD)

Applicant: IL-777 WEST CHICAGO AVENUE LLC
643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
632-740 W. ERIE STREET; 627-661 W. ERIE STREET;

Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018



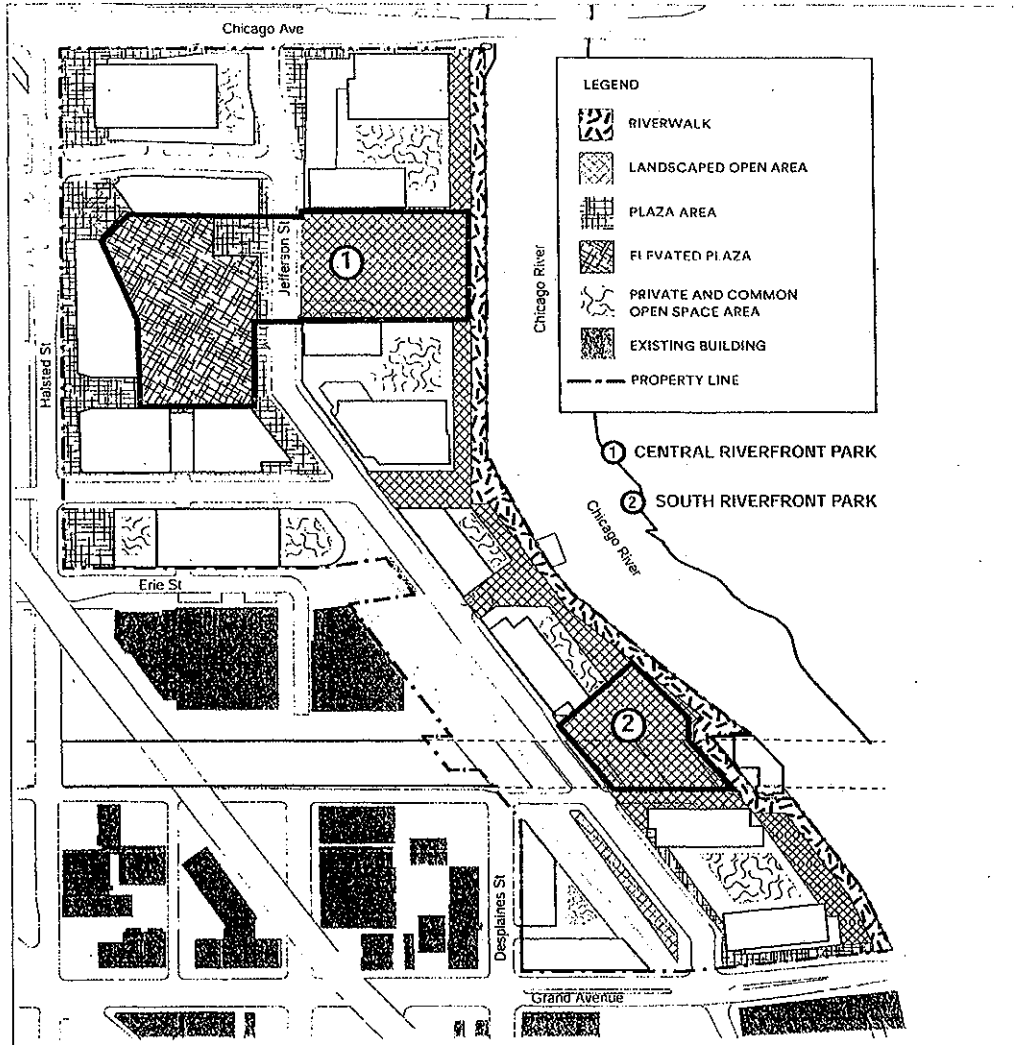
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**SITE LEVEL PLAN: LEVEL 3 / TOWER
(+35.1 CCD AND ABOVE)**

Applicant: 11-777 WEST CHICAGO AVENUE LLC
643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

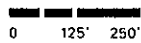
Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018



OPEN SPACE CALCULATIONS	GSF	Acres	
Publicly Accessible Open Space			
- Riverwalk (includes natural areas and landscaped stormwater features)	68,766	1.6	14% (of Public Open Space)
- Landscaped Open Area (includes natural areas and landscaped stormwater features)	227,652		46% (of Public Open Space)
- Plaza Area/Elevated Plaza (includes natural areas and landscaped stormwater features)	198,905		40% (of Public Open Space)
Subtotal: Publicly Accessible Open Space	495,323	11.4	44% Overall Site
Subtotal: Private and Common Open Space Area	60,000	1.4	6% Overall Site
Total Open Space (Public & Private Areas)	555,323	12.7	50% Overall Site (1)

1. The total amount and distribution of open space may be modified, but shall not in any event be less than 50% of the overall site.

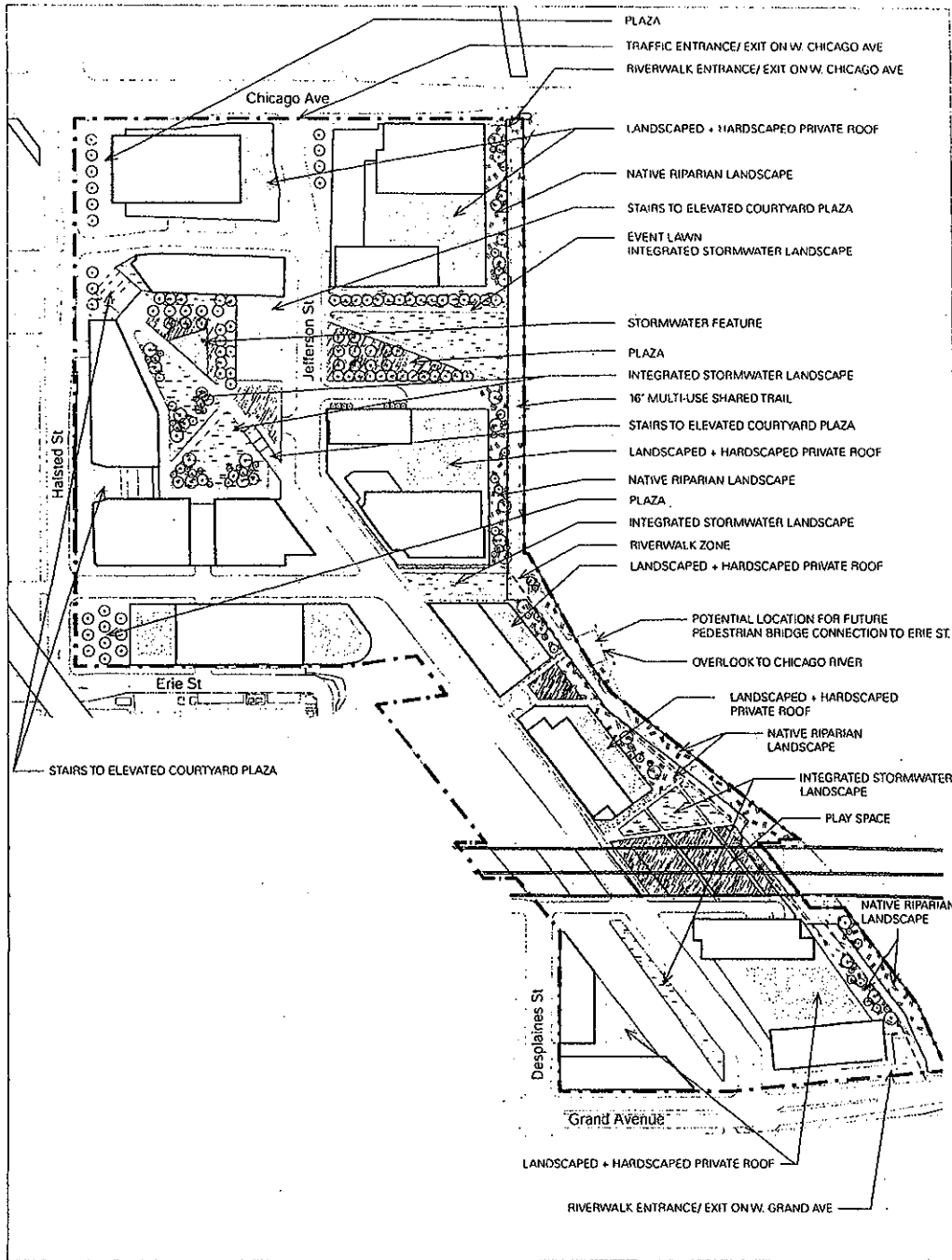


PUBLIC OPEN SPACE PLAN

Applicant: 11-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018



GENERALIZED LANDSCAPE PLAN

Applicant: 11-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 501-31 N. DESPLAINES STREET; 624-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018

SIGNIFICANT DOCUMENTS: CHICAGO APPROVED URBAN TREE PLANTING LIST

City of Chicago Urban Tree Planting List		Planting Site										Other									
Common Name	Scientific Name	City Parkway	Neighborhood	Block	Address	Parcel ID	City Property	Planting Year	Planting Priority	Planting Method	Planting Date	Planting Status	Planting Notes	Planting Agency	Planting Contact	Planting Date	Planting Status	Planting Notes	Planting Agency	Planting Contact	
...

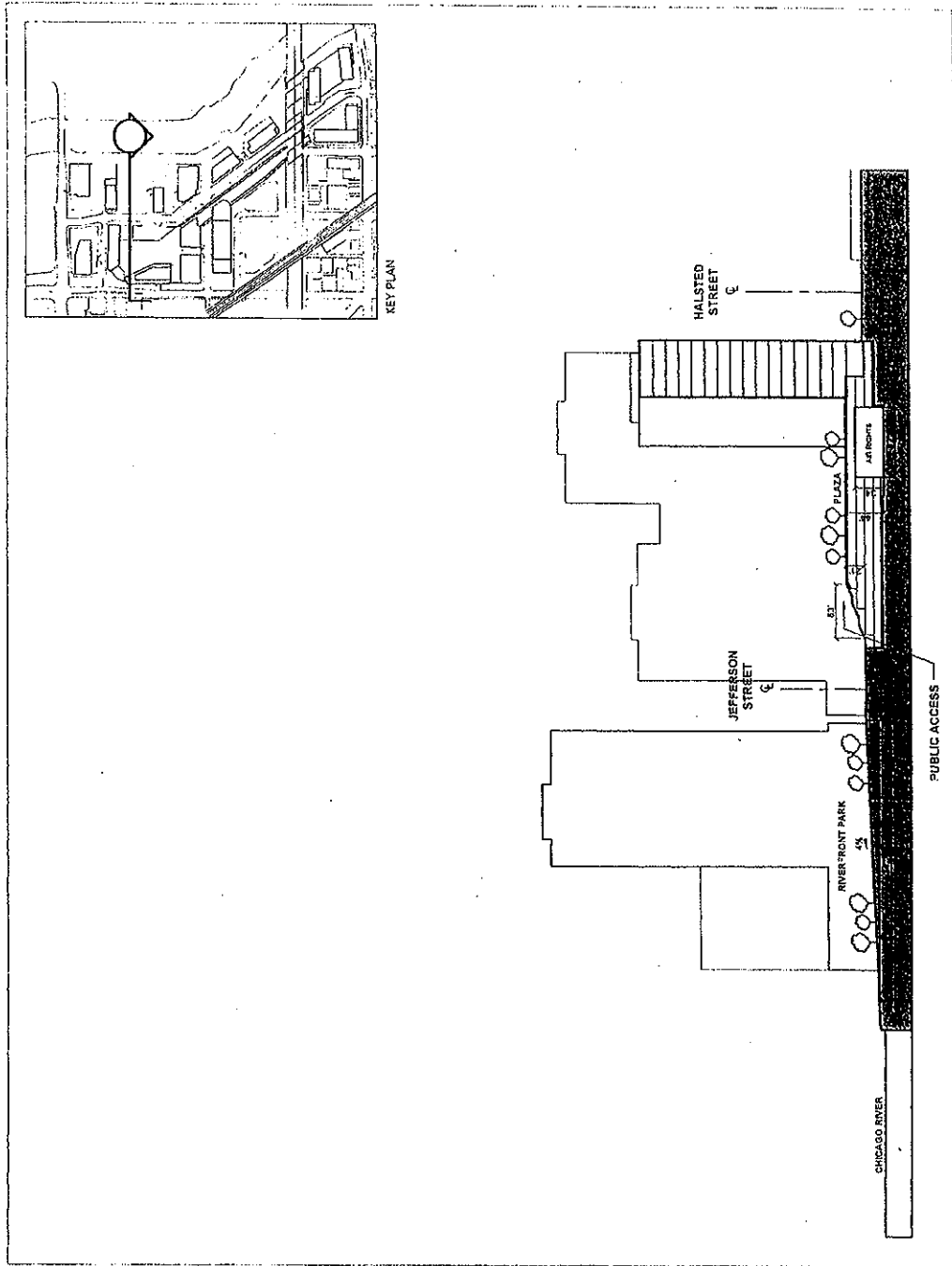
LANDSCAPE PLANT LIST

Applicant: 1L-777 WEST CHICAGO AVENUE LLC
643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018





SITE SECTION PLAN (1 OF 2)

0 75' 150'



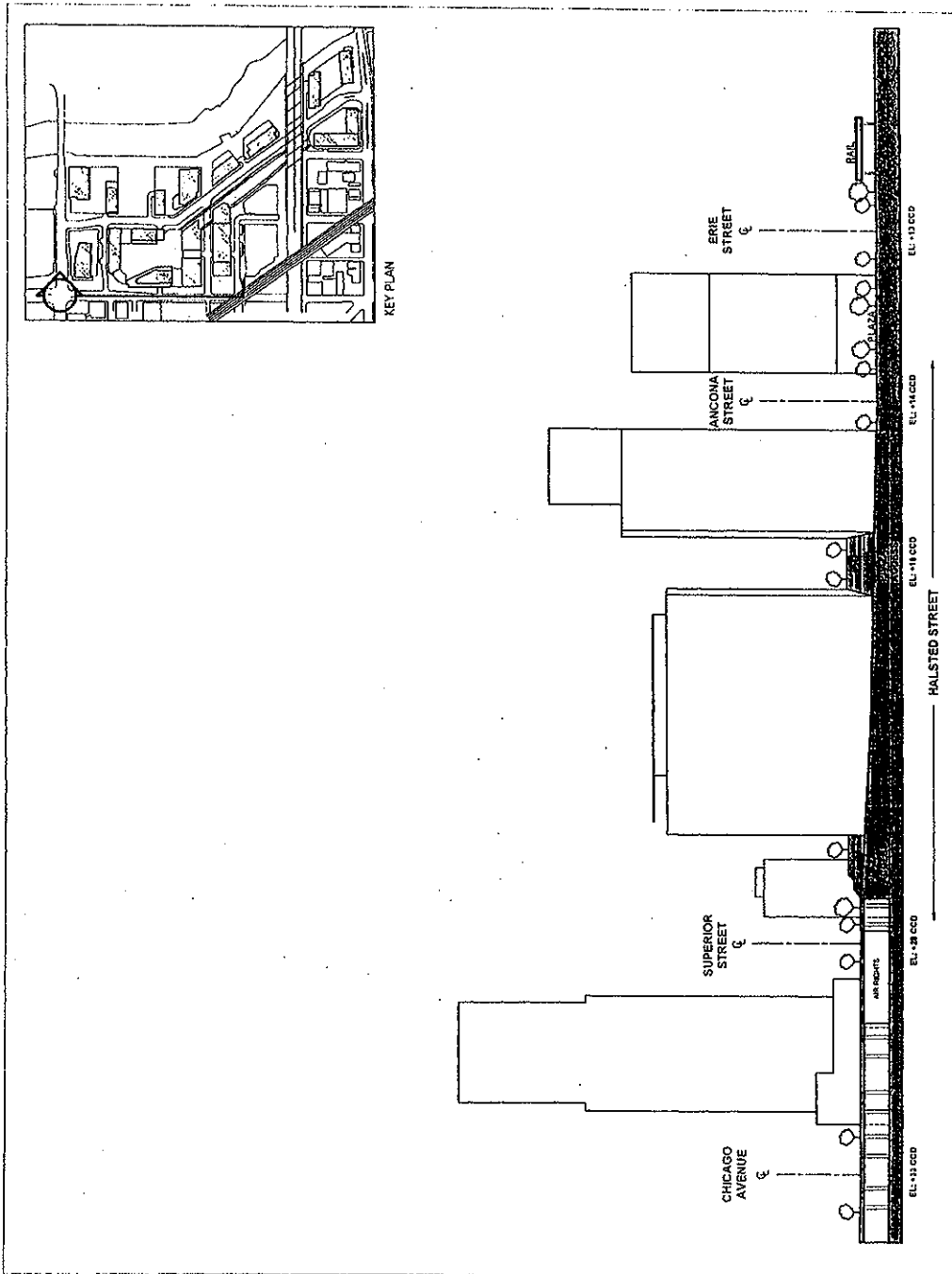
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Applicant: IL-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018

INITIAL PLAN CONSTRUCTION

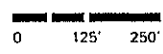
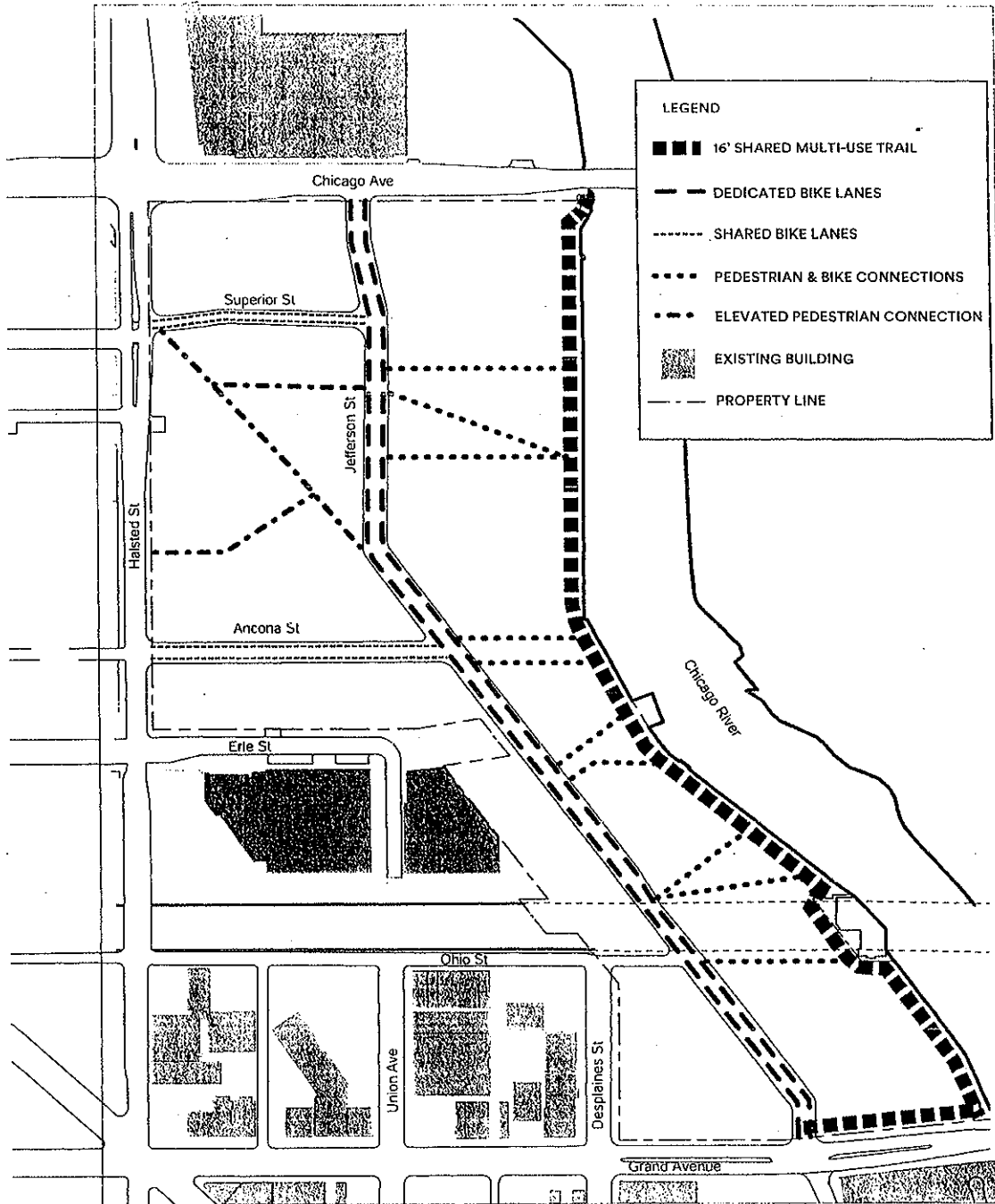


SITE SECTION PLAN (2 OF 2)

Applicant: IL-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018



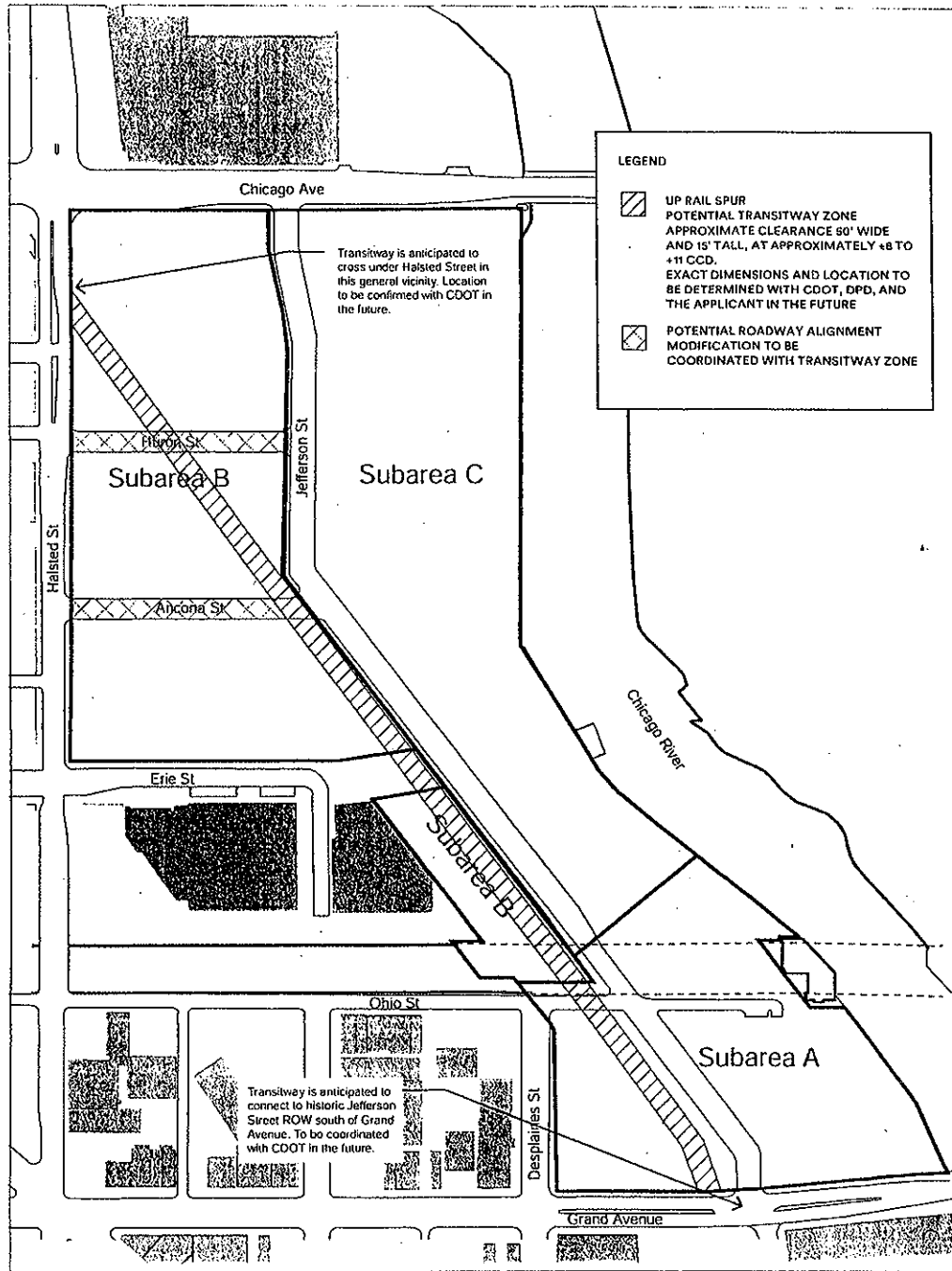
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BIKE AND PEDESTRIAN CONNECTIVITY PLAN



Applicant: IL-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018



LEGEND

-  UP RAIL SPUR POTENTIAL TRANSITWAY ZONE APPROXIMATE CLEARANCE 60' WIDE AND 15' TALL, AT APPROXIMATELY ±8 TO ±11 CCD. EXACT DIMENSIONS AND LOCATION TO BE DETERMINED WITH CDOT, DPD, AND THE APPLICANT IN THE FUTURE
-  POTENTIAL ROADWAY ALIGNMENT MODIFICATION TO BE COORDINATED WITH TRANSITWAY ZONE

Transitway is anticipated to cross under Halsted Street in this general vicinity. Location to be confirmed with CDOT in the future.

Transitway is anticipated to connect to historic Jefferson Street ROW south of Grand Avenue. To be coordinated with CDOT in the future.

0 125' 250'



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TRANSITWAY ACCOMMODATION PLAN

Applicant: 11-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018

All development within this planned development must substantially comply with the Design Guidelines of the North Branch Framework adopted by the Chicago Plan Commission on May 18, 2017, or as amended, the Chicago River Corridor Design Guidelines and Standards, or as amended, and with the design standards and guidelines outlined in the Zoning Ordinance, Section 17-8-0900 Standards and Guidelines. The guidelines listed below provide additional standards for buildings and public spaces to complement the specific context of this planned development:

GENERAL DEVELOPMENT GUIDELINES

- Building siting and base design shall define the street walls along Jefferson Street, Chicago Avenue, and Halsted Street, with setbacks allowed for plazas and open spaces.
- Jefferson Street shall be developed as a central street with dedicated bike lanes, wide sidewalks, and landscaping to promote stormwater management.
- Active ground floor uses shall be integral to buildings along Jefferson Street, Chicago Avenue, and Halsted Street, and shall include uses such as retail, lobbies, or office space.
- Active ground-floor uses shall have direct access to publicly-accessible open spaces and the riverwalk where feasible.
- Buildings at the corner of Halsted Street and Chicago Avenue shall incorporate massing, siting, and façade designs to express and reinforce a district gateway.
- In general, vehicular pick-up and drop-off shall be oriented away from Chicago Avenue and Grand Avenue. Drop off zones on Halsted to be coordinated with CDOT and CTA. Parking and service entrances for buildings along Jefferson Street will be provided via access drives or driveways. Pick-up and drop-off locations within the public way may be incorporated only where curbside space exists for that use.
- Curb-cuts should not exceed 20' in width and parking entries should be integrated with the building's architecture through the use of similar materials, patterns, textures, and color. Curb cuts shall be coordinated with CDOT and may be widened within CDOT's standards if required to provide adequate access for service, loading, and shuttle services.
- To preserve access to light, a minimum separation of 40' between towers is required. Where site constraints permit, towers will be oriented to maximize passive cooling, natural lighting, and energy efficiency.
- Undeveloped parcels may support interim uses including, but no limited to recreational open space, dog friendly areas, and surface parking. Interim use plan improvements and time frame will require review and approval by DPD.

BUILDING ENVELOPE DESIGN GUIDELINES

- Building designs shall achieve a varied and distinctive skyline.
- Mechanical penthouses and any upper level parking floors shall be screened with high-quality architectural elements consistent with the overall building facade.
- Parking levels shall be screened adequately to obscure car headlights and sound from neighboring buildings.
- Podium and tower designs shall relate to each other to provide a cohesive expression.
- Podium roofs will incorporate active amenity decks and landscaping as appropriate to building uses.
- Balconies and any required ventilation shall be integrated within the design of the building facade.
- Tenant signage for each building should be considered as part of the facade design to ensure consistency of placement, size, materials, and method of illumination.

MATERIAL GUIDELINES FOR BUILDINGS

- Buildings will not be clad with low quality materials, and materials with low aesthetic value such as Exterior Insulation and Finish System (EIFS), exterior exposed Concrete Masonry Units (CMU), thin brick, or residential-type vinyl and metal siding.
- Buildings will employ architectural materials consistent with contemporary building practices, such as high quality wall systems in glass, metal, masonry, high-quality architectural concrete, or hardwood. Glazing shall not be highly reflective or mirrored.
- Building envelopes will support environmentally responsible design by reducing heat loads, improving energy efficiency, maximizing occupant comfort, and using sustainable materials.
- Podium and ground-floor level materials and design facing the streets, publicly accessible open spaces, and the riverwalk will be detailed to enhance the pedestrian environment and will be complimentary to the context.
- Ground-floor facades will have a high percentage of clear / un-tinted glazing along the primary street frontages.

LANDSCAPE GENERAL GUIDELINES

- A riverfront publicly accessible park will connect directly to the riverwalk and include gathering places, a recreational lawn, passive play spaces, native landscaping, opportunities for direct viewing of river, and stormwater infiltration.
- Pocket parks will connect the riverwalk and Jefferson Street, and to provide view corridors from Halsted Street to the river. Programming will include a variety of uses such as seating, play areas, a location for public art, and a dog park.
- The space under the Ohio Feeder Ramp will be activated with recreational uses that connect to the riverwalk park spaces and to Jefferson Street.
- Publicly accessible open spaces will be designed to the applicable standards of Chicago Park District Parks.
- The riverwalk will connect to the north side of Chicago Avenue using space provided by CDOT under the new Chicago Avenue Bridge.
- The riverwalk will connect to the street level intersection of Grand Avenue and Jefferson Street, and not preclude space for future Riverwalk connection under Grand Avenue bridge.
- The riverwalk will incorporate space to receive a future pedestrian bridge over the river connecting between riverwalk and Ward Park (aka Erie Street Pedestrian Bridge).
- Landscaping throughout will incorporate stormwater management best practices to detain, clean, and reduce the volume of stormwater discharge.
- Landscaped areas will enhance local habitat for wildlife through use native plantings, and incorporate strategic riparian edge restoration along the riverwalk where feasible.

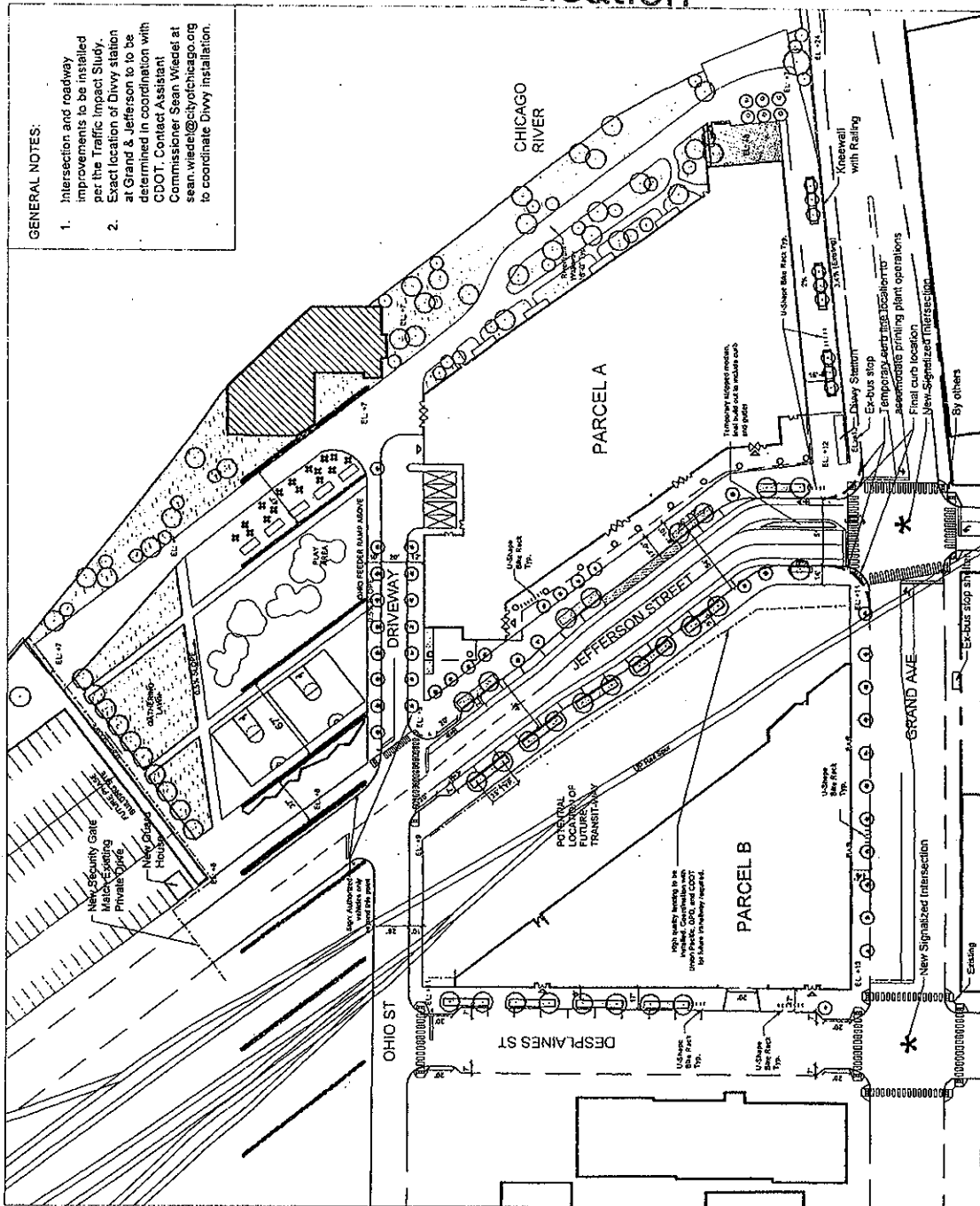
DESIGN GUIDELINES

Applicant: IL-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 501-51 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE

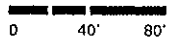
Date: 10 - 18 - 2018





GENERAL NOTES:

1. Intersection and roadway improvements to be installed per the Traffic Impact Study.
2. Exact location of Divvy station at Grand & Jefferson to be determined in coordination with CDOT. Contact Assistant Commissioner Sean Wierdel at sean.wierdel@cityofchicago.org to coordinate Divvy installation.



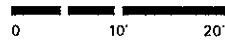
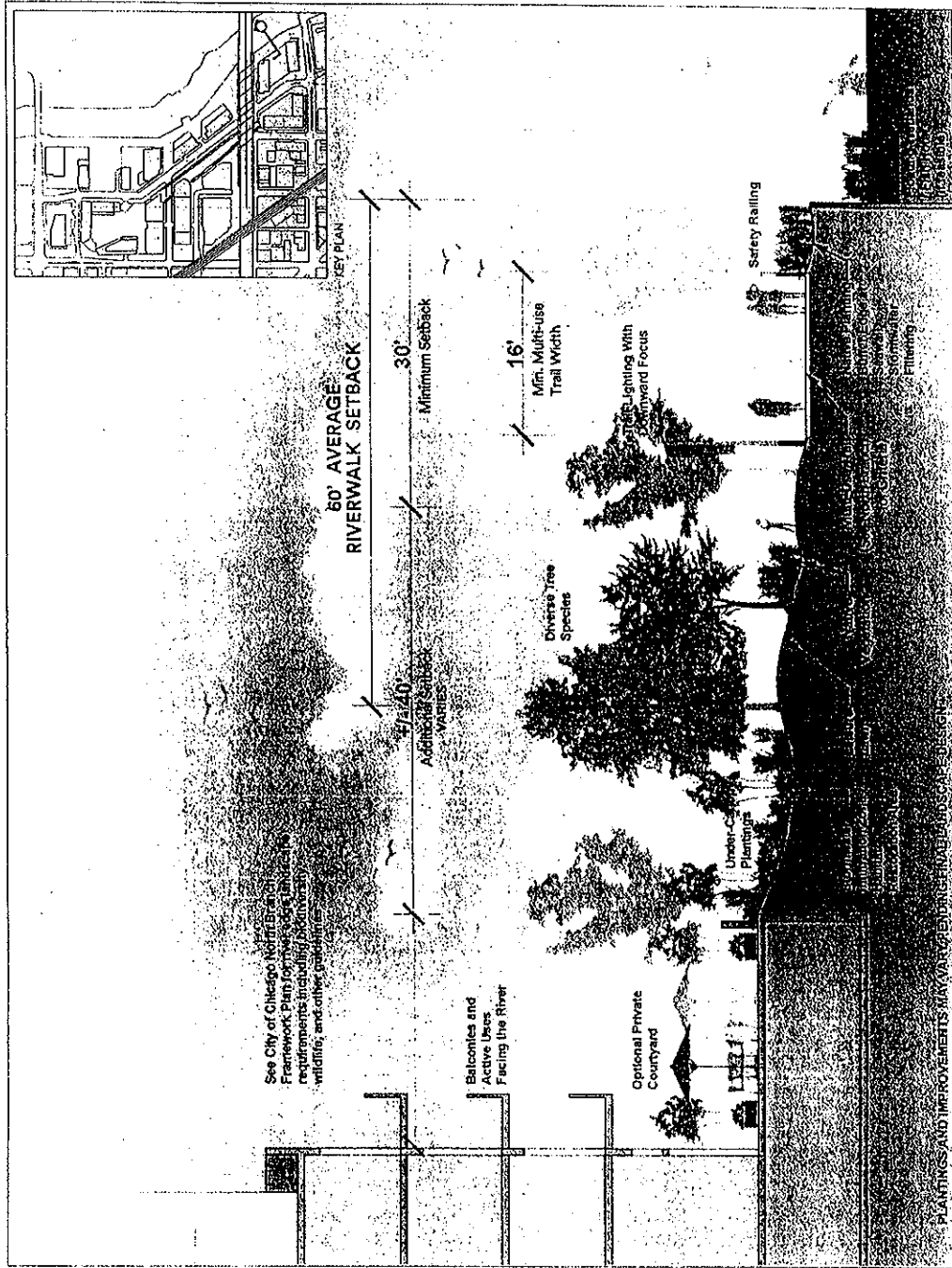
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PHASE 1A.1 AND 1A.2: SITE PLAN AND LANDSCAPE PLAN (PARCEL A AND B)

Applicant: 11-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 501-51 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2016



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PHASE 1A: RIVERWALK SECTION PLAN (PARCEL A)

Applicant: IL-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE.

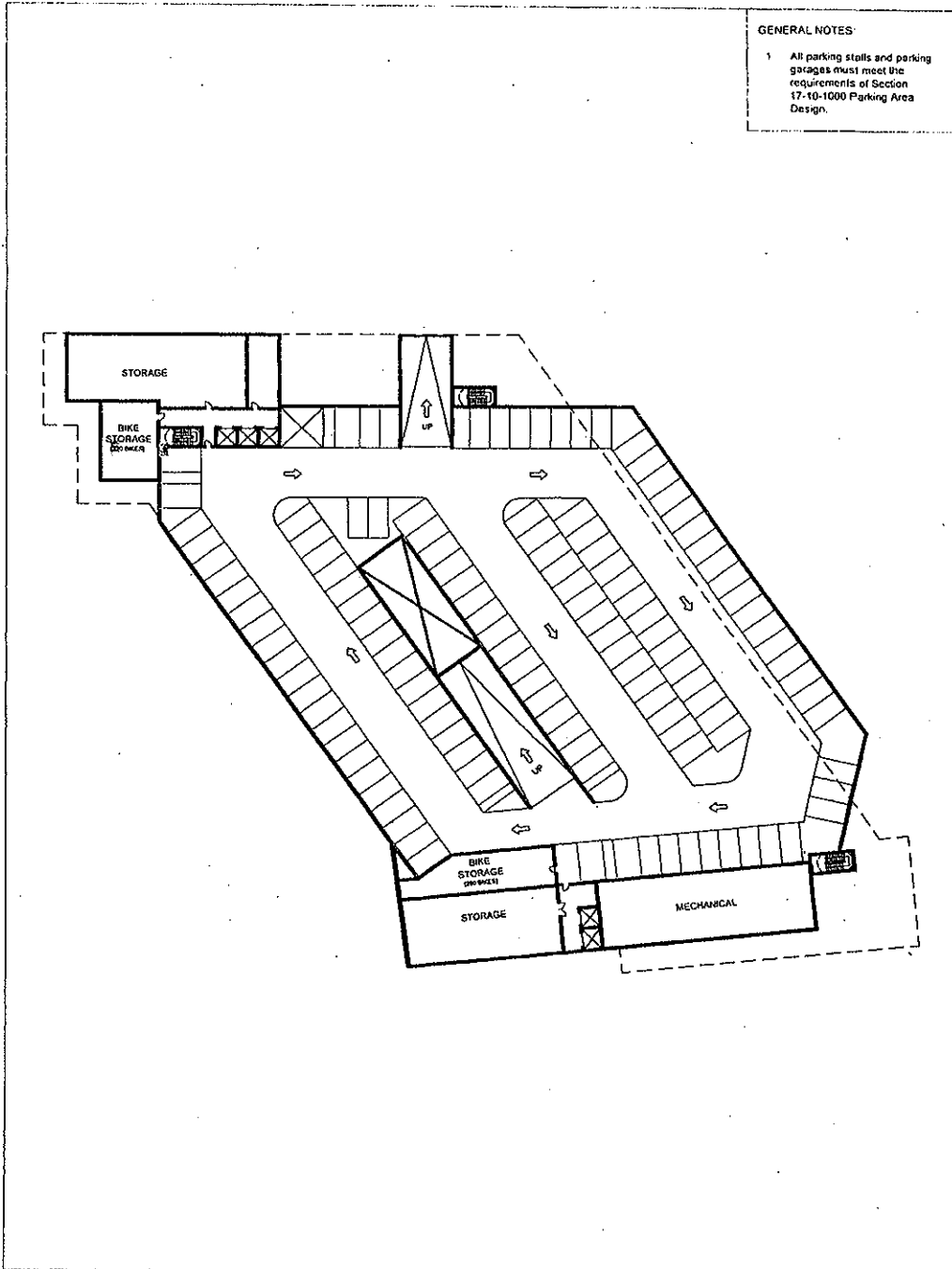
Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018

Final for Publication

GENERAL NOTES:

- 1 All parking stalls and parking garages must meet the requirements of Section 17-10-1000 Parking Area Design.



0 35' 70'



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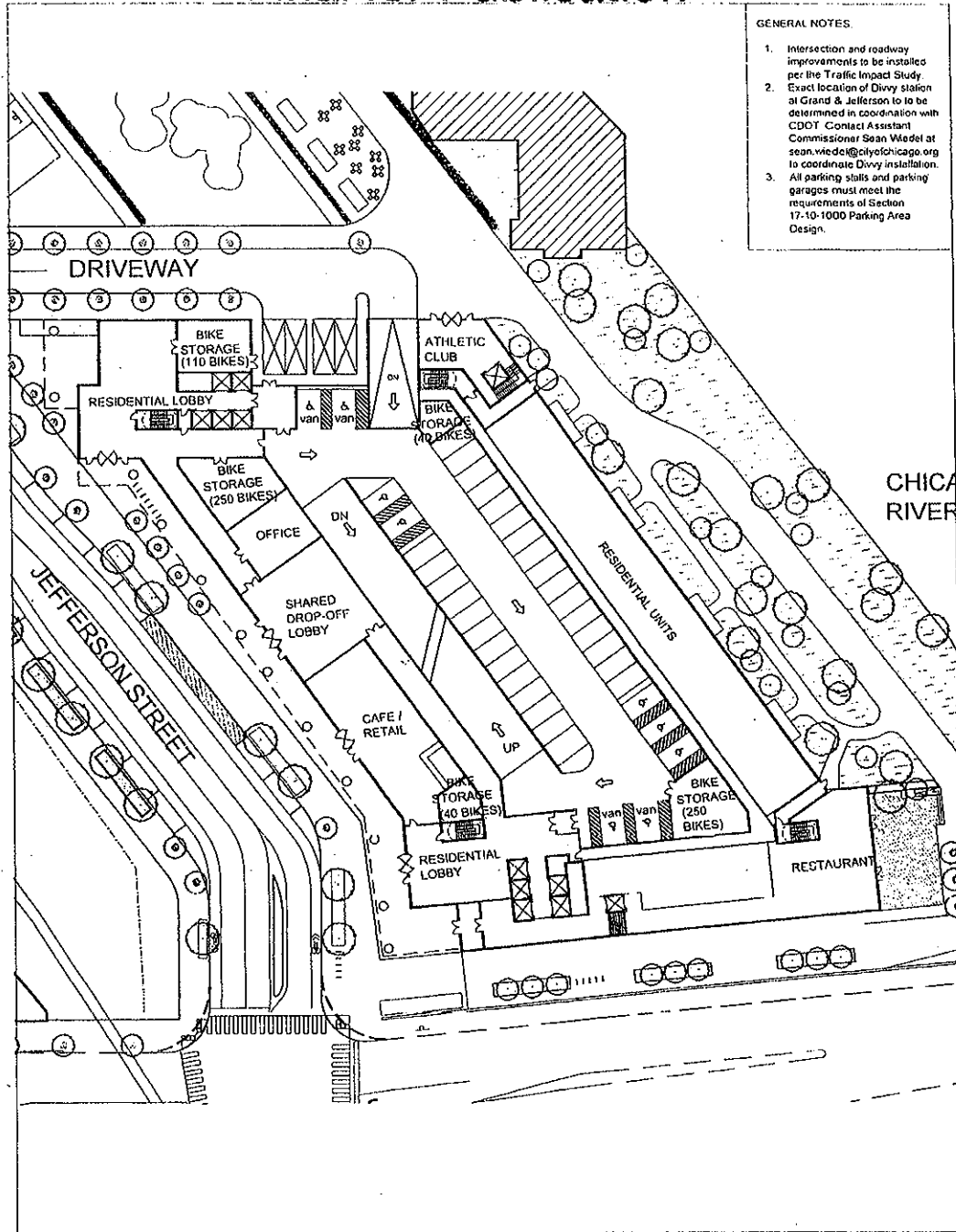
PHASE 1A.1 FLOOR PLAN: LOWER LEVEL PLAN (PARCEL A)

Applicant: 1L-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE

Date: 10 - 16 - 2018

Final for Publication



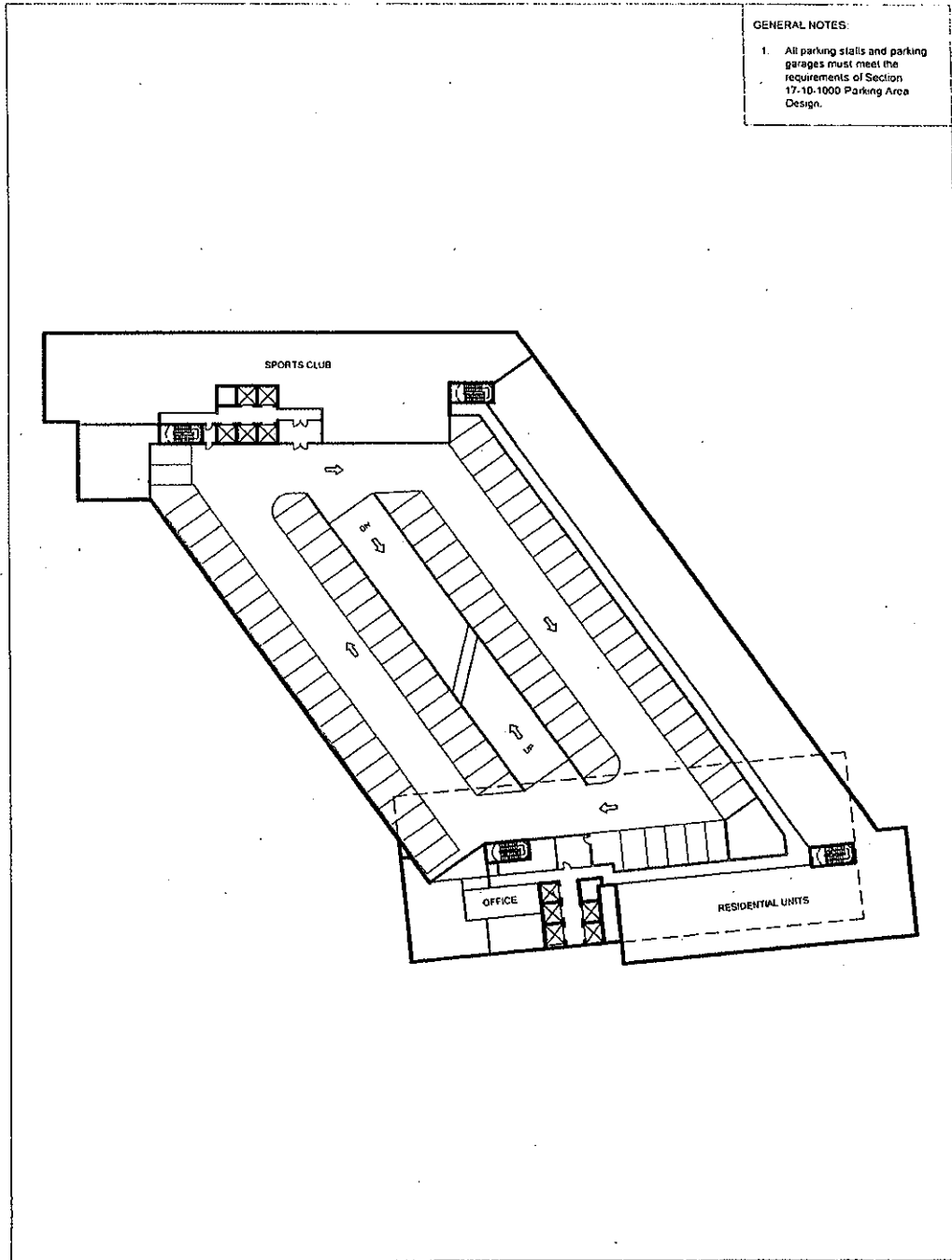
- GENERAL NOTES**
1. Intersection and roadway improvements to be installed per the Traffic Impact Study.
 2. Exact location of Divvy station at Grand & Jefferson to be determined in coordination with CDOT Contact Assistant Commissioner Sean Wiedel at sean.wiedel@cityofchicago.org to coordinate Divvy installation.
 3. All parking stalls and parking garages must meet the requirements of Section 17-10-1000 Parking Area Design.

PHASE 1A.1 FLOOR PLAN: GROUND LEVEL 1 PLAN (PARCEL A)

Applicant: 11-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

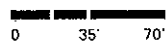
Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018



GENERAL NOTES:

1. All parking stalls and parking garages must meet the requirements of Section 17-10-1000 Parking Area Design.



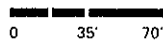
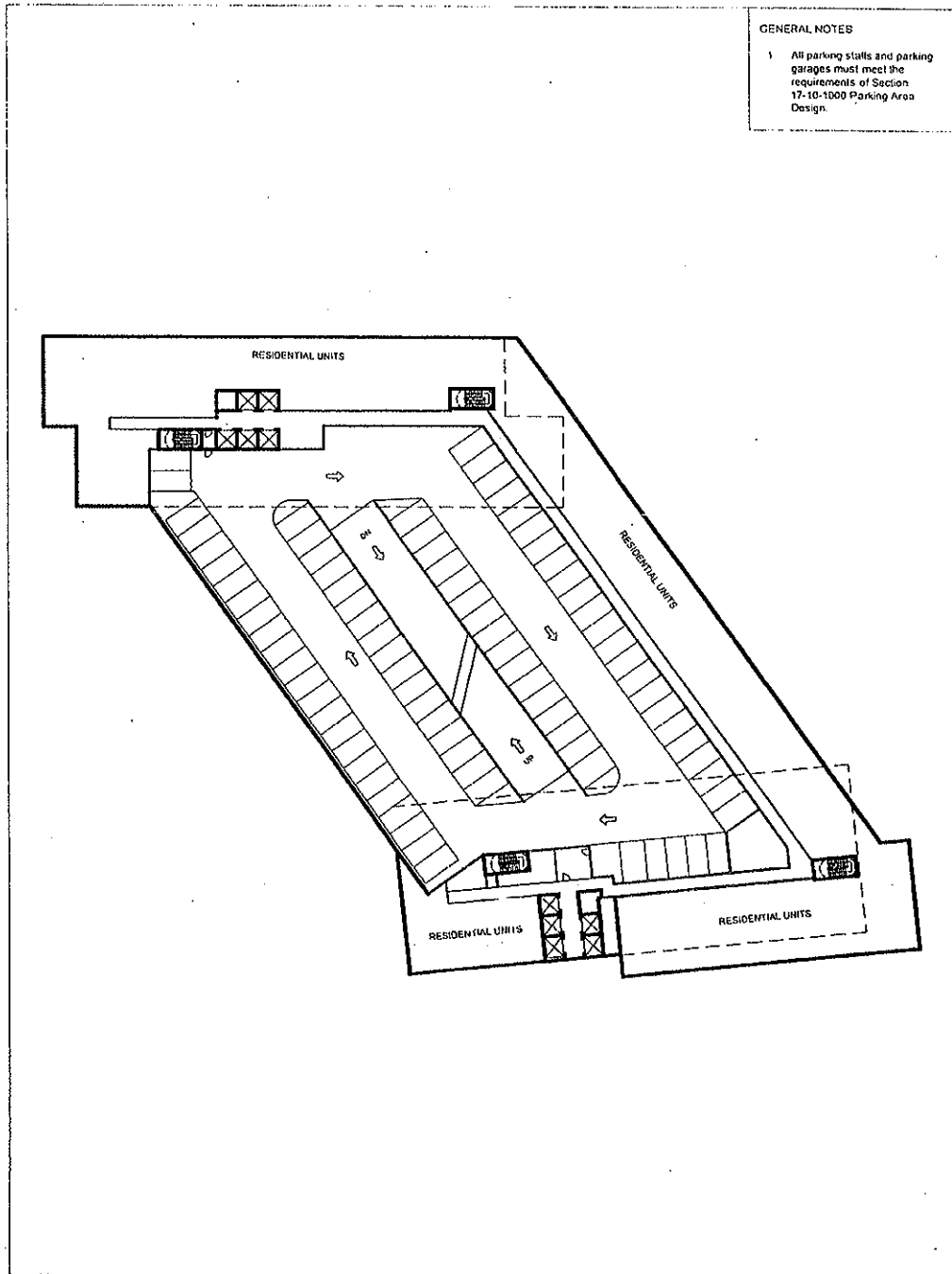
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PHASE 1A.1 FLOOR PLAN: LEVEL 2 PLAN (PARCEL A)

Applicant: 11-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018



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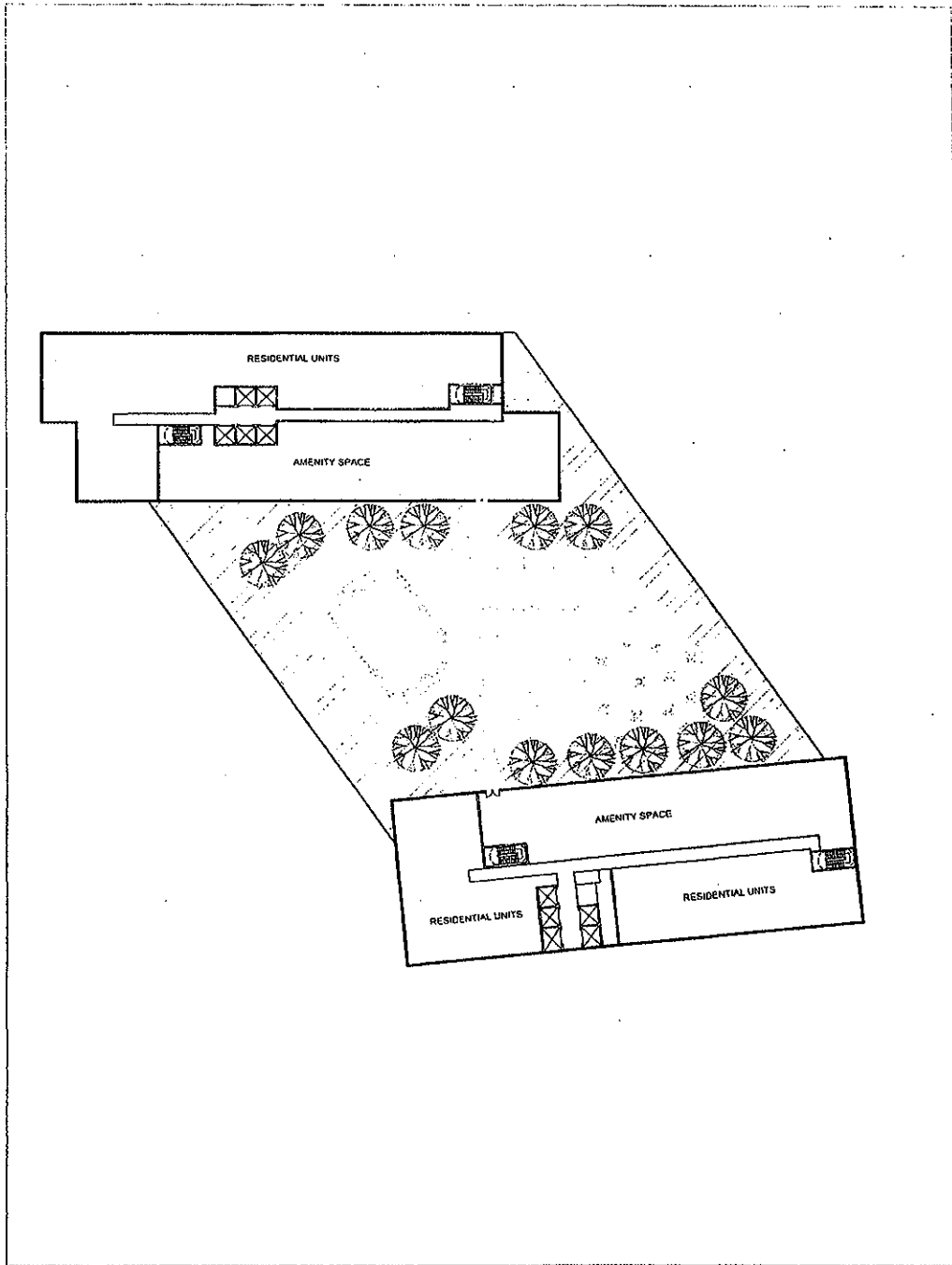
PHASE 1A.1 FLOOR PLAN: LEVEL 3-4 PLAN (PARCEL A)

Applicant: 11-777 WEST CHICAGO AVENUE LLC
643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018

PHASE 1A.1 FLOOR PLAN: LEVEL 5 / AMENITY DECK (PARCEL A)



0 35' 70'



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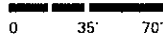
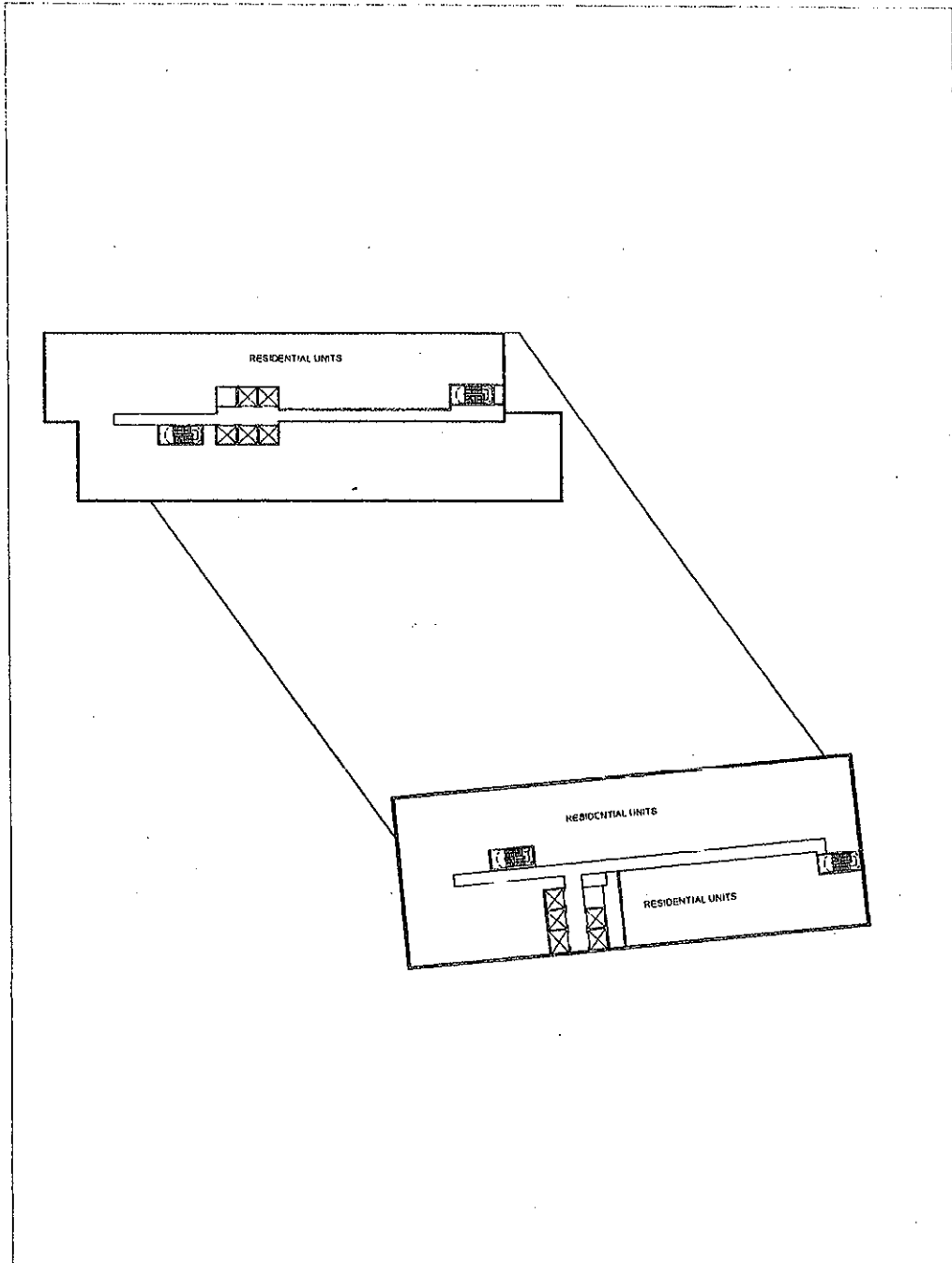
PHASE 1A.1 FLOOR PLAN: LEVEL 5 / AMENITY DECK (PARCEL A)

Applicant: 11-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018

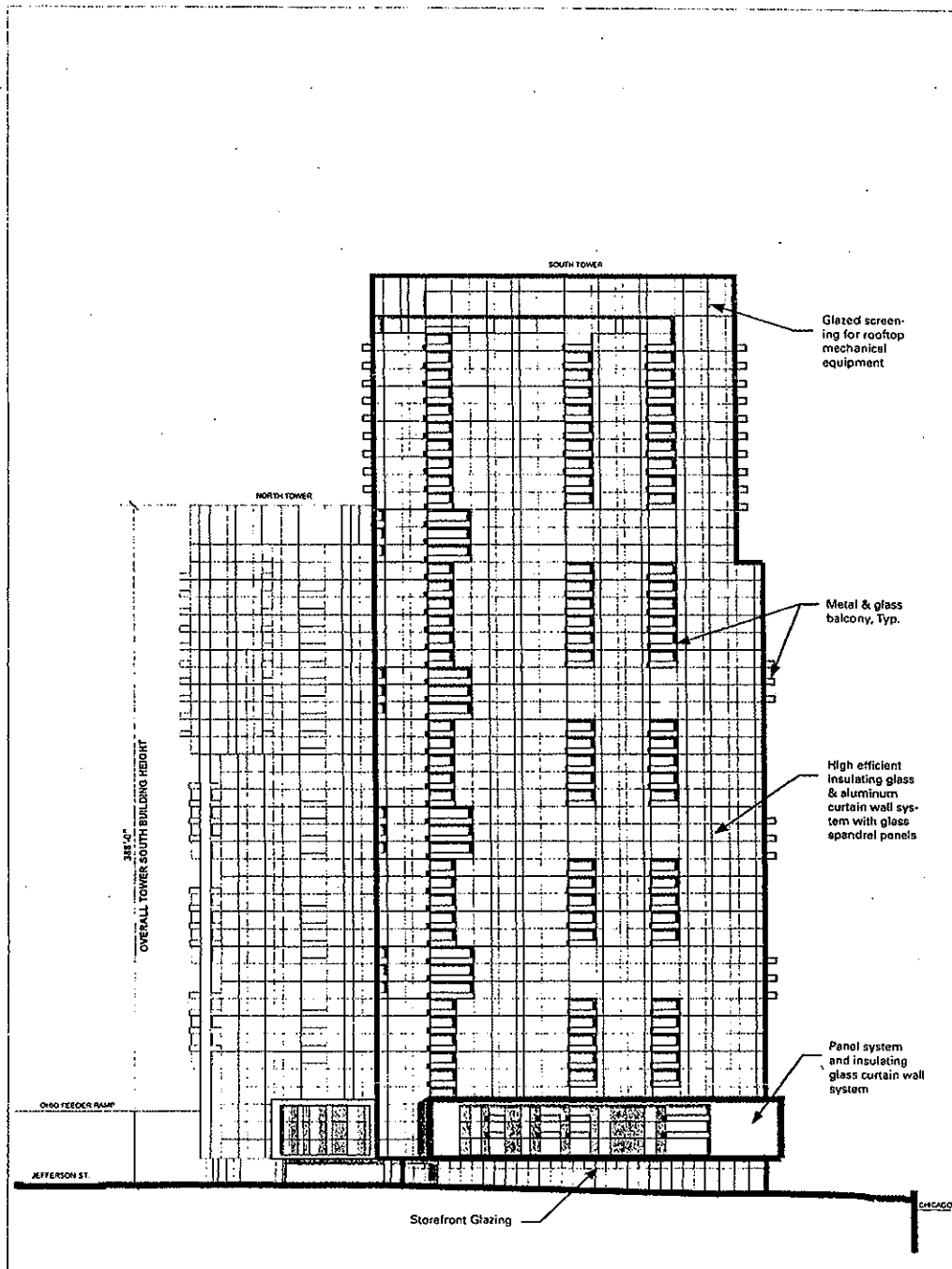
Final for Publication



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PHASE 1A.1 FLOOR PLAN: TOWER PLAN (PARCEL A)

Applicant: 12-777 WEST CHICAGO AVENUE LLC
643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE
Project: 777 W CHICAGO AVE
Date: 10 - 18 - 2018



PHASE 1A.1 BUILDING ELEVATION: SOUTH (PARCEL A)

0 40' 80'

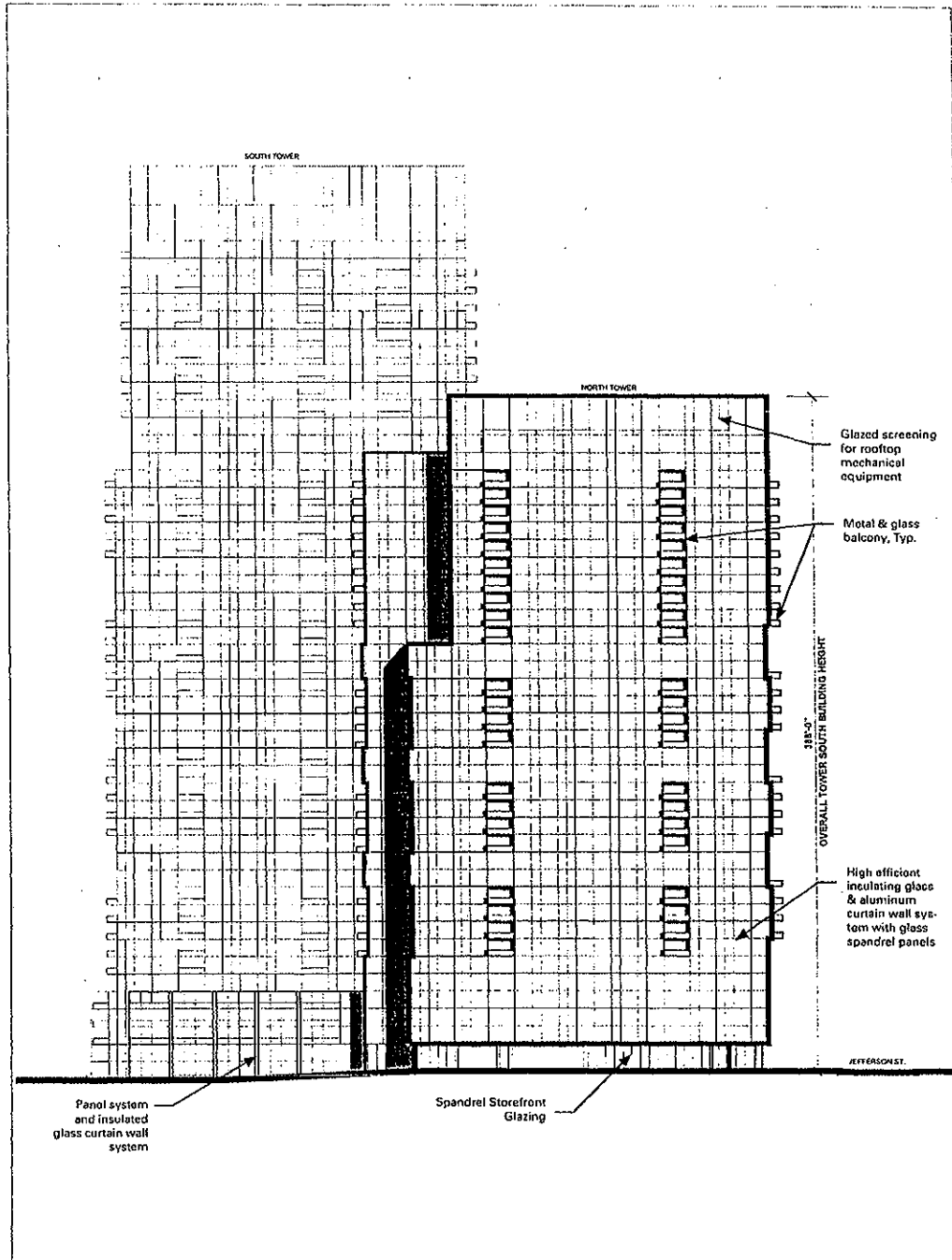


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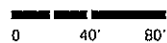
Applicant: IL-777 WEST CHICAGO AVENUE LLC
643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018



PHASE 1A.1 BUILDING ELEVATION: NORTH (PARCEL A)

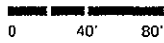
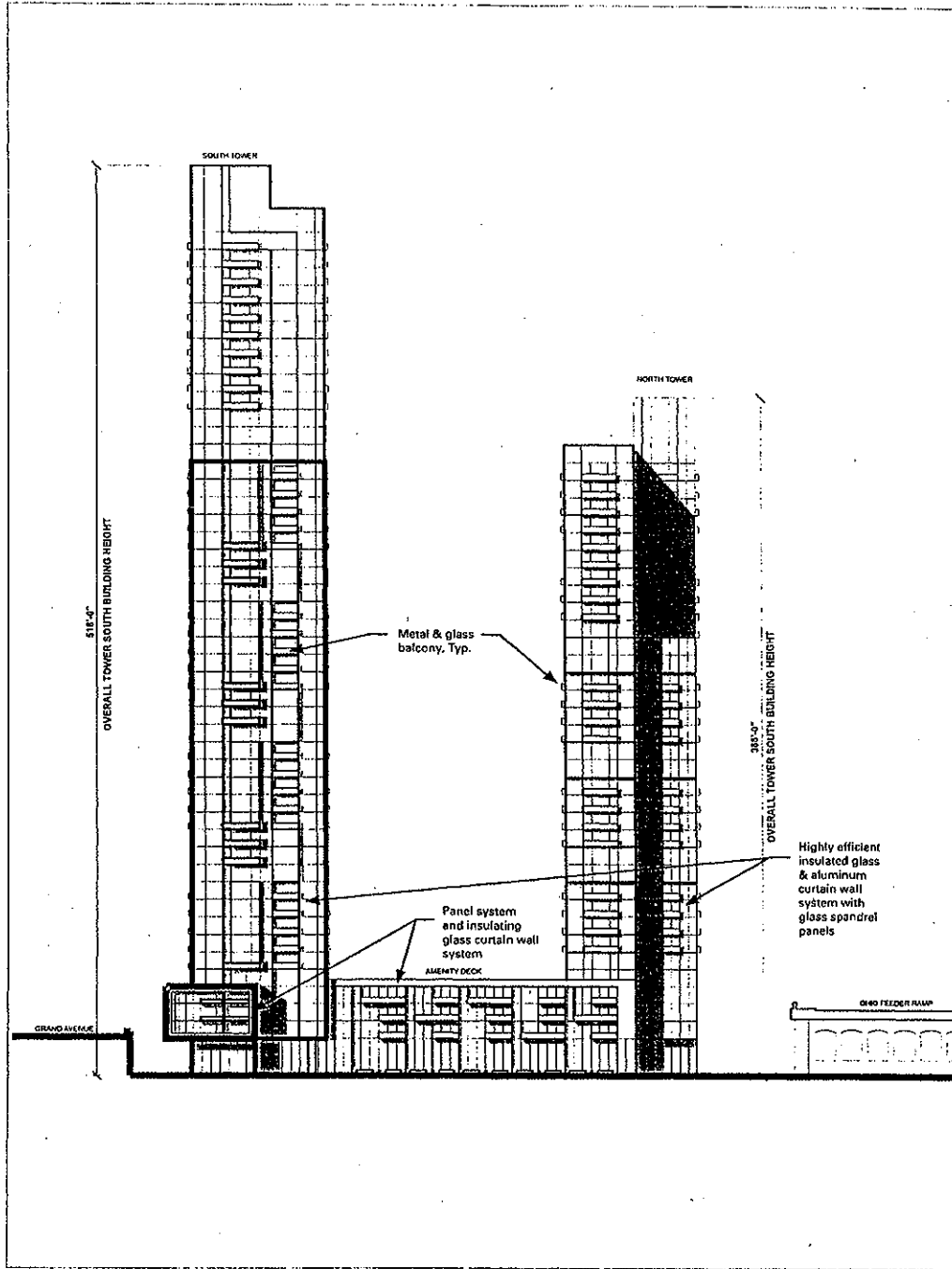


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Applicant: IL-777 WEST CHICAGO AVENUE LLC
643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVÉ

Date: 10 - 18 - 2018

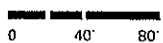
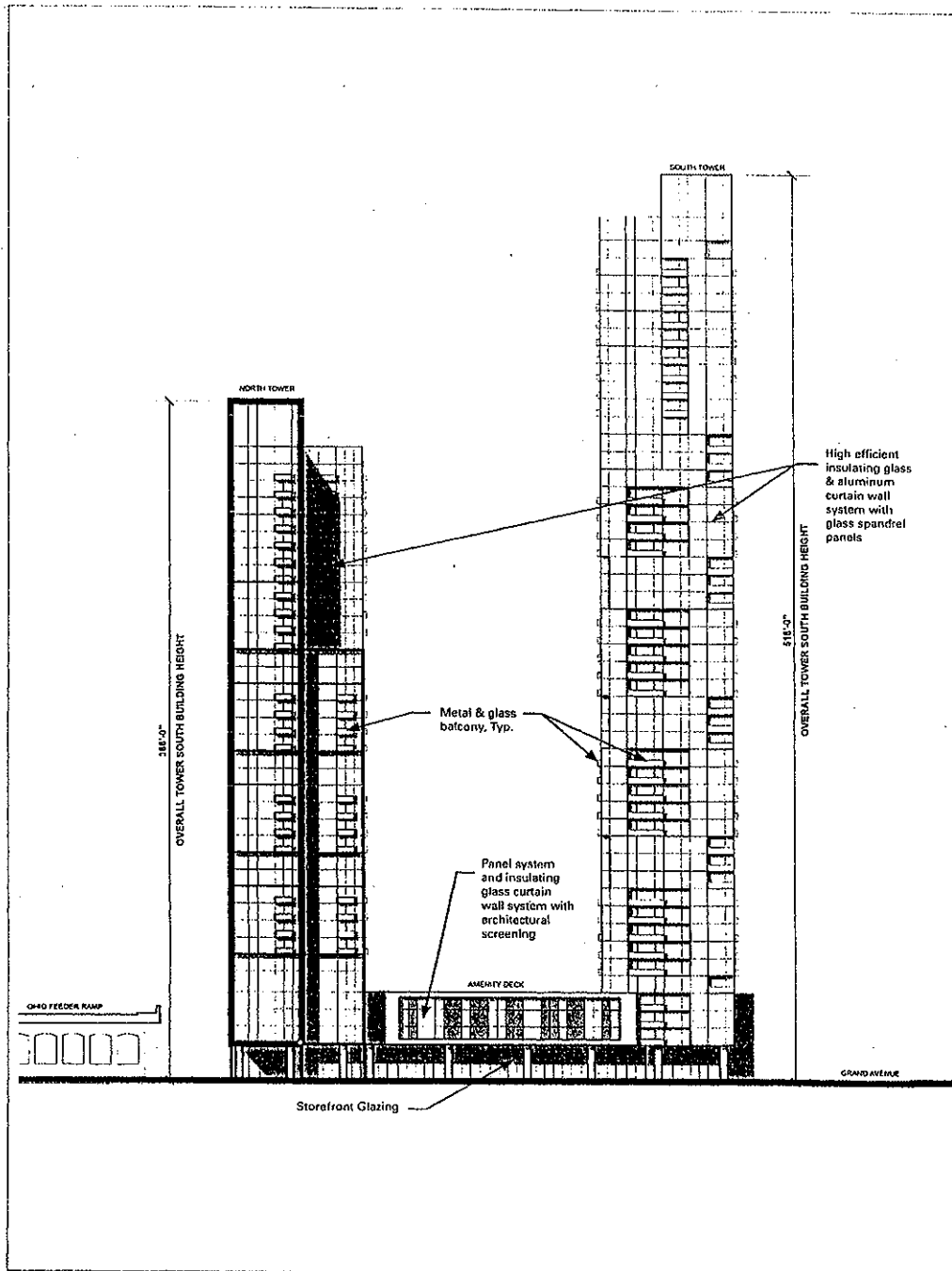


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PHASE 1A.1 BUILDING ELEVATION: EAST (PARCEL A)

Applicant: IL-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE
Date: 10 - 18 - 2018



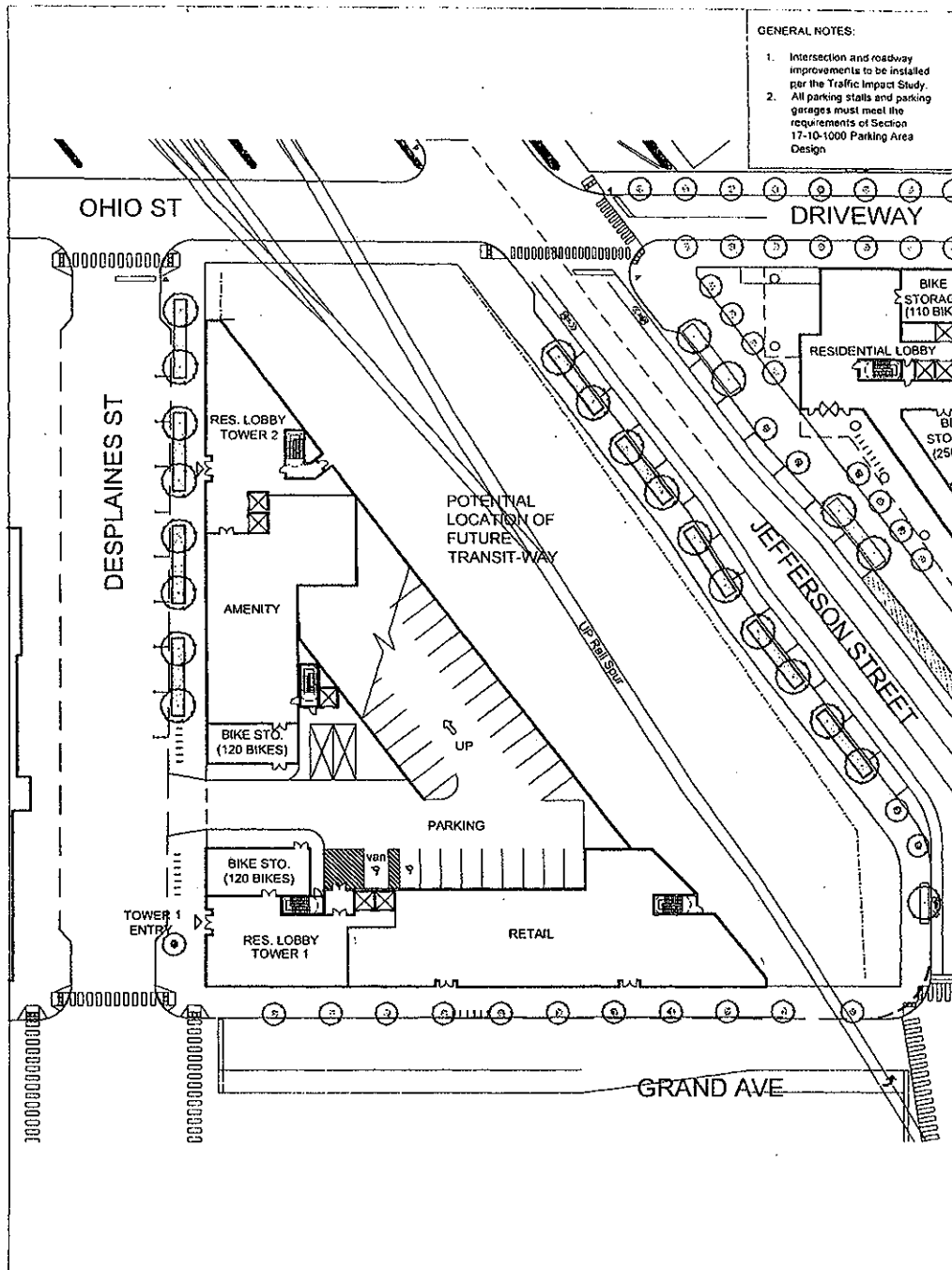
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PHASE 1A.1 BUILDING ELEVATION: WEST (PARCEL A)

Applicant: 11-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

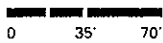
Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018



GENERAL NOTES:

1. Intersection and roadway improvements to be installed per the Traffic Impact Study.
2. All parking stalls and parking garages must meet the requirements of Section 17-10-1000 Parking Area Design

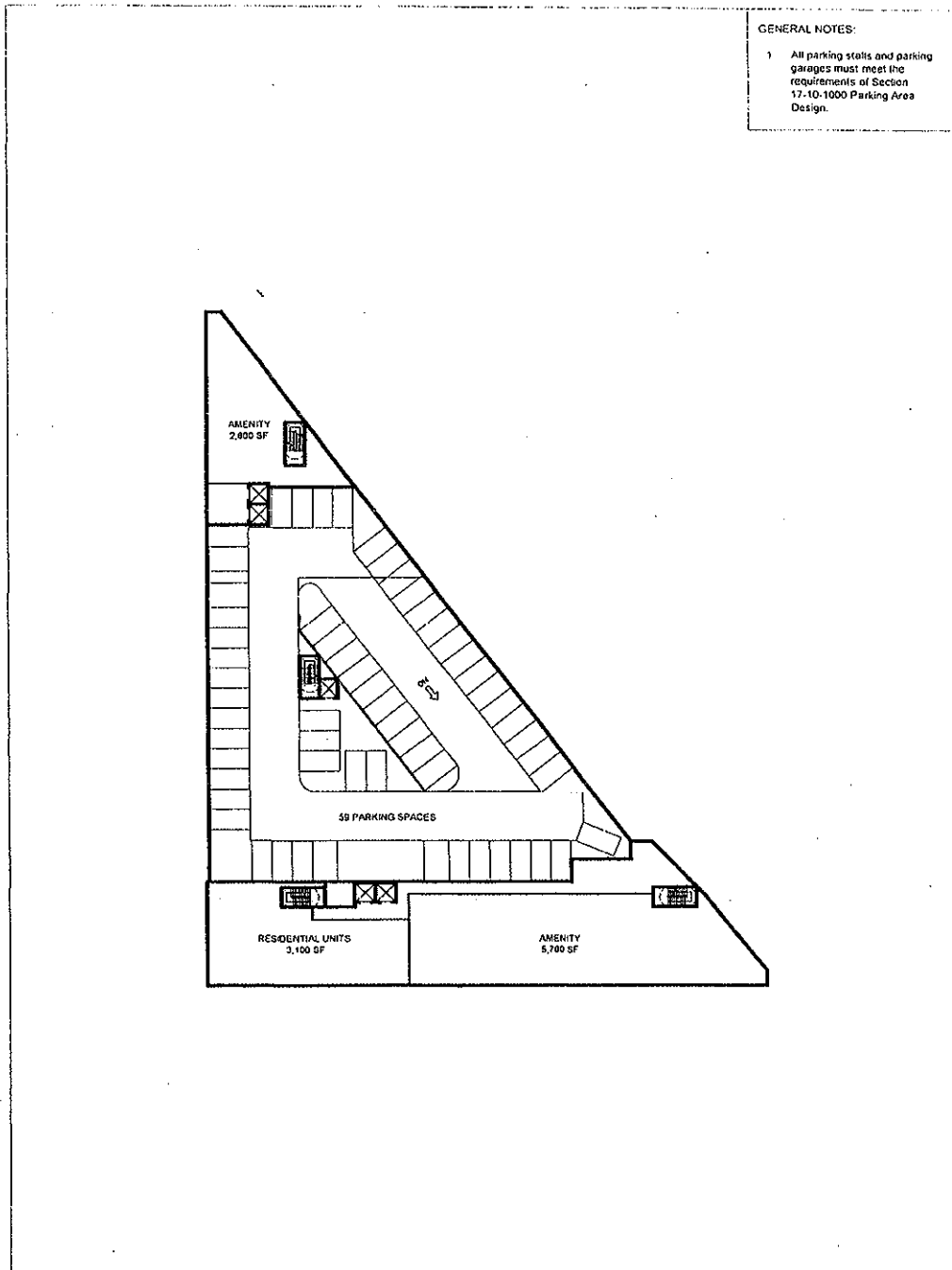


PHASE 1A.2 FLOOR PLAN: GROUND LEVEL 1 PLAN (PARCEL B)

Applicant: 1L-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 601-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018



GENERAL NOTES:
1 All parking stalls and parking garages must meet the requirements of Section 17-10-1000 Parking Area Design.

0 35' 70'



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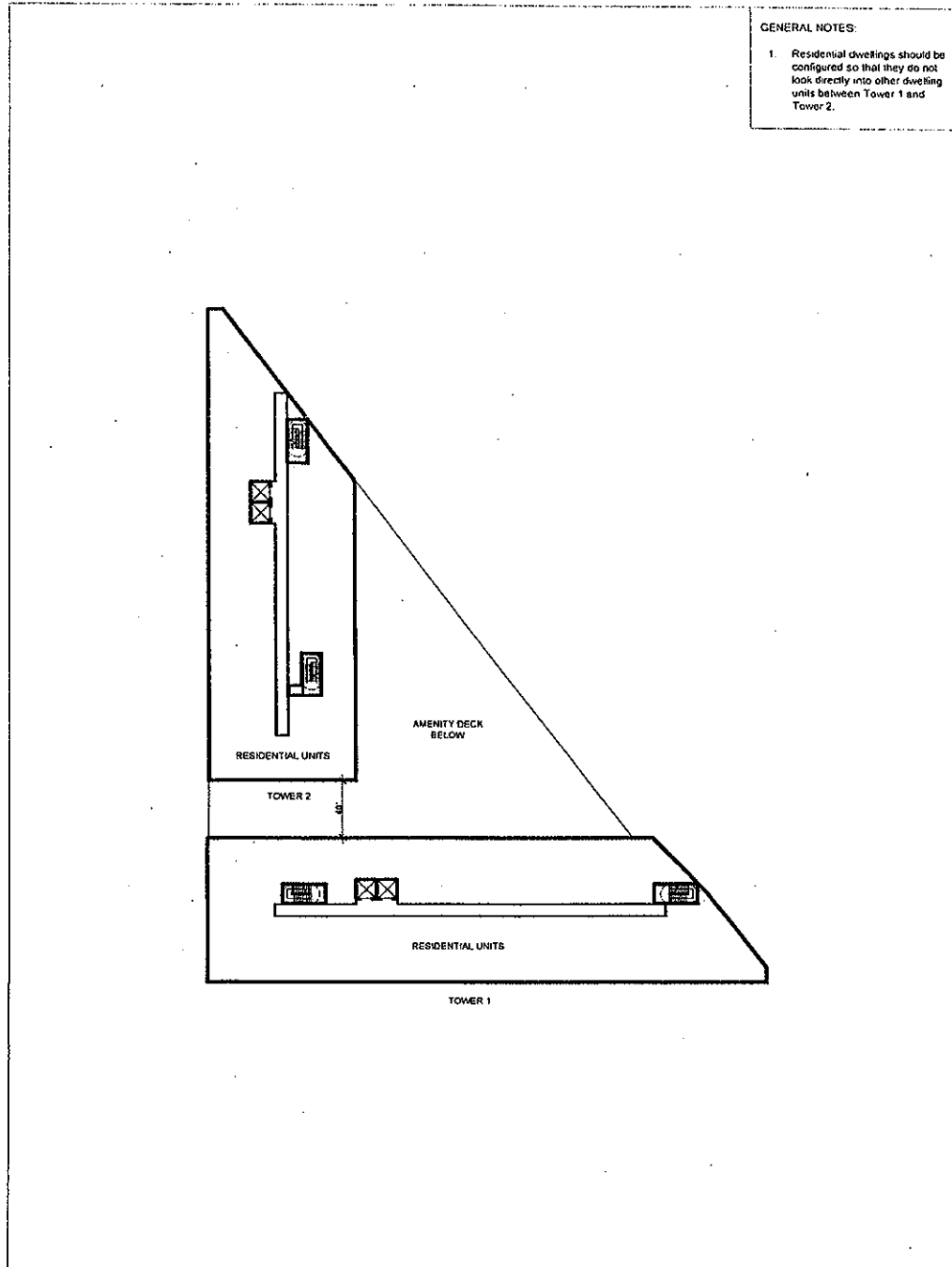
PHASE 1A.2 FLOOR PLAN: LEVEL 2 PLAN(PARCEL B)

Applicant: IL-777 WEST CHICAGO AVENUE LLC
643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018

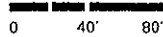
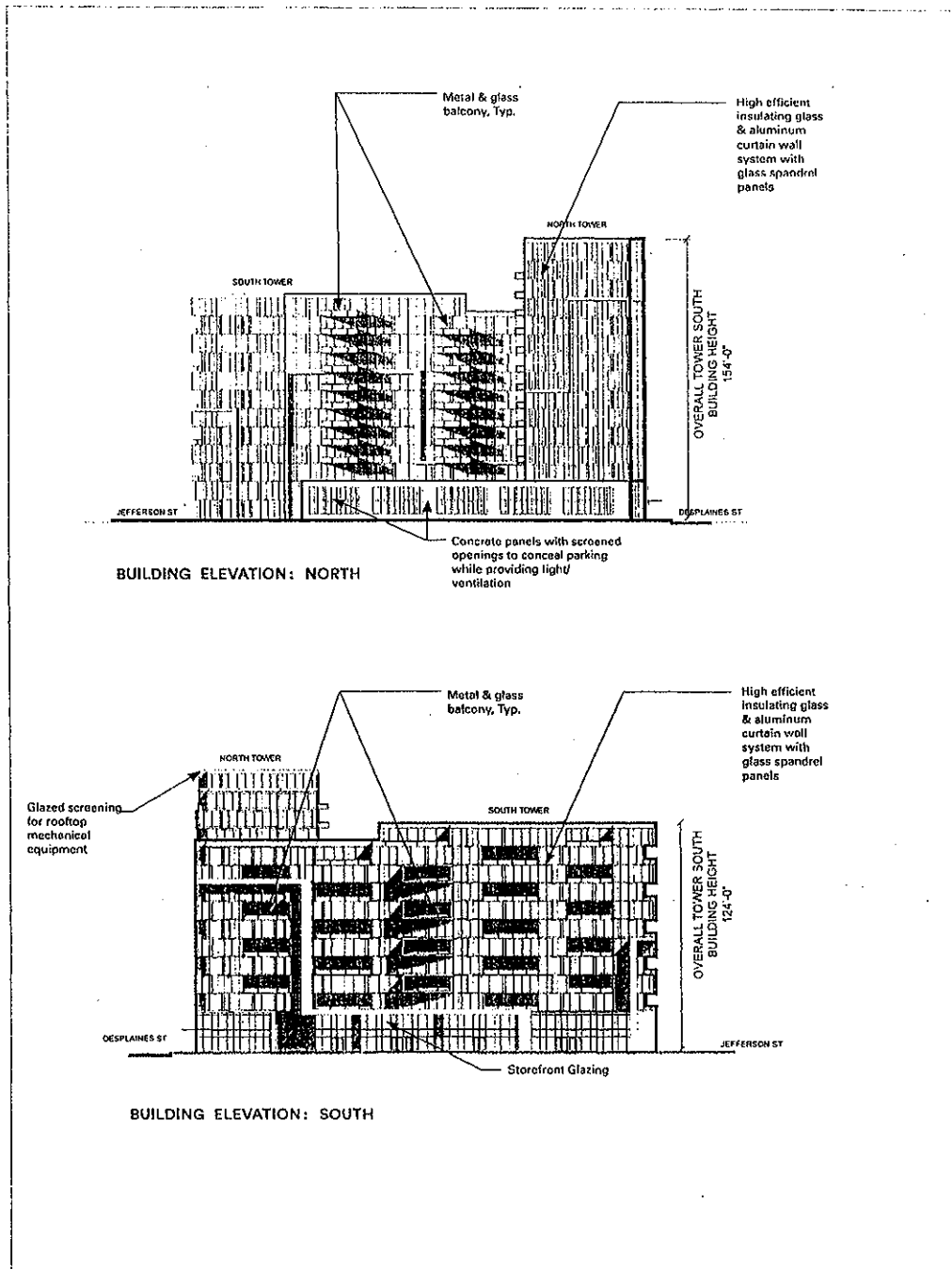
3. INITIAL CITY COUNCIL MEETING



PHASE 1A.2 FLOOR PLAN: TOWER PLAN (PARCEL B)

Applicant: 1L-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE
Date: 10 - 18 - 2018



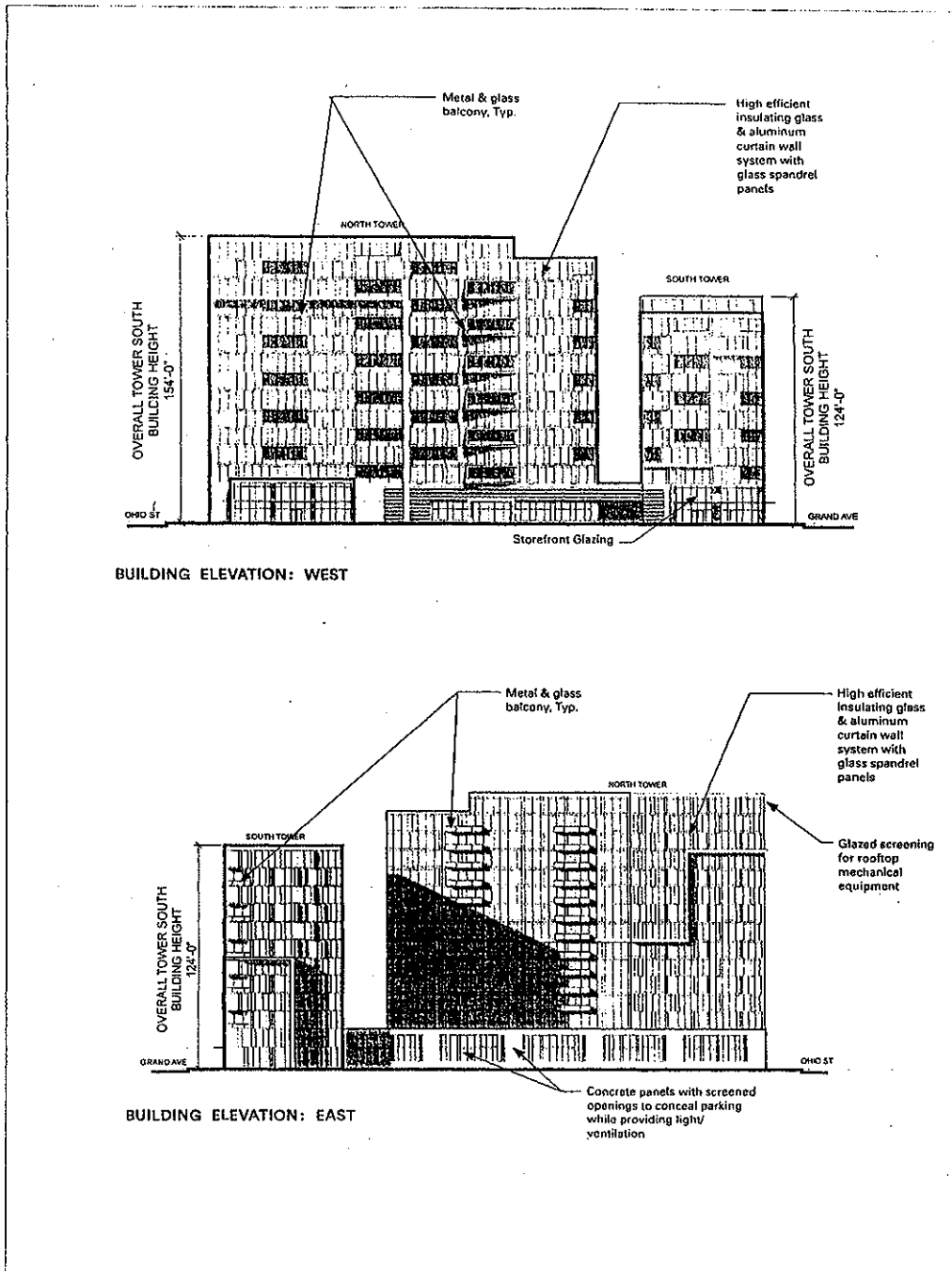
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PHASE 1A.2 BUILDING ELEVATION: NORTH AND SOUTH (PARCEL B)

Applicant: IL-777 WEST CHICAGO AVENUE LLC
643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE

Date: 10 - 18 - 2018



0 40' 80'



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PHASE 1A.2 BUILDING ELEVATION: WEST AND EAST (PARCEL B)

Applicant: 11-777 WEST CHICAGO AVENUE LLC
 643-741 W. CHICAGO AVENUE; 641-739 N. HALSTED STREET;
 632-740 W. ERIE STREET; 627-661 W. ERIE STREET;
 501-31 N. DESPLAINES STREET; 524-630 W. GRAND AVENUE

Project: 777 W CHICAGO AVE

Date: 10 - 16 - 2018

